

## MEDIA STATEMENT

<b>Subject</b>	Yeppoon Road Safety Improvements		
<b>Due date (expiry date for release or embargo date)</b>	31 August 2020	<b>DocTrak ID</b>	MSxxxx
<b>Written by</b>	Janelle Chapman Principal Communications Advisor PDOCSMCQ Region/Fitzroy 17 July 2020	<b>Approved by</b>	Peter Trim District Director (Fitzroy)  Dave Grosse Regional Director (Central Queensland) Central Queensland Region xx July 2020
<b>For release by</b>	<input checked="" type="checkbox"/> Minister for Transport and Main Roads <input type="checkbox"/> Department	<b>Media and Issues contact</b>	
<b>Distribution</b>	All Qld Media		
<b>Funding</b>	<input checked="" type="checkbox"/> State Government <input checked="" type="checkbox"/> Federal Government - Has federal approval been received for release of media statement? Yes/No <input type="checkbox"/> Regional Council		
<b>Impacted stakeholders</b>	Road users		
<b>Electorate/s</b>	Rockhampton, Keppel, Mirani (State); Capricornia (Federal)		
<b>SOCIAL MEDIA PITCH (if approved, email social media pitch and photo/video to <a href="mailto:social.media@tmr.qld.gov.au">social.media@tmr.qld.gov.au</a> and <a href="mailto:media@tmr.qld.gov.au">media@tmr.qld.gov.au</a>)</b>			
<b>Opportunity</b>	<input type="checkbox"/> Facebook <input type="checkbox"/> Twitter <input type="checkbox"/> LinkedIn		
<b>Proposed copy:</b>			
<b>Photo/Video:</b>	[insert photo if available otherwise provide a description of the visual accompaniment] (Videos will need to be emailed to <a href="mailto:social.media@tmr.qld.gov.au">social.media@tmr.qld.gov.au</a> and <a href="mailto:media@tmr.qld.gov.au">media@tmr.qld.gov.au</a> separately with the MS number and title).		

15 August 2020

### Central Queensland to have their say on Yeppoon Road upgrade

The Department of Transport and Main Roads has introduced road safety enhancements at the Artillery Road/Dairy Inn Road intersection with Rockhampton-Yeppoon Road, following a road safety investigation in November 2019.

Transport and Main Roads Minister Mark Bailey said as a result of the investigation, a suite of safety enhancements was completed in June 2020.

"To improve awareness of the intersection with Rockhampton-Yeppoon Road, the speed on Artillery Road and Dairy Inn Road approaches has been reduced from 80km/h to 60km/h," Mr Bailey said.

"We have also introduced additional signage on Artillery and Dairy Inn Roads to alert road users to the approaching intersection with Rockhampton-Yeppoon Road."

"The signage includes "60 Ahead" advisory signs, large size 60km/h regulatory speed signage, digital flashing signs that flash "SLOW DOWN" when a vehicle is approaching Rockhampton-Yeppoon Road at a greater speed than 60km/h and "GIVE WAY" painted on the road surface."

"The signage location has also been moved to provide additional advance warning to drivers before the intersection."

Initial safety improvements on the Rockhampton-Yeppoon Road include installation of intersection warning signs and additional overtaking lane signage in the vicinity of the intersection with Artillery and Dairy Inn Road.

Further safety enhancements are in the pipeline for the approaches to the intersection and will be completed in coming months. These additional changes will include:

- Reduction of the speed zone on the Rockhampton – Yeppoon Road either side of the intersection
- Advance warning signs to conjunction with the reduce speed signs
- Vehicle activated warning signs on the Rockhampton – Yeppoon Road to reinforce the reduced speed through the intersection

"This safety work is in advance of the Yeppoon Road Upgrade project, an \$80 million project to improve travel times and capacity, road safety, local access and flood immunity on this important regional freight and commuter route," Mr Bailey said.

The Australian Government has committed \$64 million, and the Queensland Government \$16 million for upgrade works under the Roads of Strategic Importance corridor investment. \$10 million to progress the Scoping Phase of the project was announced in early July 2020.

"Community consultation will commence later this month to give road users an opportunity to share their priorities for targeted upgrades on this critical strategic regional road linking the Capricorn Coast to Rockhampton and the Bruce Highway."

ENDS

**Media contact:            Name, phone number**

Released Under FOI

## Profile Request

<b>Priority:</b>	Urgent		
<b>Department: *</b>	Transport and Main Roads	<b>Accountable Area: *</b>	Program Delivery and Operations
<b>Contact Confirmation Number:</b>			

Document Details			
<b>Document Type:</b>	Media Statement	<b>Sub Document Type: *</b>	Statement instigated by Department
		<b>Minister:</b>	Minister for Transport and Main Roads
<b>Subject:</b>	Rockhampton- Yeppoon Road - Safety Enhancements at intersection with Artillery Road and Dairy Inn Road		
<b>Notes:</b>	Safety work completion on local side road and planned work for approaches to intersection on Rton-Yeppoon Road including speed reduction.		
<b>Topic Name:</b>	Safety - Road (TMR)		
<b>Subject Electorate:</b>	Keppel		
<b>Deadline:</b>	24/07/2020		

Requested by: Janelle M Chapman  
 Date requested: 22/07/2020 01:21:20 PM

Released under RTI / DMR

## Hannah M Sorensen

---

**From:** Mailbox DocTrak Media - TMR  
**Sent:** Thursday, 25 June 2020 11:26 AM  
**To:** Hannah M Sorensen  
**Subject:** FW: Yeppoon-Rocky Road at the Artillery Road intersection

---

**From:** personal information @ministerial.qld.gov.au>  
**Sent:** Thursday, 25 June 2020 11:25 AM  
**To:** Mailbox DocTrak Media - TMR <mail.doctrak.media.tmr@tmr.qld.gov.au>  
**Cc:** Media <media@tmr.qld.gov.au>; MinisterBaileyMedia@ministerial.qld.gov.au; Natalie Gauld <Natalie.Z.Gauld@tmr.qld.gov.au>  
**Subject:** Re: Yeppoon-Rocky Road at the Artillery Road intersection

Thanks, Hannah - the sooner we can receive it would be much appreciated 😊

Thanks,

personal information  
NR

Sent from my iPhone

On 25 Jun 2020, at 11:22 am, Mailbox DocTrak Media - TMR <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)> wrote:

Thanks personal information

I've spoken to the region and they are going to put a HIB together with advice.

Kind regards,

**Hannah Sorensen**  
Advisor | Media  
**Governance Branch** | Corporate Division | Department of Transport and Main Roads

---

Floor 8 | 61 Mary Street | Brisbane Qld 4000  
GPO Box 1549 | Brisbane Qld 4001  
(07) 3066 7251  
[hannah.m.sorensen@tmr.qld.gov.au](mailto:hannah.m.sorensen@tmr.qld.gov.au)  
[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** personal information @ministerial.qld.gov.au>  
**Sent:** Thursday, 25 June 2020 10:50 AM  
**To:** Mailbox DocTrak Media - TMR <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)>; Media <[media@tmr.qld.gov.au](mailto:media@tmr.qld.gov.au)>  
**Cc:** [MinisterBaileyMedia@ministerial.qld.gov.au](mailto:MinisterBaileyMedia@ministerial.qld.gov.au); Natalie Gauld <[Natalie.Z.Gauld@tmr.qld.gov.au](mailto:Natalie.Z.Gauld@tmr.qld.gov.au)>  
**Subject:** RE: Yeppoon-Rocky Road at the Artillery Road intersection

The local MP has received a media enquiry from the Morning Bulletin. Can we get some advice what happened RE: the investigation?

My understanding is that the investigation concluded it was driver error. QPS' investigation I understand came to the same conclusion.

Can we get a summary of what the investigation found, and subsequently what recommendations were made?

---

**From:** Mailbox DocTrak Media - TMR <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)>

**Sent:** Thursday, 25 June 2020 10:35 AM

**To:** personal information <[personal information@ministerial.qld.gov.au](mailto:personal information@ministerial.qld.gov.au)>; Media <[media@tmr.qld.gov.au](mailto:media@tmr.qld.gov.au)>; Mailbox DocTrak Media - TMR <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)>

**Cc:** @Minister Bailey Media <[MinisterBaileyMedia@ministerial.qld.gov.au](mailto:MinisterBaileyMedia@ministerial.qld.gov.au)>; Natalie Gauld <[Natalie.Z.Gauld@tmr.qld.gov.au](mailto:Natalie.Z.Gauld@tmr.qld.gov.au)>

**Subject:** RE: Yeppoon-Rocky Road at the Artillery Road intersection

Hi personal information

Could we get some more background please? Do you need the info to respond to a media enquiry?

Kind regards,

**Hannah Sorensen**

Advisor | Media

**Governance Branch** | Corporate Division | Department of Transport and Main Roads

---

Floor 8 | 61 Mary Street | Brisbane Qld 4000

GPO Box 1549 | Brisbane Qld 4001

(07) 3066 7251

[hannah.m.sorensen@tmr.qld.gov.au](mailto:hannah.m.sorensen@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** personal information <[personal information@ministerial.qld.gov.au](mailto:personal information@ministerial.qld.gov.au)>

**Sent:** Thursday, 25 June 2020 10:33 AM

**To:** Media <[media@tmr.qld.gov.au](mailto:media@tmr.qld.gov.au)>; Mailbox DocTrak Media - TMR <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)>

**Cc:** [MinisterBaileyMedia@ministerial.qld.gov.au](mailto:MinisterBaileyMedia@ministerial.qld.gov.au)

**Subject:** Yeppoon-Rocky Road at the Artillery Road intersection

Hi team,

Following the fatal crash at Yeppoon-Rocky Road at the Artillery Road intersection last year, apparently TMR conducted an investigation.

Below is the response from March last year.

Can we find out what happened?

Thanks,

personal information

---

**Response: (Please attribute to a Transport and Main Roads spokesperson)**

We are investigating the circumstances following a tragic death on Rockhampton-Yeppoon Road on Sunday.

In addition to the Queensland Police investigation, we will separately investigate the circumstances surrounding the crash.

Our investigation is not a duplication of the Queensland Police Forensic Crash Unit investigation.

We will collect road-based information from the approaches to the crash sites to determine if any remedial measures or road improvements are required.

The initial fatal crash investigation is expected to be completed and recommendations provided by early next month.

We cannot pre-empt the findings of this report before it is delivered.

Motorists must take the fatal five seriously: drink and drug driving, fatigue, inattention, not using a seat belt and speeding.

We all have a role to play in road safety, and motorists are reminded to obey the speed limits, abide by the road rules and drive to conditions.

**Background**

The Artillery Road (Dairy Inn Road) intersection is constructed with protected turn lanes for Rockhampton-Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection lighting and intersection warning signage in place on all approaches.

This email, together with any attachments, is intended for the named recipient(s) only; and may contain privileged and confidential information. If received in error, you are asked to inform the sender as quickly as possible and delete this email and any copies of this from your computer system network.

If not an intended recipient of this email, you must not copy, distribute or take any action(s) that relies on it; any form of disclosure, modification, distribution and /or publication of this email is also prohibited.

Unless stated otherwise, this email represents only the views of the sender and not the views of the Queensland Government.

Please consider the environment before printing this email.

\*\*\*\*\*

WARNING: This email (including any attachments) may contain legally privileged, confidential or private information and may be protected by copyright. You may only use it if you are the person(s) it was intended to be sent to and if you use it in an authorised way. No one is allowed to use, review, alter, transmit, disclose, distribute, print or copy this email without appropriate authority.

If this email was not intended for you and was sent to you by mistake, please telephone or email me immediately, destroy any hardcopies of this email and delete it and any copies of it from your computer system. Any right which the sender may have under copyright law, and any legal privilege and confidentiality attached to this email is not waived or destroyed by that mistake.

It is your responsibility to ensure that this email does not contain and is not affected by computer viruses, defects or interference by third parties or replication problems (including incompatibility with your computer system).

Opinions contained in this email do not necessarily reflect the opinions of the Department of Transport and Main Roads, or endorsed organisations utilising the same infrastructure.

\*\*\*\*\*

This email, together with any attachments, is intended for the named recipient(s) only; and may contain privileged and confidential information. If received in error, you are asked to inform the sender as quickly as possible and delete this email and any copies of this from your computer system network.

If not an intended recipient of this email, you must not copy, distribute or take any action(s) that relies on it; any form of disclosure, modification, distribution and /or publication of this email is also prohibited.

Unless stated otherwise, this email represents only the views of the sender and not the views of the Queensland Government.

Please consider the environment before printing this email.

This email, together with any attachments, is intended for the named recipient(s) only; and may contain privileged and confidential information. If received in error, you are asked to inform the sender as quickly as possible and delete this email and any copies of this from your computer system network.

If not an intended recipient of this email, you must not copy, distribute or take any action(s) that relies on it; any form of disclosure, modification, distribution and /or publication of this email is also prohibited.

Unless stated otherwise, this email represents only the views of the sender and not the views of the Queensland Government.

Please consider the environment before printing this email.

Released under RTI  
DTMR

## Hannah M Sorensen

---

**From:** Mailbox DocTrak Media - TMR  
**Sent:** Friday, 29 March 2019 12:43 PM  
**To:** Sch.4 Part 4 s.6 PI@capnews.com.au  
**Cc:** Media  
**Subject:** Yeppoon and Artillery road intersection

Hi Sch.4 Part 4 s.6 PI

We have received your enquiry.

We'll get working on a response and get something through to you as soon as we can.

Thanks

### Stacey Fenech

A/Principal Advisor (Media) | Media Unit

**Governance Branch** | Corporate Division | Department of Transport and Main Roads

---

Floor 1 | 61 Mary Street | Brisbane City Qld 4000

GPO Box 1549 | Brisbane City Qld 4000

(07) 30667060

[media@tmr.qld.gov.au](mailto:media@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From** Sch.4 Part 4 s.6 PI

**Sent:** Thursday, 28 March 2019 5:34 PM

**To:** 'Mailbox DocTrak Media - TMR' <[mail.doctrak.media.tmr@tmr.qld.gov.au](mailto:mail.doctrak.media.tmr@tmr.qld.gov.au)>

**Subject:** RE: test

Good afternoon,

Thanks for your time on the phone Tom.

Further to our conversation, in tomorrow's paper, in response to a community outcry after a serious fatal accident on Sunday, we will be calling for a 80kmh zone to be put in place while investigations continue into the cause of the crash.

The Yeppoon Rd, Artillery Rd intersection is notorious and has been the site of many accidents and near misses in recent years.

I understand the investigations will take time but there has been an overwhelming call for an 80km zone to be put in place in the meantime.

- What is involved in doing that?
- What reasons might be given for not doing so?
- How does TMR view this?
- What would be needed to make this kind of decision quickly?

<https://www.themorningbulletin.com.au/news/bipartisan-agreement-on-urgent-road-upgrades-on-ho/3683451/>

<https://www.themorningbulletin.com.au/news/russian-roulette-intersection-death-spurs-calls-fo/3681390/>

Regards

Sch.4 Part 4 s.6 PI



Sch.4 Part 4 s.6 PI

Chief Reporter

220 Bolsover Street Rockhampton QLD 4700

T +61 7 4970 3050 M + [redacted] NR

E [redacted]@capnews.com.au

W NewsCorpAustralia.com

Follow us online  

**The Bulletin**

Proudly supporting [1 degree](#), A News Corp Australia initiative.

*News Corp Australia*

Released under RTI - DTMR

# MEDIA ENQUIRY

<b>Subject</b>	Rockhampton Yeppoon Road Fatal Crash Investigation Process		
<b>Deadline</b>	29/3/2019	<b>DocTrak ID</b>	ME12019
<b>Journalist</b>	Morning Bulletin	<b>Contact details</b>	Morning Bulletin
<b>Written by</b>	Kris Biddle Communication Advisor (07) 4931 1546	<b>Approved by</b>	Dave Grosse Regional Director (CQ) PDO/CQ Region/Fitzroy 25/03/2019
<b>For release by</b>	<input type="checkbox"/> Minister for Transport and Main Roads <input checked="" type="checkbox"/> Department	<b>Media and Issues contact</b>	
<b>Background</b>			

## Questions:

Good afternoon,

Thanks for your time on the phone personal

Further to our conversation, in tomorrow's paper, in response to a community outcry after a serious fatal accident on Sunday, we will be calling for a 80kmh zone to be put in place while investigations continue into the cause of the crash.

The Yeppoon Rd, Artillery Rd intersection is notorious and has been the site of many accidents and near misses in recent years.

I understand the investigations will take time but there has been an overwhelming call for an 80km zone to be put in place in the meantime.

- What is involved in doing that?
- What reasons might be given for not doing so?
- How does TMR view this?
- What would be needed to make this kind of decision quickly?

<https://www.themorningbulletin.com.au/news/bipartisan-agreement-on-urgent-road-upgrades-on-ho/368345/>

<https://www.themorningbulletin.com.au/news/russian-roulette-intersection-death-spurs-calls-fo/3681390/>

## Response: (Please attribute to a Transport and Main Roads spokesperson)

We are aware of the tragic death on Rockhampton-Yeppoon Road on Sunday.

In addition to Queensland Police, we will investigate the circumstances surrounding the incident in line with our protocols for fatalities on state-controlled roads.

TMR's investigations are not a duplication of the Queensland Police Forensic Crash Unit investigation. The purpose is to collect road-based information at

and on the approaches to fatal crash sites to determine if there are any remedial measures or road improvements that may be required to prevent future crashes at the location.

The initial fatal crash investigation is expected to be complete and recommendation identified by 5 April 2019.

TMR cannot pre-empt the findings of that report before it is delivered.

### **Background**

The Artillery Road (Dairy Inn Road) intersection is constructed with protected turn lanes for Rockhampton-Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection lighting and intersection warning signage in place on all approaches.

**ENDS**

Released under RTI - TMR

## Hannah M Sorensen

---

**From:** Erin L Goldsack  
**Sent:** Friday, 29 March 2019 3:57 PM  
**To:** Sch.4 Part 4 s.6 PI  
**Cc:** Media  
**Subject:** TMR media

Hi Sch.4 Part 4 s.6 PI

Please find a response to enquiry below.

Thanks,  
Erin

### **Response: (Please attribute to a Transport and Main Roads spokesperson)**

We are investigating the circumstances following a tragic death on Rockhampton-Yeppoon Road on Sunday.

In addition to the Queensland Police investigation, we will separately investigate the circumstances surrounding the crash.

Our investigation is not a duplication of the Queensland Police Forensic Crash Unit investigation.

We will collect road-based information from the approaches to the crash sites to determine if any remedial measures or road improvements are required.

The initial fatal crash investigation is expected to be completed and recommendations provided by early next month.

We cannot pre-empt the findings of this report before it is delivered.

Motorists must take the fatal five seriously: drink and drug driving, fatigue, inattention, not using a seat belt and speeding.

We all have a role to play in road safety, and motorists are reminded to obey the speed limits, abide by the road rules and drive to conditions.

### **Background**

The Artillery Road (Dairy Inn Road) intersection is constructed with protected turn lanes for Rockhampton-Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection lighting and intersection warning signage in place on all approaches.

### **ENDS**

**The Media Unit**  
**Governance Branch** | Corporate Division | Department of Transport and Main Roads

---

Floor 1 | 61 Mary Street | Brisbane City Qld 4000  
GPO Box 1549 | Brisbane City Qld 4000  
(07) 30667060  
[media@tmr.qld.gov.au](mailto:media@tmr.qld.gov.au)  
[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

25 June 2020

## Rockhampton Yeppoon Road/Artillery Road intersection

### **Media response**

After a fatal traffic crash on 24 March 2019, both the Queensland Police Service and the Department of Transport and Main Roads undertook fatal crash investigations which determined that the road and road conditions did not contribute to the tragic crash.

The Department of Transport and Main Roads also undertook a road safety investigation in November 2019 to determine any improvements that would enhance the safety of the Artillery Road/Dairy Inn Road intersection with Rockhampton-Yeppoon Road.

As a result of the safety investigation, a number of safety enhancements were identified, including:

- Reducing the speed on the Artillery Road and Dairy Inn Road approaches to Rockhampton-Yeppoon Road from 80km/h to 60km/h.
- Improving signage on the Artillery Road and Dairy Inn Road approaches to Rockhampton-Yeppoon Road by installing:
  - "60 Ahead" advisory signs
  - large size 60km/h regulatory speed signage
  - digital flashing signs that flash "SLOW DOWN" when a vehicle is approaching Rockhampton-Yeppoon Road at a greater speed than 60km/h
  - "GIVE WAY" painted on the road surface
  - improved the location of this signage to provide additional advance warning before the intersection.
- Improving signage on Rockhampton-Yeppoon Road by installing:
  - intersection warning signs
  - additional overtaking lane signage.

These works were completed on 19 June 2020.

Prepared by:	Document author	Contact for further information:	Content expert
	Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR		Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR

25 June 2020

## Rockhampton Yeppoon Road/Artillery Road intersection

### Additional information for media

In 2017, the Department of Transport and Main Roads commenced strategic planning to determine the feasibility, including staging options and strategic costs, to four-lane the remaining 21.7 kilometre 2-lane section of the Rockhampton–Yeppoon Road, between Iron Pot Road and Tanby Road.

In the 2019 Federal Budget, the Australian Government committed \$64 million towards upgrading the Rockhampton–Yeppoon Road under the federal Roads of Significant Importance (ROSI) initiative, based on 80:20 federal:state funding arrangements.

The Queensland Transport and Roads Investment Program 2019–20 to 2022–23 includes the Queensland Government's \$16 million in matching Roads of Strategic Importance funding towards the Rockhampton–Yeppoon Road bringing the total allocation to \$80 million.

The Queensland Government will work with the Australian Government to determine priorities for upgrading the Rockhampton–Yeppoon Road to deliver targeted upgrades, including duplicating sections of Yeppoon Road.

This week (25 June 2020) the Australian Government approved funding for development of the Business Case, which is expected to be completed in mid-2021.

### **BACKGROUND**

- The Queensland Police Service investigation into the fatal crash at the intersection of Artillery Road/Dairy Inn Road and Rockhampton–Yeppoon Road on 24 March 2019, concluded the primary contributing factor as driver inattention/distraction. Department of Transport and Main Roads' (TMR) investigation identified that the road conditions did not contribute to the crash.
- Artillery Road is an alternate connection to the Bruce Highway north of Rockhampton, and a shortcut for motorists travelling from the Capricorn Coast to the north of Rockhampton. Dairy Inn Road connects Rockhampton–Yeppoon Road to the Cawarral community, between Rockhampton and Yeppoon.

Prepared by:	Document author	Contact for further information:	Content expert
	Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR		Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR

25 June 2020

## Rockhampton Yeppoon Road/Artillery Road intersection

- TMR has recorded six crashes within a 500-metre radius of the Artillery Road/Dairy Inn Road intersection between 2014 and 2019.
- The intersection is currently constructed with eastbound and westbound protected right-turn lanes for Rockhampton–Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection flag lighting located on all intersection legs and warning signage is in place on all intersection approaches.
- Additional works to enhance the safety of the intersection was undertaken by TMR in November 2019. Outcomes of this investigation included:
  - Reducing the speed on the Artillery Road and Dairy Inn Road approaches to Rockhampton-Yeppoon Road from 80km/h to 60km/h.
  - Improving signage on the Artillery Road and Dairy Inn Road approaches to Rockhampton-Yeppoon Road by installing:
    - "60 Ahead" advisory signs
    - large size 60km/h regulatory speed signage
    - digital flashing signs that flash "SLOW DOWN" when a vehicle is approaching Rockhampton-Yeppoon Road at a greater speed than 60km/h
    - "GIVE WAY" painted on the road surface
    - improved the location (distance from intersection) of this signage to provide additional advance warning.
  - Improving signage on Rockhampton-Yeppoon Road by installing:
    - intersection warning signs
    - additional overtaking lane signage.
  - These works were completed on 19 June 2020.

### General background information

- Rockhampton–Yeppoon Road is one of two state-controlled roads connecting Rockhampton and the Capricorn Coast in Central Queensland. The corridor serves as the principal link between the regional centres of Rockhampton and Yeppoon and serves as the primary connection between the wider Capricorn Coast area and the Bruce Highway.
- 2018 TMR traffic data records 11,558 vehicles per day using Rockhampton–Yeppoon Road in the area of Artillery Road/Dairy Inn Road intersection. Traffic volumes on the road have increased by 1.2 per cent annually, over the past five years. Rockhampton–Yeppoon Road is a B-double route (23 metre and 25 metre configuration).
- The road caters for commuter traffic travelling between Rockhampton and Yeppoon, freight movements serving the two centres, the wider Capricorn Coast and surrounding agricultural properties, as well as tourists.
- The road is one of four main access routes used by the Australian Defence Force for access into the Shoalwater Bay Training Area.

Prepared by:	Document author	Contact for further information:	Content expert
	Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR		Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR

25 June 2020

## Rockhampton Yeppoon Road/Artillery Road intersection

- Rockhampton–Yeppoon Road is approximately 35.6 kilometres in length, comprised of two sections; a 4.03 kilometre section traversing the Rockhampton Central Business District (Fitzroy Street, Queen Elizabeth Drive and Musgrave Street) and the second a 31.6 kilometre section from the Bruce Highway intersection in Parkhurst to Tanby Road roundabout at Yeppoon.
- The section from the Bruce Highway to Tanby Road roundabout is made up of a four-lane section from chainage 4.03-13.9km, and a two-lane section for the remainder of the road from Iron Pot through to Tanby Road roundabout (chainage 13.9-35.6km).
- In 2017, the TMR commenced strategic planning to determine staging options to four-lane the remaining 21.7 kilometre section of Rockhampton–Yeppoon Road, between Ironpot and Yeppoon (Tanby Road roundabout). The planning identified the potential cost (based on a 2017 estimate) to install four lanes:
  - between Neils Road and the Tanby Road roundabout at Yeppoon required an investment of \$98 million (estimated cost if construction was to commence in 2019).
  - Further investment of \$64 million would be required to four-lane between Gravel Pit Road and Neils Road.
  - The 2020 Preliminary Evaluation identified investment of \$155.8 million to four-lane from Iron Pot to Gravel Pit Road.
- In the 2019 Federal Budget, the Australian Government committed \$64 million (Queensland Government \$16 million) towards upgrading the Rockhampton–Yeppoon Road under the federal Roads of Significant Importance (ROSI) initiative (80:20 funding split). \$80 million represents about 25 per cent of the total required budget to four-lane the remaining 21.7 kilometres of Rockhampton-Yeppoon Road.
- The Project Proposal Report was progressed to PIP on 8 June 2020. Australian Government approval for the release of \$8 million for Business Case development was received on 25 June 2020.
- The Department of Infrastructure, Transport, Cities and Regional Development's website states the project is expected to:
  - increase economic productivity by reducing costs to industry through better efficiency on the route and lower travel times
  - improve road safety by reducing unsafe overtaking and the associated risks of head-on collisions
  - increase route capacity to accommodate future growth in traffic volume through increased economic activity in the region.
- The approved timeline for the delivery of the \$80 million funding commitment:
  - submit PPR to Australian Government mid-2020
  - undertake high-level consultation July/August 2020
  - start Business Case activities August 2020

Prepared by:	Document author	Contact for further information:	Content expert
	Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR		Dave Grosse, Regional Director (Central Queensland) 4931 1639 NR



25 June 2020

**Rockhampton Yeppoon Road/Artillery Road intersection**

- start detailed design mid-2021
- start construction early-2023 (duration about 24 - 36 months).

Released under RTI - DTMR

<b>Prepared by:</b>	<i>Document author</i>	<b>Contact for further information:</b>	<i>Content expert</i>
	<i>Dave Grosse, Regional Director (Central Queensland) 4931 1639</i> NR		<i>Dave Grosse, Regional Director (Central Queensland) 4931 1639</i> NR

Not Relevant

Released under RTI - DTMR

Released under RTI - DTMR

Not Relevant

Released under RTI - DTMR

Not Relevant

Sch.3(6)(c)(i) Infringe the privileges of Parliament

<b>Division:</b>	<b>Infrastructure Management and Delivery – PDO Central Queensland Region</b>		
	<b>Executive Director:</b>	<b>General Manager:</b>	<b>Deputy Director-General:</b>
<b>Name:</b>	Dave Grosse	Les Dunn	Amanda Yeates
<b>Telephone:</b>	(w) 49311501 NR	(w) 3066 5498 NR	(w) 3066 7118 NR
<b>Date:</b>	6 April 2020	25 March 2020	25 March 2020

## Profile Request

<b>Priority:</b>	Routine	<b>Referring Site:</b>
<b>Department: *</b>	Transport and Main Roads	<b>Accountable Area: *</b> Unassigned
<b>Contact Confirmation Number:</b>	SG201903241313092737	

Document Details			
<b>Document Type:</b>	Website Enquiry	<b>Sub Document Type: *</b>	For you: information
<b>Subject:</b>	Enquiry	<b>Category:</b>	Enquiry
<b>Notes:</b>		<b>Third Party Details Consent Given:</b>	No
<b>Service/Request Type:</b>	Roads/Maintenance		

Address block details (on the outgoing letter)			
<b>Author Type:</b>	Member of the public	<b>Organisation:</b>	
<b>Author Name:</b>	Sch.4 Part 4 s.6 PI	<b>Preferred Method of Contact:</b>	No contact required
<b>Title:</b>		<b>Phone Number:</b>	
<b>First Name:</b>		<b>Email Address:</b>	
<b>Last Name:</b>			
<b>Address:</b>			
<b>Postcode:</b>			

Details	
<b>Has Enquirer Previously Raised Issue with Dept?:</b>	
<b>Feedback:</b>	Suggestion: Installation of traffic lights at the busy four-way intersection of Yeppoon Road, Artillery Road and Dairy Inn Road at Iron Pot, between Rockhampton and Yeppoon.

Sch.4 Part 4 s.6 PI

 consider installing traffic lights on Yeppoon Road at Iron Pot, in an attempt to clear a much safer path for those motorists attempting to enter Yeppoon Road from Artillery Road and Dairy Inn Road.

With yet another serious traffic accident occurring at the intersection of Yeppoon Road/Artillery Road/Dairy Inn Road today, between the city of Rockhampton and the Capricorn Coast, I'm hoping the Department of Transport and Main Roads can finally respect and appreciate the significant danger currently posed at this intersection and are willing to find a long-term solution to make it safer, such as installing traffic signals.

Sch.4 Part 4 s.6 PI

 gradual increase in the amount of traffic entering Yeppoon Road from both Artillery Road and Dairy Inn Road at the intersection in question during recent years

Sch.4 Part 4 s.6 PI

Sch.4 Part 4 s.6 PI

Sch.4 Part 4 s.6 PI

I genuinely fear that fatalities will occur if no attempt at making this intersection safer by taking such measures as installing traffic lights.

Thank you for taking the time to read this suggestion, and I sincerely hope you take my concerns on board.

Requested Outcome:  
Complaint Reviewed:

Requested by: QTMR Agent  
Date requested: 24/03/2019 01:13:50 PM

Released under RTI - DTMR

Google Maps



Map data ©2019 Google 100 m

Released under RTI - DTMR



## Brittany L McGuirk

---

**From:** [personal information]@ministerial.qld.gov.au>  
**Sent:** Monday, 25 March 2019 10:15 AM  
**To:** TMR DLO  
**Subject:** EE7854 - Fwd: Constituent's enquiry Yeppoon Road accident

**Categories:** Brittany

EE please - turnaround by Wednesday COB please.

[personal information]

Caucus Liaison Officer  
Office of Minister for Transport and Main Roads

Begin forwarded message:

**From:** Keppel Electorate Office <[Keppel@parliament.qld.gov.au](mailto:Keppel@parliament.qld.gov.au)>  
**Date:** 25 March 2019 at 10:12:32 am AEST  
**To:** [personal information]@ministerial.qld.gov.au>  
**Cc:** Keppel Electorate Office <[Keppel@parliament.qld.gov.au](mailto:Keppel@parliament.qld.gov.au)>  
**Subject:** Constituent's enquiry Yeppoon Road accident

Hi [personal information]

There was a fatal accident on the Rockhampton-Yeppoon Road at Artillery Road intersection on the weekend and subsequently a lot of Facebook and media comment.

Could you please source some advice for our constituent [PI] who asks:

*In the past 12 months there has been a pedestrian killed at this intersection and another serious accident involving a cattle truck at the Old Byfield Road, Yeppoon Road intersection, some 500metres up the road. These accidents would have been prevented if there was some traffic slowing, round about or traffic lights. The Yeppoon Road is becoming increasingly busy which makes attempting to cross very difficult. This road needs attention immediately.*

Kind Regards  
John Schalch

Electorate Officer

M: [NR]



# BRITTANY LAUGA MP

B Urb Dev (Urb&Reg Plan)(Hons), C.dec

**MEMBER FOR KEPPEL  
AND ASSISTANT MINISTER  
FOR EDUCATION**



**ELECTORATE OFFICE**  
Shop 3, Evia Building  
Normanby Street  
Yeppoon QLD 4703

**POSTAL ADDRESS**  
Post Office Box 1474  
Yeppoon QLD 4703

**CONTACT**  
Phone: (07) 4913 1500  
Fax: (07) 4913 1509  
Email: keppel@parliament.qld.gov.au

Regards

PI

Consider the environment before you print this email.

NOTICE - This e-mail and any attachments are confidential and only for the use of the addressee.

If you have received this e-mail in error, you are strictly prohibited from using, forwarding, printing, copying or dealing in anyway whatsoever with it, and are requested to reply immediately by e-mail to the sender or by telephone to the Parliamentary Service on +61 7 3553 6000.

Any views expressed in this e-mail are the author's, except where the e-mail makes it clear otherwise. The unauthorised publication of an e-mail and any attachments generated for the official functions of the Parliamentary Service, the Legislative Assembly, its Committees or Members may constitute a contempt of the Queensland Parliament. If the information contained in this e-mail and any attachments becomes the subject of any request under Right to information legislation, the author or the Parliamentary Service should be notified.

It is the addressee's responsibility to scan this message for viruses. The Parliamentary Service does not warrant that the information is free from any virus, defect or error.

This email, together with any attachments, is intended for the named recipient(s) only; and may contain privileged and confidential information. If received in error, you are asked to inform the sender as quickly as possible and delete this email and any copies of this from your computer system network.

If not an intended recipient of this email, you must not copy, distribute or take any action(s) that relies on it; any form of disclosure, modification, distribution and /or publication of this email is also prohibited.

Unless stated otherwise, this email represents only the views of the sender and not the views of the Queensland Government.

Please consider the environment before printing this email.

## Advice for Minister for Transport and Main Roads

### Subject: Rockhampton-Yeppoon Road and Artillery Road intersection

---

Document ID: EE7854

- Rockhampton-Yeppoon Road is one of two state-controlled roads connecting Rockhampton and the Capricorn Coast in Central Queensland.
- The Department of Transport and Main Roads (TMR) traffic data records 11,558 vehicles per day using Rockhampton-Yeppoon Road in the area of Artillery Road (Dairy Inn Road) intersection. Traffic volumes on the road have increased by 1.2 per cent annually, over the past five years.
- Artillery Road provides an alternate connection to the Bruce Highway, north of Rockhampton and is a shortcut for motorists travelling from the Capricorn Coast to the north of Rockhampton. Dairy Inn Road connects Rockhampton-Yeppoon Road to the Cawarral community, between Rockhampton and Yeppoon.
- TMR has recorded six crashes within a 500-metre radius of the Artillery Road intersection between 2014 and 2019. Prior to 24 March 2019, there were no fatal crashes recorded at the intersection over a five-year period.
- On 31 January 2019, there was a serious crash at the Rockhampton-Yeppoon Road and Old Byfield Road intersection, approximately 500 metres east, involving two trucks. This crash resulted from a vehicle entering from the side road, failing to give-way at the intersection, and colliding with a heavy vehicle on Rockhampton-Yeppoon Road.
- On 21 February 2019, there was a crash at the Rockhampton-Yeppoon Road and Old Byfield Road intersection, approximately 500 metres east, involving two cars. This crash resulted from a vehicle attempting to avoid a cow on the road.
- Queensland Police Service is continuing its investigation into the fatal crash at the Rockhampton-Yeppoon Road and Artillery Road intersection on 24 March 2019.
- TMR's Road Safety team will also investigate the circumstance to identify if the road, or road conditions contributed to the crash.

Action Officer/Approved by:	Endorsed by GM	Endorsed by DDG	Endorsed by DG
Dave Grosse	Les Dunn	Amanda Yeates	Neil Scales
Regional Director (Central Queensland)	General Manager (Program Delivery and Operations)	Deputy Director-General (Infrastructure Management and Delivery)	Director-General
Tel: 4931 1501	Tel: 3066 5498	Tel: 3066 7118	Tel: 3066 7316
Date: 25 March 2019	Date: progressed under RD approval	Date:	Date:

- The intersection is currently constructed with eastbound and westbound protected turn lanes for Rockhampton-Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection flag lighting located on all intersection legs and warning signage is in place on all intersection approaches.
- TMR currently has no projects identified in the 2018/19 - 2021/22 Queensland Transport and Roads Investment Program, to upgrade the Rockhampton-Yeppoon Road and Artillery Road intersection.
- In 2017, TMR completed strategic planning to determine an indicative cost and staging options to four lane the remaining 20-kilometre (approximately) section of Rockhampton-Yeppoon Road, between Iron Pot and Yeppoon (Tanby Road intersection).
- The planning identified the potential cost to four lane, between Neils Road and Tanby Road intersection, required investment of \$95 million. Further investment in the order of \$160-\$200 million would be required to four lane between Neils Road and Ironpot.
- In January 2019, the Australian Labor Party made a \$47.5 million election commitment to contribute to the four laning of Rockhampton-Yeppoon Road, between Neils Road and Tanby Road intersection. The election commitment did not extend to the Artillery Road section of Rockhampton-Yeppoon Road.

Not Relevant

Released under RTI - DTMR

Released under RTI - DTMR

Not Relevant

Sch.3(6)(c)(i) Infringe the privileges of Parliament

Not Relevant

Released under RTI - DTMR

Sch.3(6)(c)(i) Infringe the privileges of Parliament



Sch.3(6)(c)(i) Infringe the privileges of Parliament

<b>Division:</b>	<b>Infrastructure Management and Delivery – FDC Central Queensland Region</b>		
	<b>Executive Director:</b>	<b>General Manager:</b>	<b>Deputy Director-General:</b>
<b>Name:</b>	Dave Grosse	Les Dunn	Amanda Yeates
<b>Telephone:</b>	(w) 49311501 NR	(w) 3066 5498 NR	(w) 3066 7118 NR
<b>Date:</b>	32 March 2020		

# Advice for Minister for Transport and Main Roads

## Subject: Rockhampton-Yeppoon Road and Artillery Road intersection

---

Document ID: DLO3040

- Rockhampton-Yeppoon Road is one of two state-controlled roads connecting Rockhampton and the Capricorn Coast in Central Queensland.
- The Department of Transport and Main Roads (TMR) traffic data records 11,558 vehicles per day using Rockhampton-Yeppoon Road in the area of Artillery Road (Dairy Inn Road) intersection. Traffic volumes on the road have increased by 1.2 per cent annually, over the past five years.
- Artillery Road provides an alternate connection to the Bruce Highway, north of Rockhampton and is a shortcut for motorists travelling from the Capricorn Coast to the north of Rockhampton. Dairy Inn Road connects Rockhampton-Yeppoon Road to the Cawarral community, between Rockhampton and Yeppoon.
- TMR has recorded six crashes within a 500-metre radius of the Artillery Road intersection between 2014 and 2019. Prior to 24 March 2019, there were no fatal crashes recorded at the intersection over a five-year period.
- On 31 January 2019, there was a serious crash at the Rockhampton-Yeppoon Road and Old Byfield Road intersection, approximately 500 metres east, involving two trucks. This crash resulted from a vehicle entering from the side road, failing to give-way at the intersection, and colliding with a heavy vehicle on Rockhampton-Yeppoon Road.
- On 21 February 2019, there was a crash at the Rockhampton-Yeppoon Road and Old Byfield Road intersection, approximately 500 metres east, involving two cars. This crash resulted from a vehicle attempting to avoid a cow on the road.
- Queensland Police Service is continuing its investigation into the fatal crash at the Rockhampton-Yeppoon Road and Artillery Road intersection on 24 March 2019.
- TMR's Road Safety team will also investigate the circumstance to identify if the road, or road conditions contributed to the crash.

Action Officer/Approved by:	Endorsed by GM	Endorsed by DDG	Endorsed by DG
Dave Grosse	Les Dunn	Amanda Yeates	Neil Scales
Regional Director (Central Queensland)	General Manager (Program Delivery and Operations)	Deputy Director-General (Infrastructure Management and Delivery)	Director-General
Tel: 4931 1501	Tel: 3066	Tel: 3066 7118	Tel: 3066 7316
Date: 26 March 2019	Date: Mar	Date:	Date:

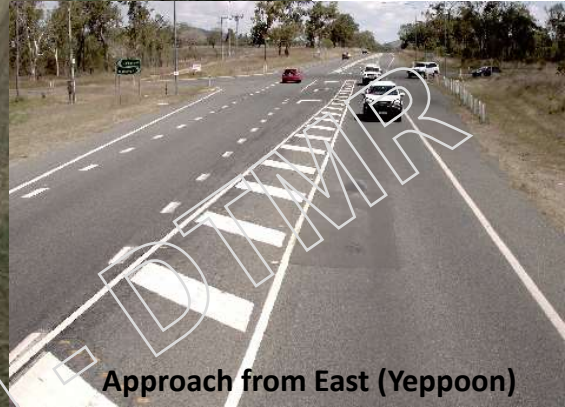
NR

DLO3040

- The intersection is currently constructed with eastbound and westbound protected turn lanes for Rockhampton-Yeppoon Road traffic. Give-way signage is in place for vehicles entering the intersection from Artillery Road and Dairy Inn Road. There is also intersection flag lighting located on all intersection legs and warning signage is in place on all intersection approaches (**Attachment 1**).
- TMR currently has no projects identified in the 2018/19 - 2021/22 Queensland Transport and Roads Investment Program, to upgrade the Rockhampton-Yeppoon Road and Artillery Road intersection.
- In 2017, TMR completed strategic planning to determine an indicative cost and staging options to four lane the remaining 20-kilometre (approximately) section of Rockhampton-Yeppoon Road, between Iron Pot and Yeppoon (Tanby Road intersection). *One word*
- The planning identified the potential cost to four lane, between Neils Road and Tanby Road intersection, required investment of \$95 million. Further investment in the order of \$160-\$200 million would be required to four lane between Neils Road and Ironpot.
- The combined Business Case for duplication of Rockhampton-Yeppoon Road Stage 1 (Neils Road to Tanby Road) and Stage 2 (Mulara Road to Neils Road) identified in **Attachment 2**, has been developed and is under review for imminent finalisation.
- The contract for development of the Strategic Assessment and Service Requirement (SASR) and Options Analysis (OA) for Stage 3 (Iron Pot to Mulara) of the Rockhampton-Yeppoon Road duplication planning was awarded in March 2019. SASR is due for completion in June 2019, and the OA in early 2020.
- In January 2019, the Australian Labor Party made a \$47.5 million election commitment to contribute to the four laning of Rockhampton-Yeppoon Road, between Neils Road and Tanby Road intersection, identified in **Attachment 2** as Stage 1. The election commitment did not extend to the Artillery Road section of Rockhampton-Yeppoon Road, which is within the extents of Stage 3.



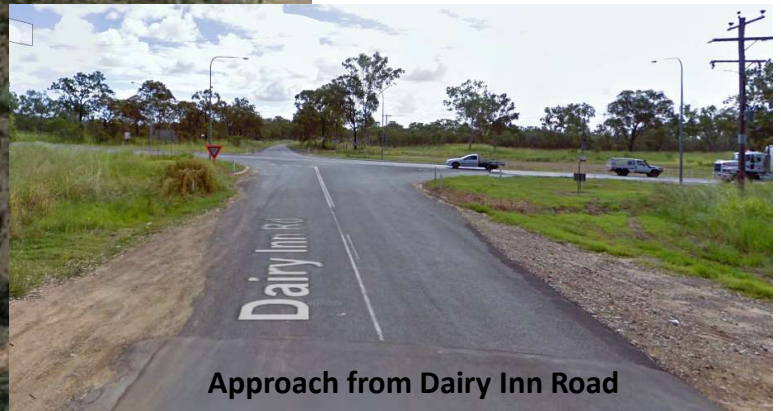
Approach from Artillery Road



Approach from East (Yeppoon)





Approach from West (Rockhampton)



Approach from Dairy Inn Road



**Rockhampton-Yeppoon Road (31km)**

-  Bruce Highway to Iron Pot four lanes (10km)
-  Stage 1: Neils Road to Tanby Road (6km)
-  Stage 2: Mulara Road to Neils Road (4km)
-  Stage 3: Iron Pot to Mulara Road (11km)

 Fatal Crash Location – 24 March 2019

Not Relevant

Released under RTI - DTMR

Released under RTI - DTMR

Not Relevant

Released under RTI - DTMR



Not Relevant

Sch.3(6)(c)(i) Infringe the privileges of Parliament

<b>Division:</b>	Infrastructure Management and Delivery – PDO Central Queensland Region		
	<b>Executive Director:</b>	<b>General Manager:</b>	<b>Deputy Director-General:</b>
<b>Name:</b>	Dave Grosse	Les Dunn	Amanda Yeates
<b>Telephone:</b>	(w) 49311501 NR	(w) 3066 5498 NR	(w) 3066 7118 NR
<b>Date:</b>	30 March 2020	9 March 2020	9 March 2020

## Advice for Minister for Transport and Main Roads

### Subject: KEPPEL EO – Fatality at Yeppoon Road and Artillery Road intersection

Document ID: EE7875

- The Department of Transport and Main Roads (TMR) has recorded six crashes within a 500 metre radius of the Artillery Road intersection between 2014 and 2019. Prior to 24 March 2019, there were no fatal crashes recorded at the intersection over a five-year period.
- To support the identification of potentially hazardous locations on the state-controlled road network, TMR investigate fatal crashes to an appropriate level.
- TMR's Road Safety Team will investigate the circumstance of the fatal crash on 24 March 2019 alongside Queensland Police Service to identify if the road, or road conditions, contributed to the crash.
- TMR investigations are not a duplication of the Queensland Police Forensic Crash Unit investigation into road crashes. The purpose is to collect road-based information at and on the approaches to fatal crash sites to determine if there are any remedial measures or road improvements that may be required to prevent future occurrences at those locations.
- The TMR Fatal Crash Investigation will not assess potential contributors to crashes that are not related to road infrastructure, such as driver behaviour or vehicle defects.
- The initial TMR Fatal Crash Investigation is expected to be completed and recommendations identified by 5 April 2019. If required, a more detailed report will then be prepared identifying immediate interim remedial measures and permanent road safety upgrades at the crash location.

Action Officer/Approved by:	Endorsed by GM	Endorsed by DDG	Endorsed by DG
Dave Grosse	Les Dunn	Amanda Yeates	Neil Scales
Regional Director (Central Queensland)	General Manager (Program Delivery) [Redacted]	Deputy Director-General (Infrastructure Management and Delivery)	Director-General
Tel: 4931 1501	Tel: 3 [Redacted]	Tel: 3066 7118	Tel: 3066 7316
Date: 27 March 2019	Date: 28/3	Date:	Date:

Released under RTI - DTMR

Not Relevant

Sch 3(6)(c)(i) Infringe the privileges of Parliament

<b>Division:</b>	Infrastructure Management and Delivery – PDO Central Queensland Region		
	<b>Executive Director:</b>	<b>General Manager:</b>	<b>Deputy Director-General:</b>
<b>Name:</b>	Dave Gosce	Les Dunn	Amanda Yeates
<b>Telephone:</b>	(w) 49311501 NR	(w) 3066 5498 NR	(w) 3066 7118 NR
<b>Date:</b>	26 June 27 July 2020	2 July 2020	2 July 2020

Released under RTI - DTMR

## Email in 11/10/2019 4:07 PM (Australia/Queensland)

*Event Type*

Email in

*Date and Time*

11/10/2019 4:07 PM (Australia/Queensland)

*Location*

196

*Summary*

Rockhampton Yeppoon Road Safety at Dairy Inn Road

*Interaction Details*

Hi Peter

Could you please provide a response to the constituent's enquiry below?

Thanks

Hi Brittany when is the Cawarral turn off going to be fixed or do we wait for someone else to die

Regards PI

Kind Regards

PI

Good afternoon Brittany,

Thank you for your email regarding Rockhampton–Yeppoon Road, and in particular comments from a local constituent about the Dairy Inn Road (Cawarral) intersection.

With three recorded crashes on Rockhampton–Yeppoon Road in the area of Iron Pot and Mulara this year, the Department of Transport and Main Road (TMR) has commenced planning to identify upgrade options on the 20km (approximately) of two-lane road east of Iron Pot, which includes the Artillery Road/Dairy Inn Road intersection.

Further to this current planning, in the 2019 Federal Budget, the Australian Government committed \$64 million towards upgrading Rockhampton–Yeppoon Road under the federal Roads of Significant Importance (ROSI) initiative, based on 80:20 funding arrangements. The Queensland Transport and Roads Investment Program 2019–20 to 2022–23 also commits \$16 million of Queensland Government matching funding towards the Rockhampton–Yeppoon Road ROSI funding, totalling \$80 million of upgrades to be delivered on Rockhampton–Yeppoon Road over the next five years.

With this funding now committed, TMR officers are working with our Australian Government counterparts to determine priorities for upgrading Rockhampton–Yeppoon Road to improve road safety and capacity of this key Central Queensland regional road. The Dairy Inn and Artillery Road intersection is a central location for upgrade as part of this \$80 million road safety and capacity upgrade.

*Team*

*Response/Actions*

Ahead of this major project being delivered, TMR is also investigating interim measures including additional warning signs that could be installed to alert drivers to the location of this intersection.

I will provide further information once details are confirmed for the installation of these additional warning signs

Kind Regards

Peter Trim PSM  
District Director (Fitzroy District) | Central Queensland Region

.Infrastructure type:Roads, Issues:DESIGN ISSUES AND IMPACTS:Safety(0), .Reporting:General Enquiry

24/10/2019 4:08 PM (Australia/Queensland)

AUSTRALIA

PDO\_Fitz\_Fitzroy BAU

175: Kris Biddle

Brittany Louise Lauga

*Issues*

*End Date*

*Address*

*Projects*

*Users*

*Properties*

*Stakeholders*

Released under RTI - DTMR

## Advice for Minister for Transport and Main Roads

### Subject: TMR investigation of Rockhampton-Yeppoon Road Fatality

Document ID: EE7875

- The Department of Transport and Main Roads (TMR) has recorded six crashes within a 500 metre radius of the Artillery Road intersection between 2014 and 2019. Prior to 24 March 2019, there were no fatal crashes recorded at the intersection over a five-year period.
- To support the identification of potentially hazardous locations on the state-controlled road network, TMR investigate fatal crashes to an appropriate level.
- TMR's Road Safety Team will investigate the circumstance of the fatal crash on 24 March 2019 alongside Queensland Police Service to identify if the road, or road conditions, contributed to the crash.
- TMR investigations are not a duplication of the Queensland Police Forensic Crash Unit investigation into road crashes. The purpose is to collect road-based information at and on the approaches to fatal crash sites to determine if there are any remedial measures or road improvements that may be required to prevent future occurrences at those locations.
- The TMR Fatal Crash Investigation will not assess potential contributors to crashes that are not related to road infrastructure, such as driver behaviour or vehicle defects.
- The initial TMR Fatal Crash Investigation is expected to be completed and recommendations identified by 5 April 2019. If required, a more detailed report will then be prepared identifying immediate interim remedial measures and permanent road safety upgrades at the crash location.

Action Officer/Approved by:	Endorsed by GM	Endorsed by DDG	Endorsed by DG
Dave Grosse	Les Dunn	Amanda Yeates	Neil Scales
Regional Director (Central Queensland)	General Manager (Program Delivery and Operations)	Deputy Director-General (Infrastructure Management and Delivery)	Director-General
Tel: 4931 1501	Tel: (07) 3066 5498	Tel: (07) 3066 7118	Tel: (07) 3066 7316
Date: 27/3/19	Date:	Date:	Date:



## Karen Stevens

---

**From:** FitzroyCommunication  
**Sent:** Friday, 29 March 2019 8:57 AM  
**To:** Ishara C Jayasinghe; Laxman A Gopali  
**Cc:** FitzroyCommunication; Darren L Richardson; Peter T Trim  
**Subject:** FW: Department of Transport and Main Roads: Fatal Crash Report for Rockhampton-Yeppoon Road 24/3/19

Hi Team,

Have just now received an email from QPS providing detail of the crash on Sunday morning.

Below is the advice for your reference.

Kind regards,

**Kris Biddle**

Communications Advisor | Central Queensland Region

**Program Delivery and Operations Branch** | Infrastructure Management and Deliver Division | Department of Transport and Main Roads

---

Level 1 | 31 Knight Street | NORTH ROCKHAMPTON QLD 4701

PO Box 5096 | NORTH ROCKHAMPTON QLD 4701

(07) 4931 1546 | M: [redacted] NR

[kristopher.m.biddle@tmr.qld.gov.au](mailto:kristopher.m.biddle@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** [redacted] <[redacted]@police.qld.gov.au>  
**Sent:** Friday, 29 March 2019 8:33 AM  
**To:** FitzroyCommunication <FitzroyCommunication@tmr.qld.gov.au>  
**Cc:** Colin J Edmonston <Colin.J.Edmonston@tmr.qld.gov.au>  
**Subject:** RE: Department of Transport and Main Roads: Fatal Crash Report for Rockhampton-Yeppoon Road 24/3/19

Morning Kris,

This is what we know so far. The occupants of the blue Toyota Corolla were travelling from Emu Park to the Capricorn Caves for the day and was travelling on Dairy Inn Road towards Yeppoon Road. Information from [redacted] personal information was that the [redacted] personal information was in the front passenger seat with the GPS open on the phone and as the driver slowed and indicated to turn left onto Yeppoon Road, [redacted] Sch.4 Part 4 s.6 PI

The driver appears to have then driven straight across the intersection and into the path of the grey Toyota Corolla which was travelling towards Yeppoon. There were no pre-impact brake marks from either vehicle and the grey Toyota collided with the back left door area of the blue Toyota. The collision occurred in the eastbound lane of Yeppoon Road.

A [redacted] personal information as seated [redacted] personal information on the left rear side and the [redacted] deceased girl was seated in the middle rear seat. The [redacted] is still in a critical condition in the Qld Childrens Hospital.

We've mapped and photographed the scene and conducted friction testing across both directions (you'll see the extra skid marks on the road when you go out there).

At this stage, we haven't interviewed the drivers of either vehicle. We have seized both vehicles, awaiting mechanical inspections and downloads.

That is the basic story and obviously the layout of the intersection, give way signs, speed limits etc will be under scrutiny. If there is anything further, let me know.

Regards,

personal information

Sergeant 7633 | Officer in Charge  
Rockhampton Forensic Crash Unit  
Road Policing Command | Queensland Police Service  
✉ PO Box 1161, Rockhampton Qld 4700  
☎ (07) 49323498 | 📠 (07) 49323465  
personal information @police.qld.gov.au

---

**From:** FitzroyCommunication <[FitzroyCommunication@tmr.qld.gov.au](mailto:FitzroyCommunication@tmr.qld.gov.au)>  
**Sent:** Thursday, 28 March 2019 9:35 AM  
**To:** personal information @police.qld.gov.au  
**Cc:** FitzroyCommunication <[FitzroyCommunication@tmr.qld.gov.au](mailto:FitzroyCommunication@tmr.qld.gov.au)>; Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>; Peter T Trim <[Peter.T.Trim@tmr.qld.gov.au](mailto:Peter.T.Trim@tmr.qld.gov.au)>  
**Subject:** Department of Transport and Main Roads: Fatal Crash Report for Rockhampton-Yeppoon Road 24/3/19

Good morning personal information  
nf  
ormation

I have been provided your contact details by Peter Trim, Transport and Main Roads District Director (Fitzroy).

Following the Rockhampton-Yeppoon Road fatal crash on Sunday morning (24/3/19), Transport and Main Roads will undertake a Fatal Crash Investigation to identify if the road, or road conditions, contributed to the crash and recommend if road improvements may be required to prevent future occurrences at this location.

To assist our Road Safety Team in conducting the site investigation, can you please provide the preliminary crash details report for the crash at Rockhampton-Yeppoon Road and Artillery Road intersection on 24/3/19. I have attached the report received by Transport and Main Roads on Monday for this crash, however there is no detail in the description to assist the investigating team while they are on-site.

Any information you are able to provide will only be used for the purpose of TMR undertaking the Fatal Crash Investigation.

Thank you for your assistance in this matter.

Kind regards,

**Kris Biddle**  
Communications Advisor | Central Queensland Region  
**Program Delivery and Operations Branch** | Infrastructure Management and Deliver Division | Department of Transport and Main Roads

---

Level 1 | 31 Knight Street | NORTH ROCKHAMPTON QLD 4701  
PO Box 5096 | NORTH ROCKHAMPTON QLD 4701  
(07) 4931 1546 | M personal information NR  
[kristopher.m.biddle@tmr.qld.gov.au](mailto:kristopher.m.biddle@tmr.qld.gov.au)  
[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

\*\*\*\*\*

WARNING: This email (including any attachments) may contain legally privileged, confidential or private information and may be protected by copyright. You may only use it if you are the person(s) it was intended to be sent to and if you use it in an authorised way. No one is allowed to use, review, alter, transmit, disclose, distribute, print or copy this email without appropriate authority.

If this email was not intended for you and was sent to you by mistake, please telephone or email me immediately, destroy any hardcopies of this email and delete it and any copies of it from your computer system. Any right which the sender may have under copyright law, and any legal privilege and confidentiality attached to this email is not waived or destroyed by that mistake.

It is your responsibility to ensure that this email does not contain and is not affected by computer viruses, defects or interference by third parties or replication problems (including incompatibility with your computer system).

Opinions contained in this email do not necessarily reflect the opinions of the Department of Transport and Main Roads, or endorsed organisations utilising the same infrastructure.

\*\*\*\*\*

\*\*\*\*\*

CONFIDENTIALITY: The information contained in this electronic mail message and any electronic files attached to it may be confidential information, and may also be the subject of legal professional privilege and/or public interest immunity. If you are not the intended recipient you are required to delete it. Any use, disclosure or copying of this message and any attachments is unauthorised. If you have received this electronic message in error, please inform the sender or contact [1300ITPSBA@psba.qld.gov.au](mailto:1300ITPSBA@psba.qld.gov.au). This footnote also confirms that this email message has been checked for the presence of computer viruses.

\*\*\*\*\*

Released under RTI - DTMR

## Karen Stevens

---

**From:** Sch.4 Part 4 s.6 PI <mcmengineers.com>  
**Sent:** Friday, 24 January 2020 1:59 PM  
**To:** Laxman A Gopali; Fitzroy District Road Safety  
**Cc:** Vasudevan Z Ramanathan <Sch.4 Part 4 s.6 PI>  
**Subject:** 0251819/14 - 196 Yeppoon Road / Dairy Inn Road / Artillery Road intersection - IFC Drawings - Rev A  
**Attachments:** 394926-Int Yeppoon Rd Dairy Inn Rd Artillery Rd-IFC-Rev A.pdf; Schedule-16B Speed Zone - East Emerald - 17.01.20.xls; MRS14\_1.docx; MRTS14\_1.docx

Hi Laxman

Please see attached 'Issued For Construction' drawings, schedule and road furniture annexures for Yeppoon Road / Dairy Inn Road / Artillery Road intersection.

Thanks.

Kind Regards,

Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: PI <mcmengineers.com>

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**mcmurtrie**  
CONSULTING ENGINEERS





## Karen Stevens

---

**From:** Katharine L Ferguson  
**Sent:** Thursday, 16 July 2020 11:33 AM  
**To:** Dave J Grosse  
**Cc:** Peter T Trim; Laxman A Gopali  
**Subject:** RE: Rockhampton-Yeppoon Road (196) - Artillery/Dairy Inn

Hi Dave,

The estimate for the supply of vehicle activated signs is 2 ½ - 3 months.

Garry and I spoke with two TMR approved suppliers:

**J-1 LED signs** Standard signs: 7 weeks manufacture & delivery  
Special signs: 12 weeks manufacture & delivery

**Pioneerz** Standard signs: 7-9 weeks manufacture & delivery  
Special signs: 8-10 weeks manufacture & delivery

J-1 LED signs suggested a staggered installation – install standard signs quickly and replace them once the special 'X' intersection warning signs are ready.

Garry also spoke with a Livingstone Shire officer who suggested we could link in with the radars for the existing vehicle activated "Give Way" signs on the side roads to trigger the signs on the R'ton-Yeppoon Rd.

Kind regards,

**Kath Ferguson**

Operations Officer (Road Safety) | Central Queensland Region

**Program Delivery & Operations Branch** | Infrastructure Management and Delivery Division | Department of Transport and Main Roads

-

---

Floor 1 | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

(07) 49311527

[www.katharine.l.ferguson@tmr.qld.gov.au](mailto:katharine.l.ferguson@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Dave J Grosse <Dave.J.Grosse@tmr.qld.gov.au>  
**Sent:** Thursday, 16 July 2020 10:26 AM  
**To:** Peter T Trim <Peter.T.Trim@tmr.qld.gov.au>  
**Cc:** Laxman A Gopali <Laxman.A.Gopali@tmr.qld.gov.au>; Katharine L Ferguson <katharine.l.ferguson@tmr.qld.gov.au>  
**Subject:** FW: Rockhampton-Yeppoon Road (196) - Artillery/Dairy Inn

Hi Peter,

Gives us an option for discussion with Mins office this afternoon.

Kind regards

**Dave Grosse**

Regional Director | Central Queensland Region

**Program Delivery and Operations Branch** | Infrastructure Management and Delivery Division | Department of Transport and Main Roads

---

Floor 1 | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

(07) 4931 1501 | M NR

[dave.j.grosse@tmr.qld.gov.au](mailto:dave.j.grosse@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Andrew Burbridge <[Andrew.Z.Burbridge@tmr.qld.gov.au](mailto:Andrew.Z.Burbridge@tmr.qld.gov.au)>

**Sent:** Thursday, 16 July 2020 10:20 AM

**To:** Dave J Grosse <[Dave.J.Grosse@tmr.qld.gov.au](mailto:Dave.J.Grosse@tmr.qld.gov.au)>

**Cc:** Michael A Gillies <[Michael.A.Gillies@tmr.qld.gov.au](mailto:Michael.A.Gillies@tmr.qld.gov.au)>; CQ Program <[CQ\\_Program@tmr.qld.gov.au](mailto:CQ_Program@tmr.qld.gov.au)>; Brett Z Franklin <[Brett.Z.Franklin@tmr.qld.gov.au](mailto:Brett.Z.Franklin@tmr.qld.gov.au)>

**Subject:** Rockhampton-Yeppoon Road (196) - Artillery/Dairy Inn

Dear Dave,

As discussed, Sections 7.2.6 and 12.4 of MUTCD Part 4 support reduced speed limit on the approaches to intersections with speed limits of 80 km/h or greater, which have either a high crash history or high potential of crash risk. This would apply to the through legs of Rockhampton-Yeppoon Road (196) - Artillery/Dairy Inn, and those sections of MUTCD Part 4 together prescribe the standard static sign circumstances and arrangements. This is probably a minimum response at this location.

Further, in the absence of staggering or other more dramatic changes to the configuration, complementary short-term countermeasures that might also be deployed include:

- Provision of side-activated "X" intersection warning signs on major road approaches, perhaps with "REDUCE SPEED supplementaries". Section 9.3 of Technical Note TN160 is the place to start. This may not stop side road drivers from entering the intersection when it is not safe to do so, but it may be enough to make a though driver more mindful to potential for a collision.
- Development of perception of a stagger across the side roads by, for example, raised median islands and hazard boards (see \*.jpg image).
- Ensure maintenance regime is adequate (or even enhanced) to keep sight distances clear (accepting that infrastructure hardware can't easily be shifted). The attached \*.png image is from Google Earth Street view from approximately driver position if propped at the yield line.

These treatments are relatively low cost and (maintenance aside) could possibly be delivered by accessing Fatal Crash Remediation funding from the TRSP.

You might also investigate whether it is possible to modify the overtaking lane taper in the southbound direction so that it is well clear of the intersection. Again, mindfulness of the through driver is my thinking here.

I hope this all helps. Best regards,

**Andrew Burbridge**

Principal Engineer (Safer Roads) | Safer Roads Infrastructure

**Engineering and Technology** | Infrastructure Management and Delivery | Department of Transport and Main Roads

---

Floor 11 | 61 Mary Street | Brisbane Qld 4000

GPO Box 2595 | Brisbane Qld 4001

(07) 3066 2424

[andrew.z.burbridge@tmr.qld.gov.au](mailto:andrew.z.burbridge@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)





**Crossing at traffic lights? Don't touch the button, elbow bump it instead!**

Released under RTI - DTMR

## Karen Stevens

---

**From:** [Sch.4 Part 4 s.6 PI]@mcmengineers.com>  
**Sent:** Monday, 2 December 2019 3:07 PM  
**To:** Laxman A Gopali; Fitzroy District Road Safety  
**Cc:** [Sch.4 Part 4 s.6 PI]  
**Subject:** 0251819/14 - Rockhampton-Yeppoon Road - Approach Intersection Speed  
**Attachments:** Yeppoon Rd Dairy Inn Rd Artillery Rd.pdf

Hi Laxman,

Please see our comments in Blue. Also we make the following notes and recommendations;

- We understand that the DTMR's direction is to leave the "Slow Down + Give Way Sign Ahead" digital sign at approx. 120m from the intersection on Dairy Inn Road and Artillery Road. The current location of the digital sign from the intersection would enable a motorists to react appropriately when travelled less than 75km/h (85<sup>th</sup> percentile speed). Therefore this digital sign should be recalibrated accordingly to suit the proposed 60km/h speed environment.
- Install "Give Way" message on pavements in Dairy Inn Road and Artillery Road to convey regulatory "Giveway" message to drivers as per attached sketch.
- Consider installing rumble strips for a 60km/h approach that will encourage drivers to reduce speed due to discomfort associated with travelling over the strips at high speed.

We seek your direction / advise on the above to finalise the attached sketch.

Kind Regards,

[Sch.4 Part 4 s.6 PI]

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: [PI]@mcmengineers.com

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**mcmurtrie**  
CONSULTING ENGINEERS

---

**From:** Laxman A Gopali [<mailto:Laxman.A.Gopali@tmr.qld.gov.au>]

**Sent:** 26 November 2019 3:53 PM

**To:** [Sch.4 Part 4 s.6 PI]@mcmengineers.com> [Sch.4 Part 4 s.6 PI]@mcmengineers.com>

**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road

Hi [PI]

As discussed can the additional treatment as below be considered :

- Can we introduce reducing speed on Dairy inn and Artillery road to 60 km/hr. The approach speed towards the intersection can be reduced down to 60km/h. Refer Clause 7.2.6 Speed limits to approaches to rural intersection, Part 4: Speed controls, Queensland MUTCD, November 2018. Refer attached sketch.

### 7.2.6 Speed limits on approaches to rural intersections

Speed limits may be reduced on the approaches to intersections with speed limits of 80 km/h or greater, which have either a high crash history or high potential of crash risk (as assessed using tools such as *AusRAP* or road safety audit) as follows:

- (a) controlled approach – speed limit may be reduced to 60 km/h
- (b) uncontrolled approach – the speed limit may be reduced by up to 30 km/h.

Lengths of speed zones with reduced speed limit and signage requirements shall be as described in Section 12.4.

- Introducing of C size Cross road signage in Yeppoon road. We can introduce 'C' size Crossroad signs in Yeppoon Road on both approaches to the intersection. Refer attached sketch.
- Can we check where is the 100km/hr signage are installed in Yeppoon road after cross road intersection so that when customer do the right or left turn movement from Dairy inn and Artillery road they have visibility of the 100km/hr signage in the Yeppoon road so that they can act accordingly. Refer attached sketch.

Any query would be much appreciated.

Kind Regards

**Laxman Gopali**

A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: [redacted] NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** [redacted] Sch.4 Part 4 s.6 PI <[redacted]@mcmengineers.com>  
**Sent:** Tuesday, 26 November 2019 3:16 PM  
**To:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>  
**Cc:** [redacted] Sch.4 Part 4 s.6 PI <[redacted]@mcmengineers.com>  
**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road

FYI.

[redacted] Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: [redacted] PI <[redacted]@mcmengineers.com>

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**mcmurtrie**  
CONSULTING ENGINEERS

**From:** [redacted] Sch 4 Part 4 s.6 PI  
**Sent:** 22 November 2019 5:14 PM  
**To:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>  
**Cc:** [redacted] Sch.4 Part 4 s.6 PI <[redacted]@mcmengineers.com>  
**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road

Laxman,

Please see below email. We can discuss this further on Monday meeting.

Kind Regards,

Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: [redacted] PI @mcmengineers.com

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



---

**From:** [redacted] Sch.4 Part 4 s.6 PI

**Sent:** 22 November 2019 11:50 AM

**To:** [redacted] Sch.4 Part 4 s.6 PI @mcmengineers.com>

**Cc:** [redacted] Sch.4 Part 4 s.6 PI @mcmengineers.com>

**Subject:** RE: 0251819/14 - Rockhampton-Yeppoon Road

Hi [redacted] PI

Having further reviewed the proposed signage layout for Yeppoon Rd & Dairy inn Rd intersection I have concerns regarding TMR's requested changes.

Our recommendations included the implementation of rumble strips on both Artillery Road and Dairy Inn Road to assist in mitigating the speed management issues at the approaches to Yeppoon Road.

Further, our recommendations required the relocation of the "slow down + give way ahead" digital sign 200m away from the Rockhampton-Yeppoon Rd intersection on both Artillery and Dairy Inn Roads to satisfy the longitudinal placement of the sign in the 100km/hr speed environment. By TMR electing not to implement the rumble strips the proposal is relying on the TC1308\_1 Reduce Speed Sign and the TC9941-B Give way sign to mitigate the safety issues as the digital slow down sign is not in a location to afford motorists enough time to slow down if they do miss the first reduce speed sign.

If MCE are required to certify this signage plan it would be on the basis of as a minimum relocating the digital sign to the appropriate location (in the attached sketch), however, if budget permits we strongly suggest the rumble strips are essential in providing further cues for motorists to slow down.

Regards,

[redacted] Sch.4 Part 4 s.6 PI

Operations Manager - BEng (Mech) (Hons), RPEQ Civil

P: (07) 4921 1780 / E: [redacted] PI @mcmengineers.com

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**Disclaimer:** This email message and any accompanying attachments are provided with all care and no responsibility. Messages may contain information that is confidential and is subject to legal privilege. Confidentiality and legal privilege attached to this communication are not waived or lost by reason of mistaken delivery to you. If you are not the intended recipient, do not read, use, disseminate, distribute or copy this message or attachments. If you have received this email in error, please notify the sender immediately and delete this message. Any views expressed in this message are those of the individual sender, except where the sender expressly, and with authority, states them to be in the views of McMurtrie Consulting Engineers. Before opening any attachments, please check them for viruses and defects. McMurtrie Consulting Engineers accepts no liability for any loss caused either directly or indirectly by a virus arising from the use of this message in any attached file.

**From:** Sch.4 Part 4 s.6 PI @mcmengineers.com>  
**Sent:** Friday, 22 November 2019 10:41 AM  
**To:** Sch.4 Part 4 s.6 PI mcmengineers.com>  
**Subject:** 0251819/14 - Rockhampton-Yeppoon Road

Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)  
P: (07) 4921 1780 / PI mcmengineers.com  
A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



\*\*\*\*\*

**WARNING:** This email (including any attachments) may contain legally privileged, confidential or private information and may be protected by copyright. You may only use it if you are the person(s) it was intended to be sent to and if you use it in an authorised way. No one is allowed to use, review, alter, transmit, disclose, distribute, print or copy this email without appropriate authority.

If this email was not intended for you and was sent to you by mistake, please telephone or email me immediately, destroy any hardcopies of this email and delete it and any copies of it from your computer system. Any right which the sender may have under copyright law, and any legal privilege and confidentiality attached to this email is not waived or destroyed by that mistake.

It is your responsibility to ensure that this email does not contain and is not affected by computer viruses, defects or interference by third parties or replication problems (including incompatibility with your computer system).

Opinions contained in this email do not necessarily reflect the opinions of the Department of Transport and Main Roads, or endorsed organisations utilising the same infrastructure.

\*\*\*\*\*

## Karen Stevens

---

**From:** Darren L Richardson  
**Sent:** Thursday, 5 December 2019 1:57 PM  
**To:** Peter T Trim  
**Cc:** Laxman A Gopali; Lisa J Abbott  
**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road - Approach Intersection Speed  
**Attachments:** Yeppoon Rd Dairy Inn Rd Artillery Rd.pdf

Hi Trimmy

Please see attached the proposed layout for Dairy Inn/Artillery Road.

Myself and Laxman have discussed this in detail and are both comfortable with the layout. Essentially the major outcome is to ensure the side road speed is reduce adequately to negotiate the "give way" treatment. There is ample sight distance from either side.

Please have a peruse and if there are any suggestions we can incorporate into the works.

This appears simple to implement under RSMW as it really only signs and can be delivered this financial year no problems. We would propose LSC to deliver. Furthermore, I have asked Laxman to propose some other intersections which may be suitable for the HRR funding through the Region.

Kind regards,  
Darren R

**Darren Richardson**  
Manager (Delivery & Operations), RPEQ CPEng MIEAust NER  
Fitzroy District/Central Queensland Region | Department of Transport and Main Roads

Floor 1 | Rockhampton - Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
P: (07) 49311680 | F: (07) 49275020  
M:   
E: [darren.l.richardson@tmr.qld.gov.au](mailto:darren.l.richardson@tmr.qld.gov.au)  
W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>  
**Sent:** Monday, 2 December 2019 5:05 PM  
**To:** Darren L Richardson <[darren.l.richardson@tmr.qld.gov.au](mailto:darren.l.richardson@tmr.qld.gov.au)>  
**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road - Approach Intersection Speed

Hi Darren

The draft drawing addressing the comment that was discussed last week in regards to signage layout for Yeppoon Rd & Dairy Inn Rd intersection.

Any feedback and comment would be much appreciated.

Kind Regards  
**Laxman Gopali**  
A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
P: 074931 1568 | M:   
E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)  
W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

Released under RTI - DTMR

## Karen Stevens

---

**From:** Laxman A Gopali  
**Sent:** Friday, 19 June 2020 12:26 PM  
**To:** Peter T Trim; Darren L Richardson  
**Subject:** FW: Rockhampton-Yeppoon Road, Artillery Road Intersection Signage  
**Attachments:** Yeppoon Rd Dairy Inn Rd Artillery Rd.pdf

Hi Peter

In reference to the email below all the works as per the attached layout is completed at the Rockhampton-Yeppoon Road, Artillery Road Intersection.

Kind Regards

**Laxman Gopali**

A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Darren Hines <Darren.Hines@livingstone.qld.gov.au>  
**Sent:** Friday, 19 June 2020 11:06 AM  
**To:** Nilupa C Hulugalla <Nilupa.C.Hulugalla@tmr.qld.gov.au>; Matthew Newmann <Matthew.Newmann@livingstone.qld.gov.au>  
**Cc:** Isuru S Perera <Isuru.S.Perera@tmr.qld.gov.au>; Laxman A Gopali <Laxman.A.Gopali@tmr.qld.gov.au>; Construction & Maintenance <infrastructure@livingstone.qld.gov.au>  
**Subject:** RE: Rockhampton-Yeppoon Road, Artillery Road Intersection Signage

Nilupa,

All signs are installed and the radar signs have been reprogrammed as required.  
All works are now complete.

If you have any queries regarding the above, please contact me.  
Thanks.

**Darren Hines**

**Rural Operations Coordinator | Construction & Maintenance | Infrastructure**

Livingstone Shire Council – Cordingley St Office

Ph: 07 4939 9863

Email: [darren.hines@livingstone.qld.gov.au](mailto:darren.hines@livingstone.qld.gov.au)



Web: [www.livingstone.qld.gov.au](http://www.livingstone.qld.gov.au) | Like us [www.facebook.com/livingstoneshirecouncil](https://www.facebook.com/livingstoneshirecouncil)



## Karen Stevens

---

**From:** Nilupa C Hulugalla  
**Sent:** Thursday, 9 April 2020 11:07 AM  
**To:** Darren L Richardson  
**Cc:** Faruk K Hossain; Laxman A Gopali; Isuru S Perera  
**Subject:** RE: Fitzroy Newspaper Clippings - Thursday, 9 April 2020

Hi Darren,

LSC complete erecting all signage and awaiting a technician on site to reset the digital sign. Hopefully will be completed by next week.

Kind regards.

### Nilupa Hulugalla

Engineer (Civil) | Fitzroy District | Central Queensland Region  
**Program Delivery and Operations** | Infrastructure Management and Delivery | Department of Transport and Main Roads

Floor 1 | Rockhampton - Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
P: (07) 4951 8517 | M NR  
E: [nilupa.c.hulugalla@tmr.qld.gov.au](mailto:nilupa.c.hulugalla@tmr.qld.gov.au)  
W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Darren L Richardson <[darren.l.richardson@tmr.qld.gov.au](mailto:darren.l.richardson@tmr.qld.gov.au)>  
**Sent:** Thursday, 9 April 2020 10:48 AM  
**To:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>  
**Cc:** Nilupa C Hulugalla <[Nilupa.C.Hulugalla@tmr.qld.gov.au](mailto:Nilupa.C.Hulugalla@tmr.qld.gov.au)>; Faruk K Hossain <[Faruk.K.Hossain@tmr.qld.gov.au](mailto:Faruk.K.Hossain@tmr.qld.gov.au)>  
**Subject:** Fwd: Fitzroy Newspaper Clippings - Thursday, 9 April 2020

Hi guys

Do we know when LSC are carrying out works at the Dairy Inn and Yeppoon Rd intersection?  
Please note the text to the editor.

Regards  
Darren R  
Manager (Delivery & Operations)  
Fitzroy District

Sent from my iPhone

Begin forwarded message:

**From:** Donna M Moore <[donna.m.moore@tmr.qld.gov.au](mailto:donna.m.moore@tmr.qld.gov.au)>  
**Date:** 9 April 2020 at 10:45:06 am AEST  
**To:** Fitzroy Newspaper Clippings <[Fitzroy\\_Newspaper\\_Clippings@tmr.qld.gov.au](mailto:Fitzroy_Newspaper_Clippings@tmr.qld.gov.au)>  
**Subject:** Fitzroy Newspaper Clippings - Thursday, 9 April 2020

Kind regards,

**Donna Moore**

Floor 1 | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
(07) 49311554  
[donna.m.moore@tmr.qld.gov.au](mailto:donna.m.moore@tmr.qld.gov.au)  
[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

Released under RTI - DTMR

## Karen Stevens

---

**From:** Nilupa C Hulugalla  
**Sent:** Friday, 10 January 2020 3:19 PM  
**To:** Darren Hines; Construction & Maintenance  
**Cc:** Isuru S Perera; Laxman A Gopali; Faruk K Hossain  
**Subject:** Rockhampton-Yeppoon Road, Artillery Road Intersection Signage  
**Attachments:** Yeppoon Rd Dairy Inn Rd Artillery Rd.pdf

Hi Darren,

Can I please ask to submit council's offer to undertake signage work Rockhampton Yeppoon road/ Artillery road intersection. Please see attached for more information.

Kind regards.

### **Nilupa Hulugalla**

Engineer (Civil) | Fitzroy District | Central Queensland Region

**Program Delivery and Operations** | Infrastructure Management and Delivery | Department of Transport and Main Roads

Floor 1 | Rockhampton - Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: (07) 4951 8517 | M:

E: [nilupa.c.hulugalla@tmr.qld.gov.au](mailto:nilupa.c.hulugalla@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

## Karen Stevens

---

**From:** Laxman A Gopali  
**Sent:** Thursday, 16 January 2020 10:00 AM  
**To:** Tracy Z Davis  
**Subject:** FW: 0251819/14 - Rockhampton-Yeppoon Road - Drawing Numbers Request  
**Attachments:** Drawing Number Request Form - 196 Yeppoon Rd.xlsx; 1.pdf; 2.pdf

Hi Tracy

Can you please arrange the drawing numbers and job number for this project.

Kind Regards

**Laxman Gopali**

A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: [redacted] NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

**From:** [redacted] Sch.4 Part 4 s.6 PI <mcmengineers.com>

**Sent:** Wednesday, 15 January 2020 5:09 PM

**To:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>; Fitzroy District Road Safety <[Fitzroy\\_District\\_Road\\_Safety@tmr.qld.gov.au](mailto:Fitzroy_District_Road_Safety@tmr.qld.gov.au)>

**Subject:** 0251819/14 - Rockhampton-Yeppoon Road - Drawing Numbers Request

Hi Laxman,

Please see attached Drawing Numbers Request Form. There are two drawings in total. Can we please have the drawing numbers and job number for this project?

Thanks.

Kind Regards,

[redacted] Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: [redacted] PI <mcmengineers.com>

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**mcmurtrie**  
CONSULTING ENGINEERS

---

**From:** Laxman A Gopali [<mailto:Laxman.A.Gopali@tmr.qld.gov.au>]

**Sent:** 10 January 2020 2:21 PM

**To:** [redacted] Sch.4 Part 4 s.6 PI <mcmengineers.com>

**Cc:** Lisa J Abbott <[Lisa.J.Abbott@tmr.qld.gov.au](mailto:Lisa.J.Abbott@tmr.qld.gov.au)>; Peter T Trim <[Peter.T.Trim@tmr.qld.gov.au](mailto:Peter.T.Trim@tmr.qld.gov.au)>

**Subject:** FW: Rockhampton-Yeppoon Road - Approach Intersection Signage

Hi [redacted] PI

As discussed today Livingston shire council have no objection with the proposed treatment on Yeppoon Rd & Dairy Inn Rd intersection. Can you please send the certified signed final drawing.

Kind Regards

**Laxman Gopali**

A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: [redacted] NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

**From:** [redacted] Sch.4 Part 4 s.6 PI [mcmengineers.com](mailto:mcmengineers.com)>

**Sent:** Friday, 10 January 2020 11:43 AM

**To:** Laxman A Gopali <[Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)>; Fitzroy District Road Safety <[Fitzroy\\_District\\_Road\\_Safety@tmr.qld.gov.au](mailto:Fitzroy_District_Road_Safety@tmr.qld.gov.au)>

**Cc:** [redacted] Sch.4 Part 4 s.6 PI [mcmengineers.com](mailto:mcmengineers.com)>

**Subject:** RE: Rockhampton-Yeppoon Road - Approach Intersection Signage

Hi Laxman,

Please see attached sketch and photos. The 'Overtaking Lane' sign for Rockhampton bound traffic does not clash with the motorists' sight line stationed at Dairy Inn Road.

Kind Regards,

[redacted] Sch.4 Part 4 s.6 PI

Engineer - BEng (Civil)

P: (07) 4921 1780 / E: [redacted] PI @mcmengineers.com

A: 63 Charles Street, North Rockhampton / PO Box 2149 Wandal QLD 4700



**mcmurtrie**  
CONSULTING ENGINEERS

---

**From:** Laxman A Gopali [<mailto:Laxman.A.Gopali@tmr.qld.gov.au>]

**Sent:** 10 January 2020 9:34 AM

**To:** [redacted] Sch.4 Part 4 s.6 PI [mcmengineers.com](mailto:mcmengineers.com)>

**Subject:** FW: Rockhampton-Yeppoon Road - Approach Intersection Signage

Hi [redacted] PI

I just wanted to check whether or not we have investigated if there was any issue with 'Overtaking Lane Ahead' sign for the Rockhampton bound traffic, similar to the other side of the road.



Kind Regards

**Laxman Gopali**

A/Principal Engineer (Road Operation), RPEQ | Fitzroy District / Central Queensland Region  
Program Delivery & Operations | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

\*\*\*\*\*

**WARNING:** This email (including any attachments) may contain legally privileged, confidential or private information and may be protected by copyright. You may only use it if you are the person(s) it was intended to be sent to and if you use it in an authorised way. No one is allowed to use, review, alter, transmit, disclose, distribute, print or copy this email without appropriate authority.

If this email was not intended for you and was sent to you by mistake, please telephone or email me immediately, destroy any hardcopies of this email and delete it and any copies of it from your computer system. Any right which the sender may have under copyright law, and any legal privilege and confidentiality attached to this email is not waived or destroyed by that mistake.

It is your responsibility to ensure that this email does not contain and is not affected by computer viruses, defects or interference by

third parties or replication problems (including incompatibility with your computer system).

Opinions contained in this email do not necessarily reflect the opinions of the Department of Transport and Main Roads, or endorsed organisations utilising the same infrastructure.

\*\*\*\*\*

Released under RTI - DTMR

## Fitzroy District Road Safety

---

**From:** Laxman A Gopali  
**Sent:** Thursday, 2 June 2022 11:19 AM  
**To:** Fitzroy District Road Safety; Karen Stevens  
**Subject:** FW: Rockhampton-Yeppoon Road - Approach Intersection Signage  
**Attachments:** Yeppoon Rd Dairy Inn Rd Artillery Rd.pdf

FYI

Kind Regards

**Laxman Gopali**

Principal Engineer (Civil) RPEQ CPEng NER MIEAust (Fitzroy District) | Central Queensland Region  
**Program Delivery and Operations Branch** | Infrastructure Management & Delivery Division | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Nilupa C Hulugalla <Nilupa.C.Hulugalla@tmr.qld.gov.au>  
**Sent:** Tuesday, 10 December 2019 1:37 PM  
**To:** Jeff Carter <Jeff.Carter@livingstone.qld.gov.au>; Darren Hines <Darren.Hines@livingstone.qld.gov.au>  
**Cc:** Laxman A Gopali <Laxman.A.Gopali@tmr.qld.gov.au>; Faruk K Hossain <Faruk.K.Hossain@tmr.qld.gov.au>  
**Subject:** Rockhampton-Yeppoon Road - Approach Intersection Signage

Hi Jeff, Darren

Please see proposed signage at Rockhampton Yeppoon road/ Dairy Inn Rd & Artillery Rd intersection. Essentially the major outcome is to ensure the side road speed is reduce adequately to negotiate the "give way" treatment. There is ample sight distance from either side.

Please have a peruse and if there are any suggestions we can incorporate into the works.

Kind regards.

**Nilupa Hulugalla**

Engineer (Civil) | Fitzroy District | Central Queensland Region

**Program Delivery and Operations** | Infrastructure Management and Delivery | Department of Transport and Main Roads

Floor 1 | Rockhampton - Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: (07) 4951 8517 | M: NR

E: [nilupa.c.hulugalla@tmr.qld.gov.au](mailto:nilupa.c.hulugalla@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)



**Network Administration Approval Form**

**Ref: Rockhampton-Yeppoon Road (196), Artillery Road and Dairy Inn Road Intersection**

- 1. Originator (Generate)      2. RMPC Administrator (Process / Approval)      3. Copy to Originator
- 4. RMPC Administrator (Close Out)

**Section 1 – This section to be completed by Originator**

Description of Work (including accurate location/chainage and reason for request – attach info where available)

*Road safety investigation was conducted at the intersection of Rockhampton-Yeppoon Road (196), Artillery Road and Dairy Inn Road with the following recommendations of works to be carried out as per attached Plan and Schedule.*

**Attachments:**

*Plan – Job No 394926 – Drawing No 819499/A*

*Schedule – Job No 394926 – Drawing No 819500/A*

*It is recommended that the works be funded from the relevant Minor Works Road Safety Project element.*

Shire – Livingstone Shire Council

Road – 196 Rockhampton-Yeppoon Road

Sections – 17.05 – 17.70

Date for Completion of Work - ASAP

Print Name/Position – Laxman Gopali, A/Principal Engineer....



.....Date: 6/02/20

**Section 2 – This section to be completed RMPC Administrator**

NAFR No. ....

Job No. ....

**Assessment**

Notice of Defect Issued    Yes     No     Notice of NCR Issued    Yes     No

Network Administration Funding Approval Required No     Yes  (Complete Variation Form)

Estimate of Cost .....

*(Prior to recommendation RMPC Administrator to negotiate with Originator if rejection likely)*

The works will be completed within the timeframe requested / will be completed by .....(date).

Recommended by ..... Dated ...../...../.....RMPC Administrator

Approved / Rejected by ..... Dated ...../...../.....Senior Engineer (Maintenance)

Network Administration Approval No .....

**Notes to the originator**

The reasons for rejections are / conditions for approval: .....

*Return Form to RMPC Administrator*

**Section 3 – Return COPY of Form to Originator**

Original form to MWR Folder

**Section 4 – This section to be completed by RMPC Administrator (Close Out)**

Works completed on .....Signed ..... RMPC Administrator



Road number

Road name

196

Rockhampton-Yeppoon Road

Section/Location

Local Authority

Artillery Rd and Dairy Inn Rd intersection. Ch17.05 - 17.70

Livingstone Shire Council

Sketch or Description

Show details and sketch or describe the location of the signs/devices (including distances from street or road junction, bridge or other permanently identifiable point to sign location); or list all the documents that define their details and location.

Reason required

To improve the safety at intersection

PLEASE REFER TO ATTACHED

PLAN 819499/A and SCHEDULE 819500/A FOR WORKS DETAILS

Complete below as applicable for either Permanent or Temporary installation and/or removal

Permanent installation or removal

Proposed date (for installation or removal)

30/16/2020

Recommended

Date

Approved (Delegate of the Director-General)

Date

NR

07/02/2020

NR

12/12/2020

Note: use to provide approval for erection and/or removal of regulatory signs/devices including traffic signals, pedestrian crossings etc. (i.e. those erected to inform motorists of regulations governing movement).

Temporary installation and/or removal

Contact number

Contractor

Contractor's Nominated Traffic Officer

Traffic Guidance Scheme number

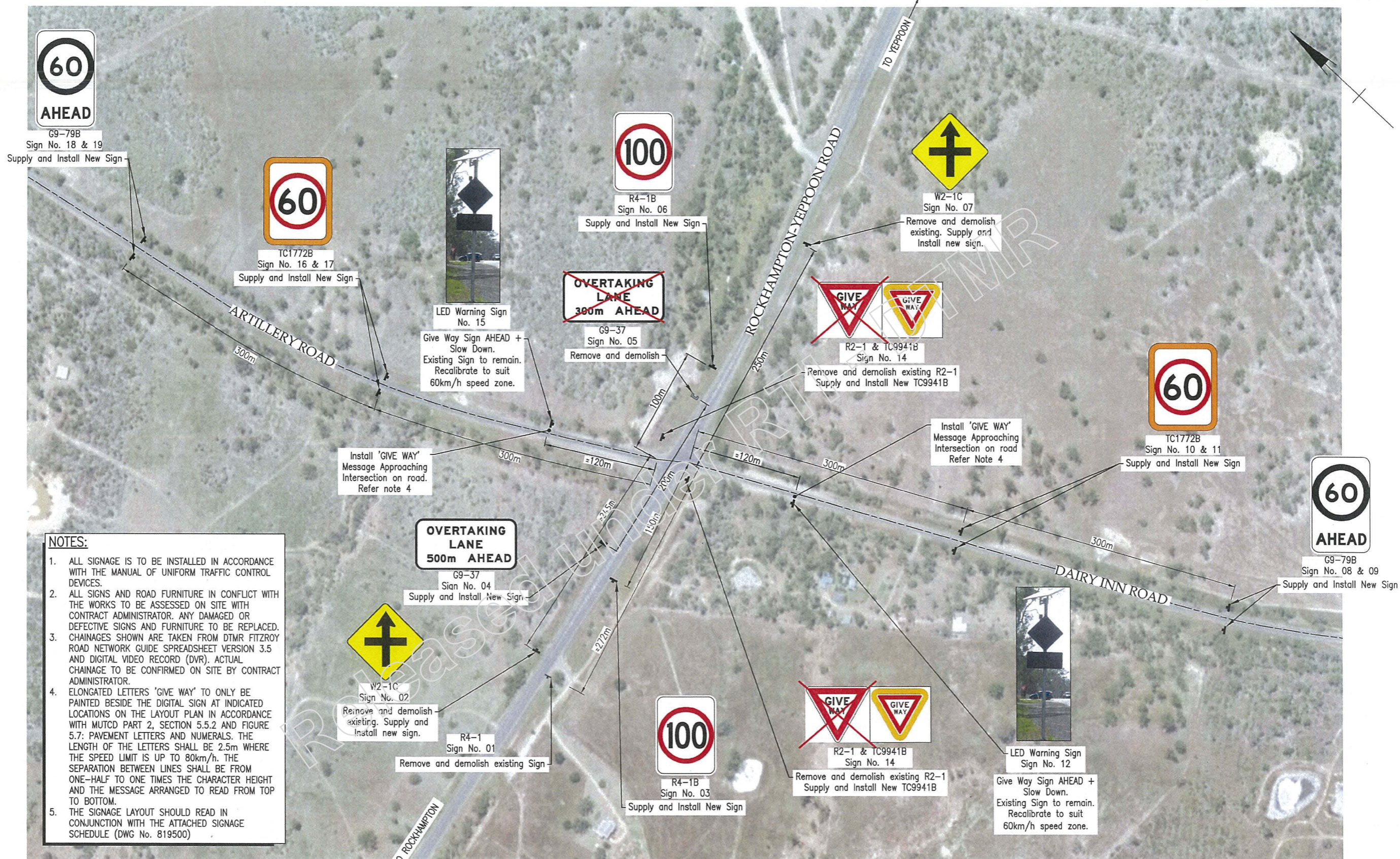
I certify that, as the Contractor's Nominated Traffic Officer, I have been adequately trained in roadworks signing and that the above changes to regulatory traffic devices are required for the construction of the Works and comply with the Contract and the MUTCD. This certification is given in accordance with section 72 of the Transport Operations (Road Use Management) Act 1995.

Contractor's Nominated Traffic Officer's signature

Date

/ /





- NOTES:**
1. ALL SIGNAGE IS TO BE INSTALLED IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
  2. ALL SIGNS AND ROAD FURNITURE IN CONFLICT WITH THE WORKS TO BE ASSESSED ON SITE WITH CONTRACT ADMINISTRATOR. ANY DAMAGED OR DEFECTIVE SIGNS AND FURNITURE TO BE REPLACED.
  3. CHAINAGES SHOWN ARE TAKEN FROM DTMR FITZROY ROAD NETWORK GUIDE SPREADSHEET VERSION 3.5 AND DIGITAL VIDEO RECORD (DVR). ACTUAL CHAINAGE TO BE CONFIRMED ON SITE BY CONTRACT ADMINISTRATOR.
  4. ELONGATED LETTERS 'GIVE WAY' TO ONLY BE PAINTED BESIDE THE DIGITAL SIGN AT INDICATED LOCATIONS ON THE LAYOUT PLAN IN ACCORDANCE WITH MUTCD PART 2, SECTION 5.5.2 AND FIGURE 5.7. PAVEMENT LETTERS AND NUMERALS. THE LENGTH OF THE LETTERS SHALL BE 2.5m WHERE THE SPEED LIMIT IS UP TO 80km/h. THE SEPARATION BETWEEN LINES SHALL BE FROM ONE-HALF TO ONE TIMES THE CHARACTER HEIGHT AND THE MESSAGE ARRANGED TO READ FROM TOP TO BOTTOM.
  5. THE SIGNAGE LAYOUT SHOULD READ IN CONJUNCTION WITH THE ATTACHED SIGNAGE SCHEDULE (DWG No. 819500)

LAYOUT  
SCALE: 1:2000(A1) 1:4000(A3)

ISSUED FOR CONSTRUCTION

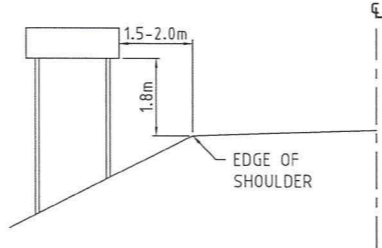
G		Associated Job Nos		Survey Data		Scales		LIVINGSTONE SHIRE COUNCIL		ROCKHAMPTON - YEPPOON RD, ARTILLERY RD & DAIRY INN RD INTERSECTION SIGNAGE LAYOUT				Queensland Government	
F		Auxiliary Drg Nos		Datum		AS SHOWN		196 ROCKHAMPTON - YEPPOON ROAD		ENGINEERING CERTIFICATION (RPEQ)		Job No. 394926		Contract No. -	
E				Horiz. Grid				CTL CHGE 17.05 - 17.70		NAME		NO. 15243		DATE 24/01/20	
D				Height Origin				Reference Points		SIGNATURE		NR			
C				Survey Books		Dimensions shown in metres except where shown otherwise		Preceding RP		M THOMAS					
B								Dist. to start of job (km)		Designed					
A								From start to end of job		M MATHEV					
		Revisions/Descriptions		Certification		Date		From end to Following RP							
								Following RP							
								Through Chainage from							

**SIGN SCHEDULE**

SIGN NO	SIGN TYPE	SIGN DESCRIPTION	WORK DESCRIPTION	DIRECTION	CHAINAGE (KM)	SIDE OF ROAD	SLOPE DETAILS (1 ON X)	SIGN DETAILS					STIFFENER DETAILS				NEW SUPPORT DETAILS					NEW FOOTING DETAILS				
								WIDTH (MM)	HEIGHT (MM)	AREA (M2)	OFFSET FROM SHOULDER HINGE (MM)	HEIGHT ABOVE SHOULDER HINGE (MM)	TYPE	NO.	SPACING (MM)	NO. OF BRACKETS	NO.	SPACING (MM)	DIMENSION (MM) NB	MATERIAL	POST 1 LENGTH (MM)	POST 2 LENGTH (MM)	SLEEVE LENGTH (MM)	SLEEVE SIZE (MM)	DIA (MM)	DEPTH (MM)
<b>ROCKHAMPTON-YEPPON ROAD</b>																										
01	R4-1	SPEED RESTRICTION	REMOVE & DEMOLISH	A	17.066	RHS	-																			
02A	W2-1	CROSS ROAD	REMOVE & DEMOLISH	G	17.093	LHS	-																			
02B	W2-1C	CROSS ROAD	SUPPLY & INSTALL	G	17.093	LHS	4	900	900	0.81	1500	1800	2	4	400	4	1	-	65	FLUTED AL.	4306	-	600	76	300	700
03	R4-1B	SPEED RESTRICTION "100"	SUPPLY & INSTALL	A	17.188	RHS	4	600	800	0.480	1500	1800	-	-	-	-	1	-	50	CHS	3500	-	-	-	300	450
04	G9-37	OVERTAKING LANE "500m AHEAD"	SUPPLY & INSTALL	G	17.222	LHS	FLAT	2600	1200	2.400	1500	1800	1	4	350	8	2	1560	80	FLUTED AL.	3700	3700	750	89	300	900
05	G9-37	OVERTAKING LANE "300m AHEAD"	REMOVE & DEMOLISH	G	17.422	LHS	-																			
06	R4-1B	SPEED RESTRICTION "100"	SUPPLY & INSTALL	G	17.438	LHS	6	600	800	0.480	1500	1800	-	-	-	-	1	-	50	CHS	3338	-	-	-	300	450
07A	W2-1	CROSS ROAD	REMOVE & DEMOLISH	A	17.588	RHS	-																			
07B	W2-1C	CROSS ROAD	SUPPLY & INSTALL	A	17.588	RHS	4	900	900	0.81	1500	1800	2	4	400	4	1	-	65	FLUTED AL.	4306	-	600	76	300	700
<b>DAIRY INN ROAD</b>																										
08	G9-79B	SPEED LIMIT AHEAD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	600	1000	0.600	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3950	-	600	76	300	700
09	G9-79B	SPEED LIMIT AHEAD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	600	1000	0.600	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3950	-	600	76	300	700
10	TC1772B	R4-1 SPEED LIMIT WITH TARGET BOARD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	800	1000	0.800	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3975	-	600	76	300	700
11	TC1772B	R4-1 SPEED LIMIT WITH TARGET BOARD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	800	1000	0.800	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3975	-	600	76	300	700
12	LED WARNING SIGN	GIVE WAY SIGN AHEAD + SLOW DOWN	TO REMAIN (RECALIBRATE)	REFER PLAN	REFER PLAN	REFER PLAN	-																			
13A	R1-2	GIVE WAY SIGN	REMOVE & DEMOLISH	REFER PLAN	REFER PLAN	REFER PLAN	FLAT																			
13B	TC9941B	GIVE WAY SIGN WITH TARGET BOARD	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	FLAT	1385	1200	0.830	1500	1800	1	1	-	1	1	-	65	FLUTED AL.	3700	-	600	76	300	700
<b>ARTILLERY ROAD</b>																										
14A	R1-2	GIVE WAY SIGN	REMOVE & DEMOLISH	REFER PLAN	REFER PLAN	REFER PLAN	FLAT																			
14B	TC9941B	GIVE WAY SIGN WITH TARGET BOARD	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	FLAT	1385	1200	0.830	1500	1800	1	1	-	1	1	-	65	FLUTED AL.	3700	-	600	76	300	700
15	LED WARNING SIGN	GIVE WAY SIGN AHEAD + SLOW DOWN	TO REMAIN (RECALIBRATE)	REFER PLAN	REFER PLAN	REFER PLAN	-																			
16	TC1772B	R4-1 SPEED LIMIT WITH TARGET BOARD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	800	1000	0.800	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3975	-	600	76	300	700
17	TC1772B	R4-1 SPEED LIMIT WITH TARGET BOARD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	800	1000	0.800	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3975	-	600	76	300	700
18	G9-79B	SPEED LIMIT AHEAD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	600	1000	0.600	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3950	-	600	76	300	700
19	G9-79B	SPEED LIMIT AHEAD "60"	SUPPLY & INSTALL	REFER PLAN	REFER PLAN	REFER PLAN	4	600	1000	0.600	1500	1800	-	-	-	-	1	-	65	FLUTED AL.	3950	-	600	76	300	700

**NOTES:**

- THE CALCULATED SUPPORT LENGTH SHOULD ONLY BE CONSIDERED AS A DESIGN LENGTH, TO BE CONFIRMED ON SITE.
- REFER STANDARD DRAWINGS: 1363 TRAFFIC SIGN - MULTIPLE TRAFFIC SIGNS SUPPORT 1364 TRAFFIC SIGN - CONNECTION STRAPS AND ERECTION CLEAT DETAILS 1368 TRAFFIC SIGN - SINGLE TRAFFIC SIGN SUPPORT 1369 TRAFFIC SIGN - DETAILS OF SIGN STIFFENING EXTRUSION
- REFER SIGNFIX PRODUCT INFORMATION: GROUND SLEEVE FOUNDATION LAYOUT RECOMMENDED INSTALLATION INSTRUCTIONS
- ALL ALUMINUM POSTS TO BE INSTALLED WITH APPROVED RESTRAINT DEVICE WITH TOP OF SOCKET 50mm - 100mm ABOVE FINISHED SURFACE AND NOT LESS THAN 28MPa (WET) CONCRETE IS TO BE USED IN FOOTING AS PER MANUFACTURER'S SPECIFICATIONS.
- SUPPLY POST SLEEVE'S WITH A MINIMUM LENGTH OF 1m AND CUT BACK ONSITE TO SUIT.
- CHAINAGES SHOWN ARE TAKEN FROM DTMR FITZROY ROAD NETWORK GUIDE SPREADSHEET VERSION 3.5 AND DIGITAL VIDEO RECORD (DVR). ACTUAL CHAINAGE TO BE CONFIRMED ON SITE BY CONTRACT ADMINISTRATOR.



**SIGN SETOUT ELEVATION**  
 SCALE: 1:100 (A1) 1:200 (A3)

**ASSUMED SITE PROPERTIES:**

WIND REGION: C  
 EXPOSED TERRAIN: NO  
 FOUNDATION SOIL: FIRM TO STIFF CLAY SOIL  
 DIRECTION OF TRAVEL: GAZETAL DIRECTION

**ISSUED FOR CONSTRUCTION**

G F E D C B A	Associated Job Nos	Survey Data	Scales  <b>AS SHOWN</b>	LIVINGSTONE SHIRE COUNCIL				ROCKHAMPTON - YEPPON RD, ARTILLERY RD & DAIRY INN RD INTERSECTION								Job No. 394926 Contract No. - Drawing No. 819500 1A Series Number 2 of 2 MRR_Detail (02/14)
	Auxiliary Drg Nos	Datum		196 ROCKHAMPTON - YEPPON ROAD				SIGN SCHEDULE								
		Horiz. Grid		CTL CHGE 17.05 - 17.70												
		Height Origin		Reference Points				Preceding RP    Dist. to start of job (km)    From start to end of job    From end to Following RP    Following RP								
	Survey Books	Survey Books	Dimensions shown in metres except where shown otherwise	Through Chainage from				Drawn: M THOMAS Designed: M MATHEV ENG. AREA: CIVIL NAME: LACHLAN McMURTRIE SIGNATURE: NR NO.: 15243 DATE: 24/01/20								

# Annexure MRS14.1 (July 2019)

## Road Furniture



### Specific Contract Requirements

**Contract Number**

**Note:** Clause references within brackets in this Annexure refer to Clauses in the parent Specification MRS14 unless otherwise noted.

#### 1 Regulatory, warning and hazard signs

##### 1.1 Method of measurement for supply (Clause 2.3.1)

The supply of regulatory, warning and hazard signs shall be measured by

Method A (lump sum)

Method B (each by sign type)

##### 1.2 Method of measurement for installation (Clause 2.3.2)

The installation of regulatory, warning and hazard signs shall be measured by

Method A (lump sum)

Method B (each by number of posts)

##### 1.3 List of regulatory, warning and hazard signs (method A only) (Table 2.1, Clauses 2.3.1 and 2.3.2)

The following is a list of the regulatory, warning and hazard signs required under the Contract.

Sign Number	Sign Type	Quantity required	
		Total	Number of Posts
Refer Drawing Number 819500, Rev A for Sign Schedule.			

**2 Direction and information signs**

**2.1 Method of measurement for supply (Clause 2.3.3)**

The supply of direction and information signs shall be measured by

Method A (lump sum)

Method B (square metres)

**2.2 Method of measurement for installation (Clause 2.3.4)**

The installation of direction and information signs shall be measured by

Method A (lump sum)

Method B (each by sign number)

Method C (each by sign size)

**2.3 List of direction and information signs (Table 2.1, Clauses 2.3.3 and 2.3.4)**

The following is a list of the direction and information signs required under the Contract.

Sign Number	Width (mm)	Depth (mm)	Location of Sign Faces Details

**3 Grids (Clause 2.3.5)**

Item numbers for the grids shall apply in the locations stated below. The required details of each grid shall be as stated in Clause 9 of Annexure MRTS14.1

Item Number	Reference Location

# Annexure MRTS14.1 (July 2019) Road Furniture

## Specific Contract Requirements

**Contract Number**



**Note:** Clause references within brackets in this Annexure refer to Clauses in the parent Technical Specification MRTS14 unless otherwise noted.

### Part A – Completed by Principal as Part of Brief

#### 1 Road furniture/structures to be salvaged

##### 1.1 *List of road furniture to be salvaged (Clause 8.2)*

The following is a list of existing road furniture/structures to be salvaged

##### 1.2 *Location of storage site (Clause 8.3)*

Salvaged road furniture/structures shall be transported to the following storage site.

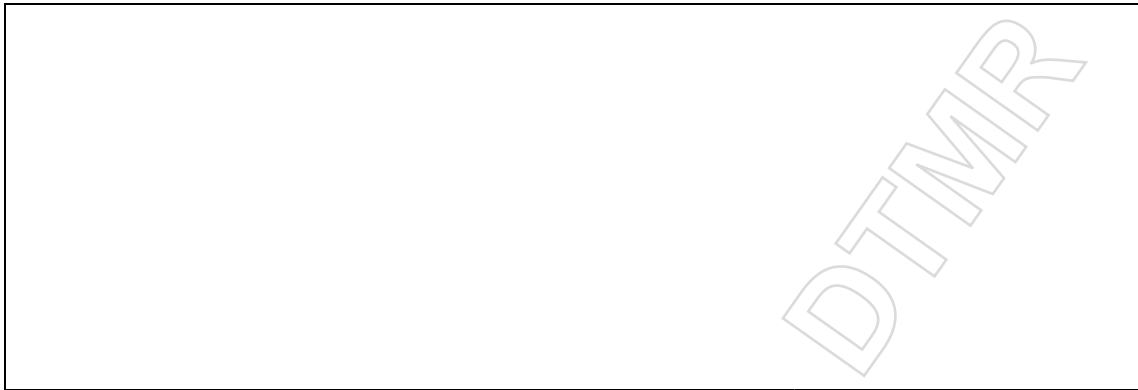


**Part B – Completed by designer under the Contract**

**2 Construction procedures (Clause 5.2)**

The following construction procedures shall be submitted.

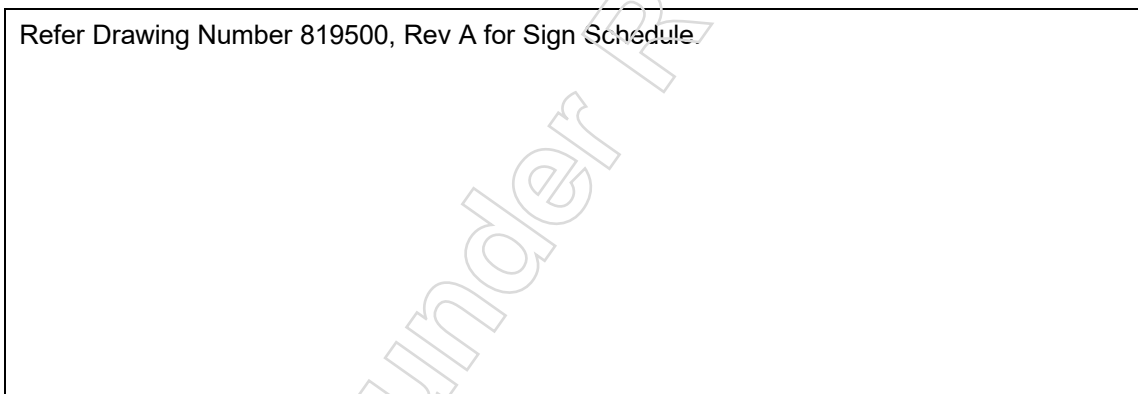
Released under RTI - DTMR



**3 Road furniture/structures to be demolished (Clause 7.2)**

The following is a list of existing road furniture/structures to be demolished and disposed of.

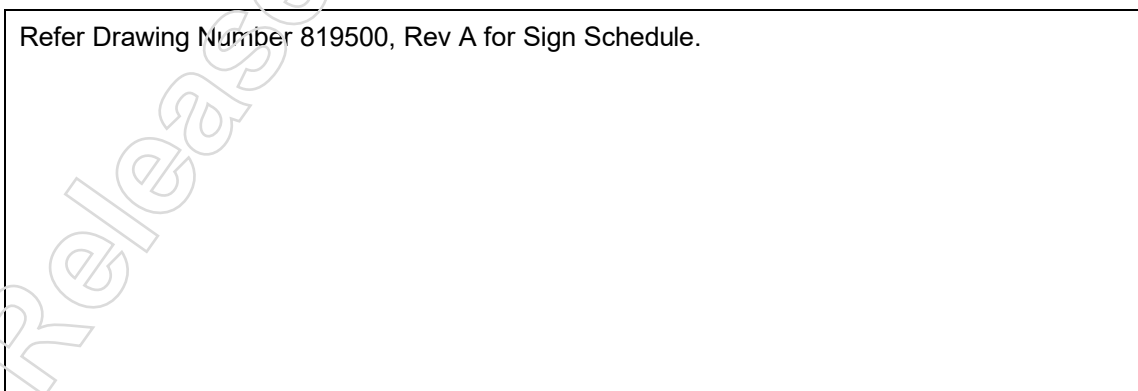
Refer Drawing Number 819500, Rev A for Sign Schedule.



**4 Road furniture/structures to be removed and re-erected (Clause 9.2)**

The following is a list of existing road furniture/structures to be removed and re-erected.

Refer Drawing Number 819500, Rev A for Sign Schedule.



**5 Delineators for rigid road edge guide posts (Clause 10.2.4)**

Delineators for rigid road edge guide posts shall be

Corner cube

Prismatic lens

**6 Fencing and Gates**

**6.1 Fencing details shall be as shown below and/or on the Drawing Numbers listed (Clause 11.2)**



**6.2 Gate details shall be as shown below and/or on the drawing numbers listed (Clause 12.2)**



## 7 Road Signs

### 7.1 Class of material (Clause 13.3.3.1)

The following is the minimum class of sign material.

Sign Type	Sign Number	Class of Sign Face Materials	
		Legend	Background
Regulatory signs	(R1-1)	Class 400	Screened
STOP	(R1-2)	Black non reflective	Class 400
GIVE WAY	(R1-3)	Black non reflective	Class 400
Roundabout	(R3-1)	Black non reflective	Class 400
Pedestrian Crossing		Black non reflective	(fluorescent yellow-green)
All other regulatory signs*		Class 400	Class 400
Warning Signs*			
All warning signs		Class 400	Class 400
Roadworks		Black non reflective	Class 400T
Special Purpose Signs		Black non reflective	Class 400
Hazard Markers		Black non reflective	Class 400
Flood Gauge Markers		Black non reflective	Class 400
Guide Signs			
Advance, Intersection, Reassurance Direction Signs and Advance Lane signs		Class 400	Class 400
Tourist and Service Signs		Class 400	Class 400
Geographical Feature and Street Name Signs		N/A	Class 400
<b>Freeway Signs</b> (white on standard green background; black on white background; white on blue background; black on yellow background; white on red background)			
General application		Class 400	Class 400
Overhead and gantry application		Class 1100	Class 1100
Traffic Instruction Signs		Class 400	Class 400

### 7.2 Warranty (Clause 13.4.12.1)

A warranty is required from the sign panel manufacturer.

**8 Project Signs (Clauses 15.2 and 15.6)**

The following is a list of project signs required to be erected.

Sign Identification	Location of Sign Face Details	Sign Size (mm)	Supports Number/Type/ Spacing	Footing Details		Remove on Completion
				Depth (mm)	Dia (mm)	

**Additional details:**

**9 Grids (Clause 17.5.1)**

The following dimensions and other details shall apply to grids.

Reference Location	Grid Span (mm)	Grid Width (mm)	Abutment Type (Cast insitu / Precast)	Base Slab Type (Cast insitu / Precast)

**10 Concrete traffic barriers**

**10.1 Compressible filler – approved products (Clause 19.2.8)**

The following products are approved as compressible joint filler.

--

**10.2 Joint sealant – approved products (Clause 19.2.9)**

The following products are approved as joint sealant.

--

**11 Design of road safety barrier device**

Design criteria/performance standards (Clause 18.1 and Clause 21.3).

Location	Containment Capacity	Deflection	Post Spacing	Min/Max system length	Terminal Type	Other

## 12 Supplementary requirements

The following supplementary requirements shall apply (Clause 22).

All aluminium fluted Signfix posts  $\geq 65\text{mm}$  diameter shall be installed with proprietary restraint device



Queensland Government

**Project Number : 394926**

**Project Boundary : LIVINGSTONE SHIRE COUNCIL**

**Location : 196 ROCKHAMPTON-YEPPOON RD / ARTILLEY RD / DAIRY INN RD  
INTERSECTION (Ch 17.05 - Ch 17.7km)**

**Revision: A**

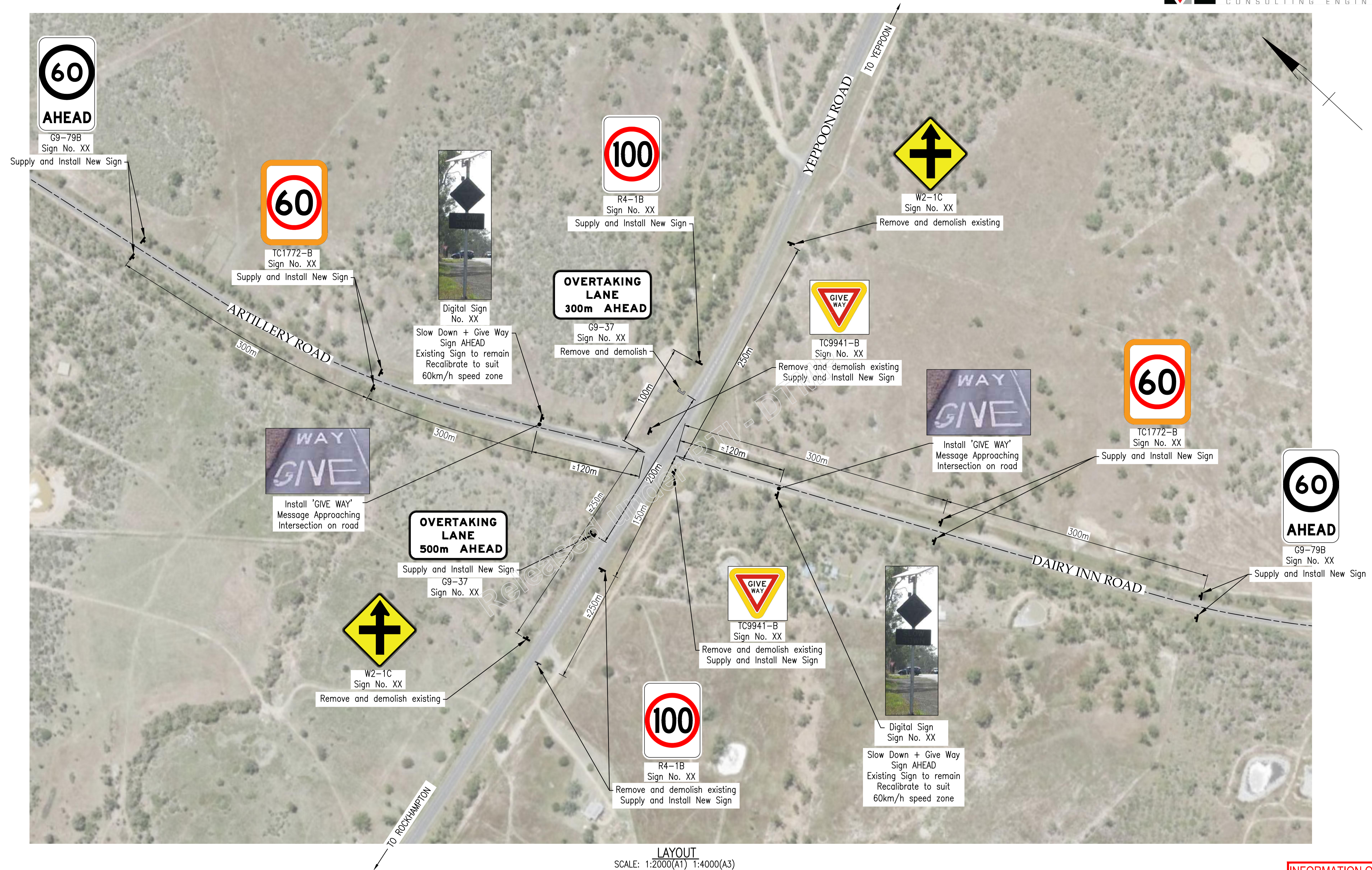
**Date: 24.01.20**

NOTE: Estimate must be considered absolutely as the property of Transport and Main Roads until the acceptance of a tender, and must under no circumstances be divulged.

### Work Package : Signage

#### Work Package : Signage Design

Item Number	Description	Additional Info	Unit of Measure	Quantity	Unit Rate (\$) (Excl. GST)	Amount (\$) (Excl. GST)
<b>MRS02 Nov 19</b>						
<b>PROVISION FOR TRAFFIC</b>						
20001.01	Provision for traffic (MRS02 Nov 19)		lump sum	1		\$0.00
<b>MRS28 Nov 19</b>						
<b>CONTRACTOR'S SITE FACILITIES AND CAMP</b>						
20101.01	Contractor's site facilities (MRS28 Nov 19)		lump sum	1		\$0.00
20102.01	Contractor's camp (MRS28 Nov 19)		lump sum	1		\$0.00
<b>MRS14 Jul 19</b>						
<b>ROAD FURNITURE</b>						
50001.01	Demolition of road furniture, as listed in Clause 3 of Annexure MRTS14.1 (MRS14 JUL 19)		lump sum	1		\$0.00
50052.01	Supply of regulatory, warning and hazard sign faces, as listed in Clause 1.3 of Annexure MRS14.1 (MRS14 JUL 19)		lump sum	1		\$0.00
50056.01	Installation of regulatory, warning and hazard signs, as listed in Clause 1.3 of Annexure MRS14.1 (MRS14 JUL 19)		lump sum	1		\$0.00
<b>Work Package Total :</b>						<b>\$0.00</b>



LAYOUT  
 SCALE: 1:2000(A1) 1:4000(A3)

Last Modified: 12 Dec 02, 2019 - 2:50pm XREFS :-

G				
F				
E				
D				
C				
B				
A				
	Revisions/Descriptions	Certification	Date	Microfiled

Associated Job Nos	Survey Data
	Datum
Auxiliary Drg Nos	Horiz. Grid
	Height Origin
	Survey Books
Scales	
AS SHOWN	
Dimensions shown in metres except where shown otherwise	

<b>ROCKHAMPTON REGIONAL COUNCIL</b>				
196 ROCKHAMPTON - YEPPOON ROAD				
<i>CTL CHGE</i>				
Reference Points				
Preceding RP	Dist. to start of job (km)	From start to end of job	From end to Following RP	Following RP
Through Chainage from				

<b>ROCKHAMPTON - YEPPOON RD, ARTILLERY RD &amp; DAIRY INN RD INTERSECTION</b>				
<b>INTERSECTION SIGNAGE LAYOUT</b>				
Drawn	ENGINEERING CERTIFICATION (RPEQ)			
M THOMAS	ENG. AREA	NAME	SIGNATURE	NO.
Designed	CIVIL	MARK VIGNALE		12084
M MATHEV				

**INFORMATION ONLY**

**Queensland Government**

Job No. 0251819

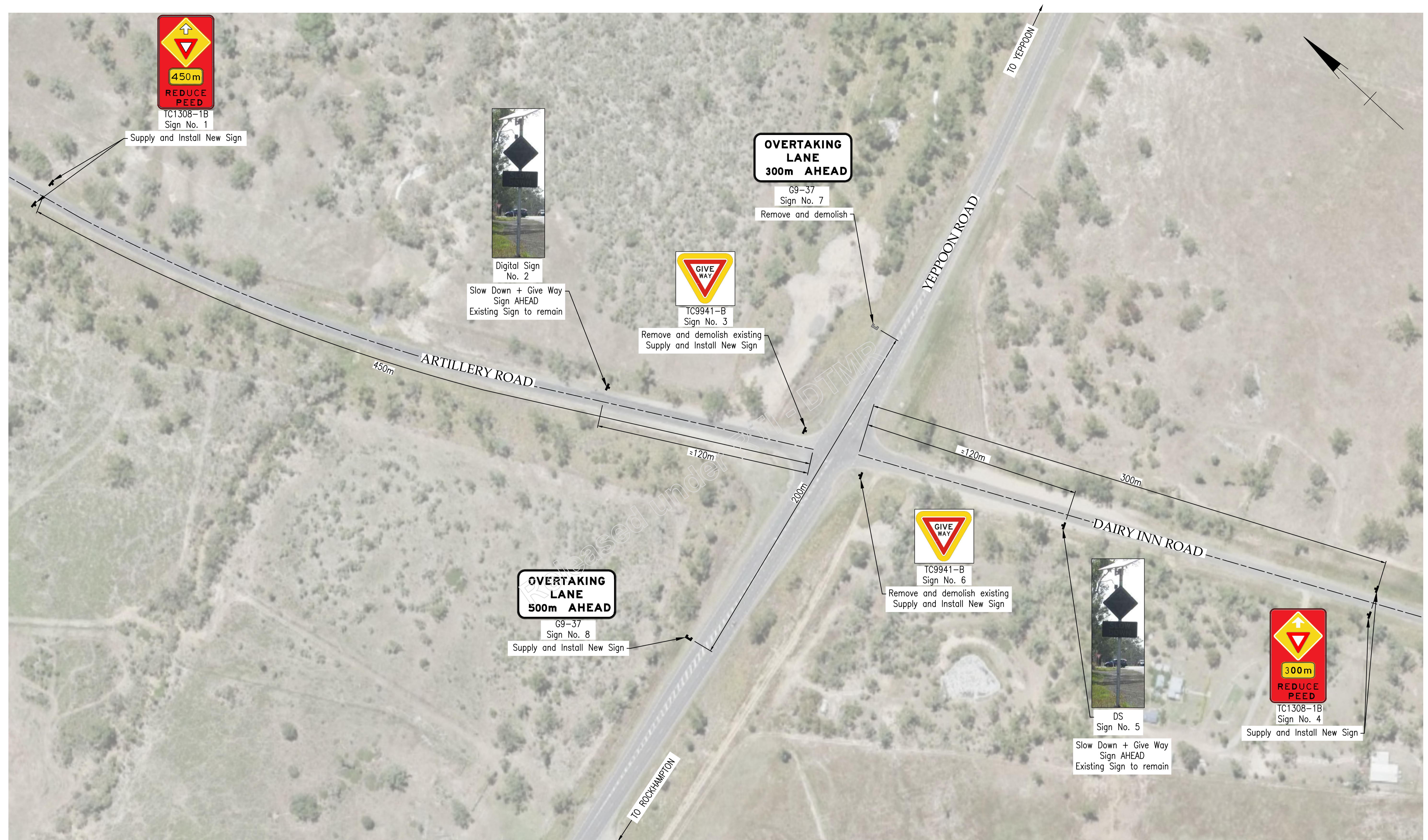
Contract No. \_\_\_\_\_

Drawing No. \_\_\_\_\_

Series Number 1 of 2

MRR\_Detail (02/14)





LAYOUT  
 SCALE: 1:1000(A1) 1:2000(A3)

INFORMATION ONLY

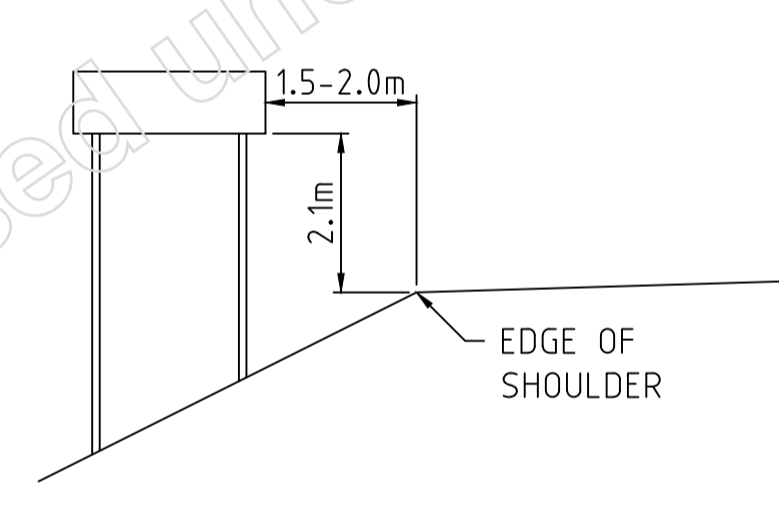
G					Associated Job Nos	Survey Data		Scales	ROCKHAMPTON REGIONAL COUNCIL					ROCKHAMPTON – YEPPOON RD, ARTILLERY RD &					Queensland Government			
						Datum			196 ROCKHAMPTON – YEPPOON ROAD					DAIRY INN RD INTERSECTION						Job No.	0251819	
F					Auxiliary Drg Nos	Horiz. Grid	Height Origin	AS SHOWN	CTL CHGE					ENGINEERING CERTIFICATION (RPEQ)					Contract No.			
D									Reference Points					SIGNATURE					NO.	DATE		
C					Survey Books			Dimensions shown in metres except where shown otherwise	Preceding RP	Dist. to start of job (km)	From start to end of job	From end to Following RP	Following RP	Drawn	ENG. AREA	NAME	SIGNATURE	NO.	DATE	Series Number	1 of 2	
B																	M THOMAS	CIVIL	MARK VIGNALE			
A														Designed								
Revisions/Descriptions		Certification	Date	Microfiled	Through Chainage from					M MATHEV												

Last Modified: 11 Nov 22, 2019 - 2:03pm XREFS :-

SIGN SCHEDULE																								
SIGN NO	SIGN TYPE	WORK DESCRIPTION	DIRECTION	CHAINAGE (km)	SIDE OF ROAD	SLOPE DETAILS (1 on X)	SIGN DETAILS					STIFFENER DETAILS				NEW SUPPORT DETAILS					NEW FOOTING DETAILS			
							WIDTH (mm)	HEIGHT (mm)	AREA (m <sup>2</sup> )	OFFSET FROM SHOULDER HINGE (mm)	HEIGHT ABOVE SHOULDER HINGE (mm)	TYPE	No.	SPACING (mm)	No. OF BRACKETS	No.	SPACING (mm)	DIMENSION (mm) NB	MATERIAL	POST 1 LENGTH (mm)	POST 2 LENGTH (mm)	SLEEVE LENGTH (mm)	SLEEVE SIZE (mm)	DIA (mm)
<b>ARTILLERY ROAD</b>																								
1	TC1308-1B (Give Way AHEAD)	SUPPLY AND INSTALL	G	-	LHS	6																		
2	Digital Sign	EXISTING SIGN TO REMAIN	G	-	LHS	6																		
3	TC9941-B (Give Way)	REMOVE AND DEMOLISH EXISTING. SUPPLY AND INSTALL	G	-	LHS	6																		
<b>DAIRY INN ROAD</b>																								
4	TC1308-1B (Give Way AHEAD)	SUPPLY AND INSTALL	G	-	LHS	6	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
5	Digital Sign	EXISTING SIGN TO REMAIN	A	-	LHS	6	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
6	TC9941-B (Give Way)	REMOVE AND DEMOLISH EXISTING. SUPPLY AND INSTALL	A	-	LHS	6	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
<b>YEPPOON ROAD</b>																								
7	OVERTAKING LANE (300m AHEAD)	REMOVE AND DEMOLISH EXISTING.			LHS																			
8	OVERTAKING LANE (500m AHEAD)	SUPPLY AND INSTALL			LHS																			

**NOTES:**

- The calculated support length should only be considered as a design length, to be confirmed on site.
- Refer Standard Drawings: 1363 Traffic Sign - Multiple Traffic Signs Support 1364 Traffic Sign - Connection Straps and Erection Cleat Details 1368 Traffic Sign - Single Traffic Sign Support 1369 Traffic Sign - Details of Sign Stiffening Extrusion
- Refer SIGNFIX Product Information: Ground Sleeve Foundation Layout Recommended Installation Instructions
- All Aluminum posts to be installed with approved restraint device with top of socket 50mm - 100mm above finished surface and not less than 28MPa (wet) concrete is to be used in footing as per manufacturer's specifications.
- Supply post sleeve's with a minimum length of 1m and cut back onsite to suit.
- Chainages are based on digital video record (DVR) files. Sign location is to be confirmed on site by the Contract Administrator



**SIGN SETOUT ELEVATION**  
 SCALE: 1:100 (A1) 1:200 (A3)

**ASSUMED SITE PROPERTIES:**

WIND REGION: A  
 EXPOSED TERRAIN: NO  
 FOUNDATION SOIL: FIRM TO STIFF CLAY SOIL  
 DIRECTION OF TRAVEL: GAZETTED DIRECTION

Last Modified: 11 Nov 22, 2019 - 2:03pm XREFS :-

G		Associated Job Nos		Survey Data		Scales		ROCKHAMPTON REGIONAL COUNCIL				ROCKHAMPTON - YEPPOON RD, ARTILLERY RD & DAIRY INN RD INTERSECTION				Queensland Government Job No. 0251819			
F		Datum				AS SHOWN		196 ROCKHAMPTON - YEPPOON ROAD				SIGN SCHEDULE							
D		Auxiliary Drg Nos		Horiz. Grid				CTL CHGE				ENGINEERING CERTIFICATION (RPEQ)				Contract No.			
C				Height Origin				Reference Points				Drawn				Job No.			
B				Survey Books				Preceding RP Dist. to start of job (km) From start to end of job Following RP				M THOMAS				0251819			
A								Dimensions shown in metres except where shown otherwise				Designed				Drawing No. A			
		Revisions/Descriptions		Certification		Date		Microfiled		Through Chainage from				M MATHEV				Series Number 2 of 2	
																		MRR_Detail (02/14)	

INFORMATION ONLY



**Crash Types**

Crash Dates  -  Alignment: Vertical

Owner  Horizontal

DCA Code  Feature

Group  Traffic Ctrl

Speed Limit

Fatalities =  Contrib Circ.

Severity  Unit Type

Nature  Risk Factor

**Area** LGA  SLA  Police Division

**Road Sections**

All Road Sections  S Include Crashes on:  Y Thru road Mid-block  Y Thru roads at Intersections  Y Intersecting roads at Intersections

Road Section	Cway	Start		End		Tdist		Number of Crashes					Total
		RPC	Dist	RPC	Dist	Start	End	Fatal	Hosp.	Medical	Minor	PDO	
196 ROCKHAMPTON - YEPPOON ROAD		5D	4.168	5D	5.168	16.838	17.838	0	9	2	2	3	16

**Intersections**

All Intersections  N



Crash No.	Date	Day	Hour	DCA	No. Units	Street/s	Nature	
20171872257	26-OCT-2017	Thu	01	705 OFF PATH-	1	Rockhampton - Yeppoon Rd (Part 1)	06 Hit fixed obstruction or tempora	
R Sect	196 Rockhampton - Yeppoon Road		RPC	5D		Alignment: Vertical	1 Level	
Cway	1	Direction	N	Dist from RPC	4.194		Horizontal	1 Straight
Inter.			Tdist	16.864		Feature	99 No feature	
Road Surface	Sealed - dry				Traffic Control	99 No Traffic Control		
Units	Age	Gender	Unit Type	Dirn.	Intended Action	BAC		
1	PI	M	01 Car, Station Wagon	N	01 Go Straight Ahead	92		

Description

MVC REPORT SUMMARY (NARRATIVE) A single vehicle traffic crash occurred along Rockhampton-Yeppoon Road near the intersection of Artillery Road. Unit 1 was travelling North East bound along Rockhampton-Yeppoon Road when multiple dogs have entered the roadway. Unit 1 swerved across the oncoming lane to avoid the dogs causing him to collide with the embankment. Unit 1 has extensive damage to the vehicle and it is highly possible that the vehicle has rolled numerous times. Unit 1 was towed from the scene authorised by the driver of the vehicle. The driver of unit 1 was taken to Rockhampton Base Hospital with unknown injuries. SCENE OBSERVATION The traffic incident occurred during night hours. Rockhampton-Yeppoon Road runs South West to north Easterly direction. With two lanes, one in each direction. The roadway is governed by a 100km/h speed limit. The road is of tarmac construction and in a good condition. WEATHER During the incident there was a no level of rainfall in the area. It was night time at the incident time but is not a contributing factor. INVESTIGATIONS Police had conversations with the following persons: X ) (Driver Unit 1) Version/Conversation as per Person MVC Report CONCLUSIONS/RECOMMENDATION As a result of Police investigations, Police believe the main factor causing the traffic crash is the dogs entering the roadway. Multiple QAS officer confirmed there were dogs present at their time of arrival. Due to this it is recommended that no enforcement action be recorded due to factors out of the control of the driver.

Contributing Circumstances

- 1 LIGHTING - NO STREET LIGHTING
- 1 CONDITION - UNDER INFLUENCE OF LIQUOR
- 1 ANIMAL UNCONTROLLED - ON ROAD
- 1 DRIVER - FATIGUE RELATED BY DEFINITION

Not Relevant

Released under RTI - DTMR

**Road N°/Name.:** 196 Rockhampton – Yeppoon Road

**Start Chge to End Chge (km):** Approx. CH 17.3km

**Description:** Safety concerns at the intersection of Rockhampton-Yeppoon Road, Artillery Road and Dairy Inn Road

**Revision:** A

**Date:** 14.11.2019

### Background Information

- Preliminary desktop review of relevant information was completed prior to inspection;
- The AADT on this section of the Rockhampton-Yeppoon Road in the vicinity of Dairy Inn Road / Artillery Road is 11,558 vpd (2018) including 9.31% heavy vehicles. 85<sup>th</sup> percentile speed in this location was not available at the time of the review.
- Estimated AADT for Artillery Road and Dairy Inn Road is approximately 725 vpd and 1795 vpd respectively based on the 02 May 2018 intersection analysis report.
- Livingstone Shire planning scheme road hierarchy classifies Artillery Road as a Rural Minor Collector and Dairy Inn Road as a Rural Arterial. Measured seal width of Dairy Inn Road (6.5m) does not conform to CMDG Rural Arterial Road width (10m) requirements.
- Review of the road crash data supplied by DTMR on the Rockhampton-Yeppoon Road from 01 January 2008 to 30 September 2018, identified 16 crashes within 500m each side of the intersection. Of the 16 crashes, 10 appear to be driver error (i.e. asleep at wheel, kangaroos etc.).
- A fatal crash has occurred on Sunday, 24 March 2019 at 9am at the intersection of Rockhampton-Yeppoon Road and Artillery Road involving 2 cars on a clear weather day. No other data of this crash was available at the time of the review.

### Inspection

An inspection was carried out and the following is noted:

- Day time site inspection of the subject section was carried out on Monday, 04 November 2019. Weather was slightly overcast;
- Movements in and out of Artillery Road appear to be predominantly left out to Yeppoon and right in to Barmoya.
- Movements in and out of Dairy Inn Road appear to be predominantly left out to Rockhampton and right in to Cawarral.
- Visibility of north and south bound traffic for vehicles on Dairy Inn Road approaching the intersection is decent (approx. 250m).
- Visibility of north bound traffic for vehicles on Artillery Road approaching the intersection is decent (approx. 250m). Visibility of south bound traffic is limited (approx. 230m).
- Inspection team travelled approximately 3km on Artillery Road and Dairy Inn Road from the intersection of Rockhampton-Yeppoon Road and did not witness any speed restriction signs on either side of the roads. Therefore a 100km/h speed limit shall apply since the roads are not in a built-up area.
- The intersection is controlled by a give way treatment. Reduce Speed, Give Way Sign Ahead and “Slow Down + Give Way Sign Ahead” (digital) signs were installed on Artillery Road and Dairy Inn Road on approach to the intersection at approximately 520m, 220m and 120m respectively away from the intersection.
- Road lighting is provided at this intersection.

**Identified Deficiencies/Adverse or Hazardous Issues:**

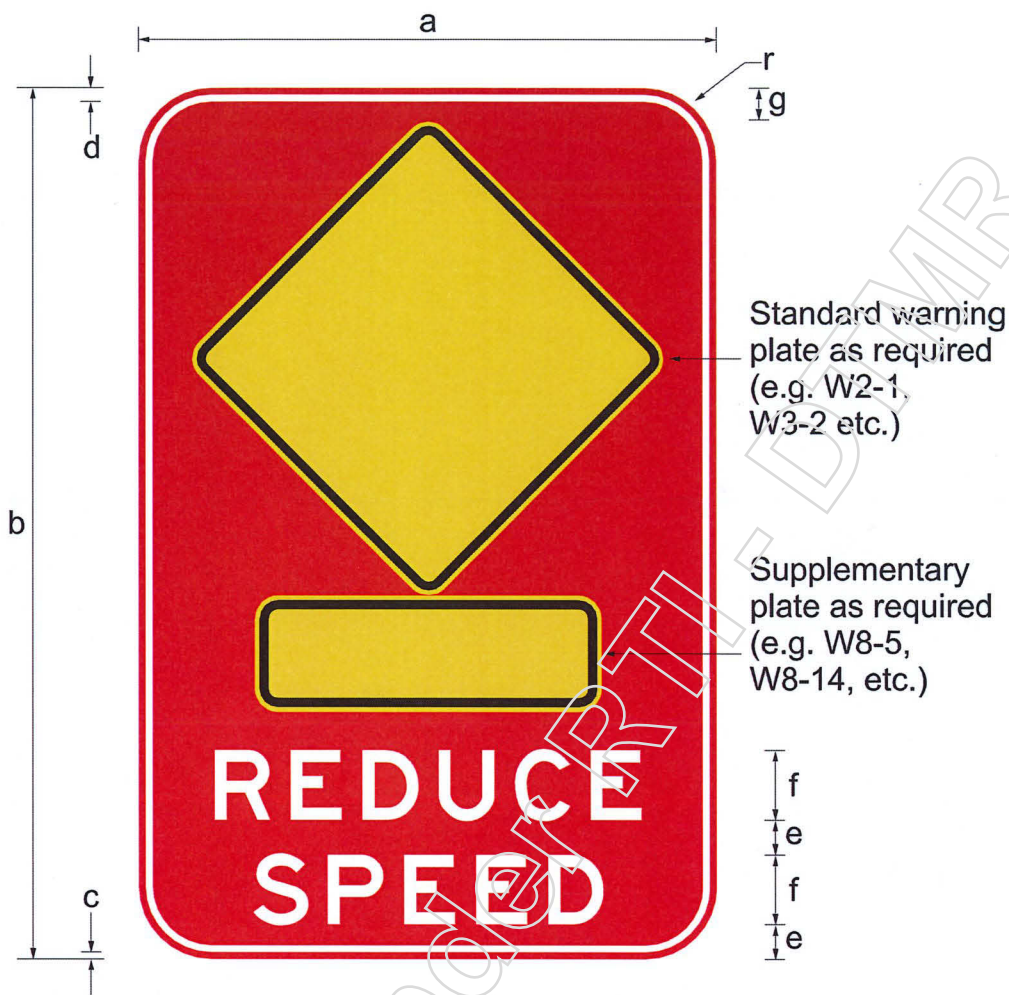
- The presence of overtaking lanes immediately south of Dairy Inn Road and approximately 300m north appears to increase vehicle speeds through the intersection as motorists accelerate to prepare to overtake.
- The left out movement from Dairy Inn Road was a relatively heavy movement during inspection, assumedly am peak traffic travelling towards Rockhampton. Vehicles making this movement were witnessed accepting very small gaps, and or not giving way and attempting to enter traffic flow when no gap was present (i.e. utilising the taper as a merge lane). Several instances were witnessed where vehicles heading south on Rockhampton-Yeppoon Road utilised the overtaking to avoid traffic merging from Dairy Inn Road.
- Loose material was noted at both Artillery Road and Dairy Inn Road intersections with Rockhampton-Yeppoon Road.
- A local truck driver stopped to inform the inspection team of the visibility difficulties due to obstructing signage (Overtaking Lane, 300m Ahead) located at north western side of Rockhampton-Yeppoon Road for vehicles attempting to exit Artillery Road.
- A blind spot is created when vehicles are stored at same time on right turning lanes on Rockhampton-Yeppoon Road at both approaches to the intersection. Visibility of approaching through vehicles for a vehicle turning right from Rockhampton-Yeppoon Road is blinded by opposite right turning vehicle stored in the right turn lane. This information is based on anecdotal evidence and not witnessed onsite.

**Recommendations & Alternatives (Options)**

- Install 'B' size Give Way sign on target boards at the intersection.
- Relocate "Slow Down + Give Way Ahead" digital sign 200m away from Rockhampton-Yeppoon Road intersection on both Artillery Road and Dairy Inn Road. This would satisfy the requirement for the longitudinal placement of a sign for a 100km/h (85<sup>th</sup> percentile speed) speed environment (Clause 1.12.2, MUTCD, Part 1).
- Install 'B' size special advance warning sign (TC1308\_1) on both sides of Artillery Road and Dairy Inn Road. Insert Give Way Sign Ahead (W3-2) and 300m (W8-5) distance plate on the TC sign.
- Consider installing rumble strips on both Artillery Road and Dairy Inn Road (refer clause 6.8 Rumble Strips, Guide to Traffic Management Part 10, Austroads 2019).
- Sweep loose material at the intersection.
- Consider relocating "Overtaking Lane, 300m Ahead" sign located at north western side of Rockhampton-Yeppoon Road 200m forward and change the sign panel to read "Overtaking Lane, 500m Ahead". Also place this sign 5m from edge of travel path to avoid clashing with the intersection direction signs (Cawarral and Barmoya) located before passing Artillery Road intersection travelling towards Yeppoon.

**Discussion**

Clause 2.5.4, MUTCD, Part 2 states sight distance conditions for determining whether a STOP sign is required on any approach to an intersection. This clause also specifies where poor sight distance is not a factor, use of STOP sign can lead to driver disobedience and lack of credibility of STOP signs and therefore no crash warrant is given for the use of STOP signs. Applying the STOP treatment at this intersection would enforce the motorists to stop at the intersection and obey the give way rules. If STOP treatment is applied to this intersection, motorist behavior should be monitored during peak times. STOP signs should replace all Give Way and Give Way Sign Ahead signs in the above recommendation if STOP treatment is applied.



	a	b	c	d	e	f	g	r
TC1308_1A	1000	1500	12	24	60	120EM	55	125
TC1308_1B	1200	1900	15	30	75	150EM	100	150
TC1308_1C	1500	2300	18	36	90	180EM	130	175
TC1308_1D	2000	3000	24	48	120	240EM	110	250

**Colour Legend**

- Retroreflective red (background)
- Retroreflective white (legend and border)
- Retroreflective yellow (warning sign background)
- Black (warning sign symbol and border)

**Notes**

1. TC1308\_4A Illustrated.



**Queensland  
Government**

**SPECIAL WARNING SIGN  
"REDUCE SPEED"**

Department of Transport and Main Roads  
Infrastructure Management & Delivery Division

APPROVED AS OFFICIAL TRAFFIC SIGN

**TC1308\_1**

Page 1 of 4

Designed  
RH 04/01

Checked  
KB 02/02

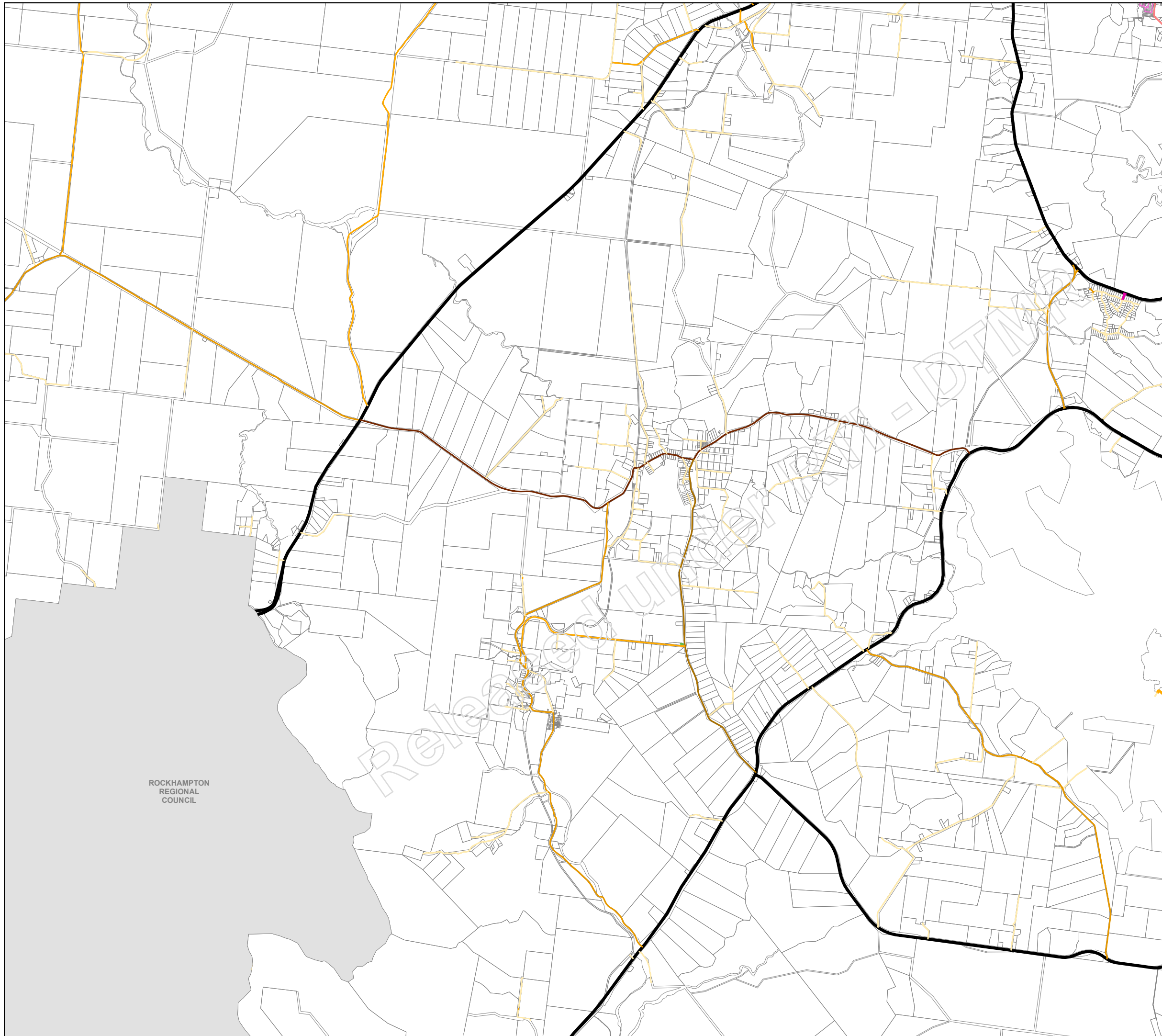
Scale  
Not to Scale

NR  
PRINCIPAL ENGINEER  
(Traffic Engineering)

04/11/15  
Date

A B C D E





ROCKHAMPTON  
REGIONAL  
COUNCIL

# Legend

## Road Hierarchy 2016

- State-controlled road
- Urban Arterial
- Rural Arterial
- Urban Sub-Arterial
- Rural Sub-Arterial
- Urban Major Collector
- Rural Major Collector
- Industrial Collector
- CBD Collector
- Urban Minor Collector
- Rural Minor Collector
- Urban Access Street
- CBD Access
- Industrial Access
- Urban Access Place
- Rural Access
- Urban busway, car park, shared pathway et cetera
- Private

## Cadastre

- Cadastre

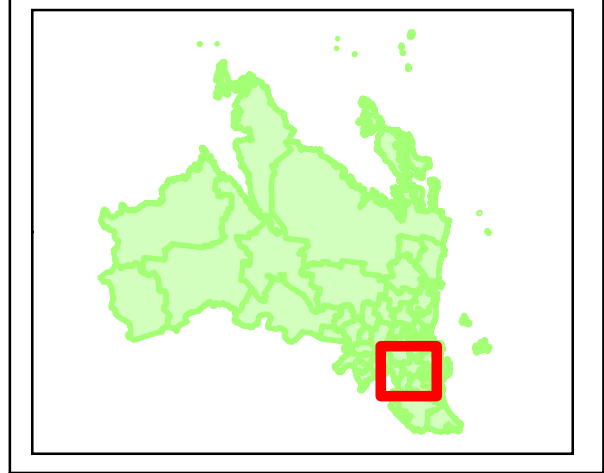
N



Scale @ A3  
1:75,000



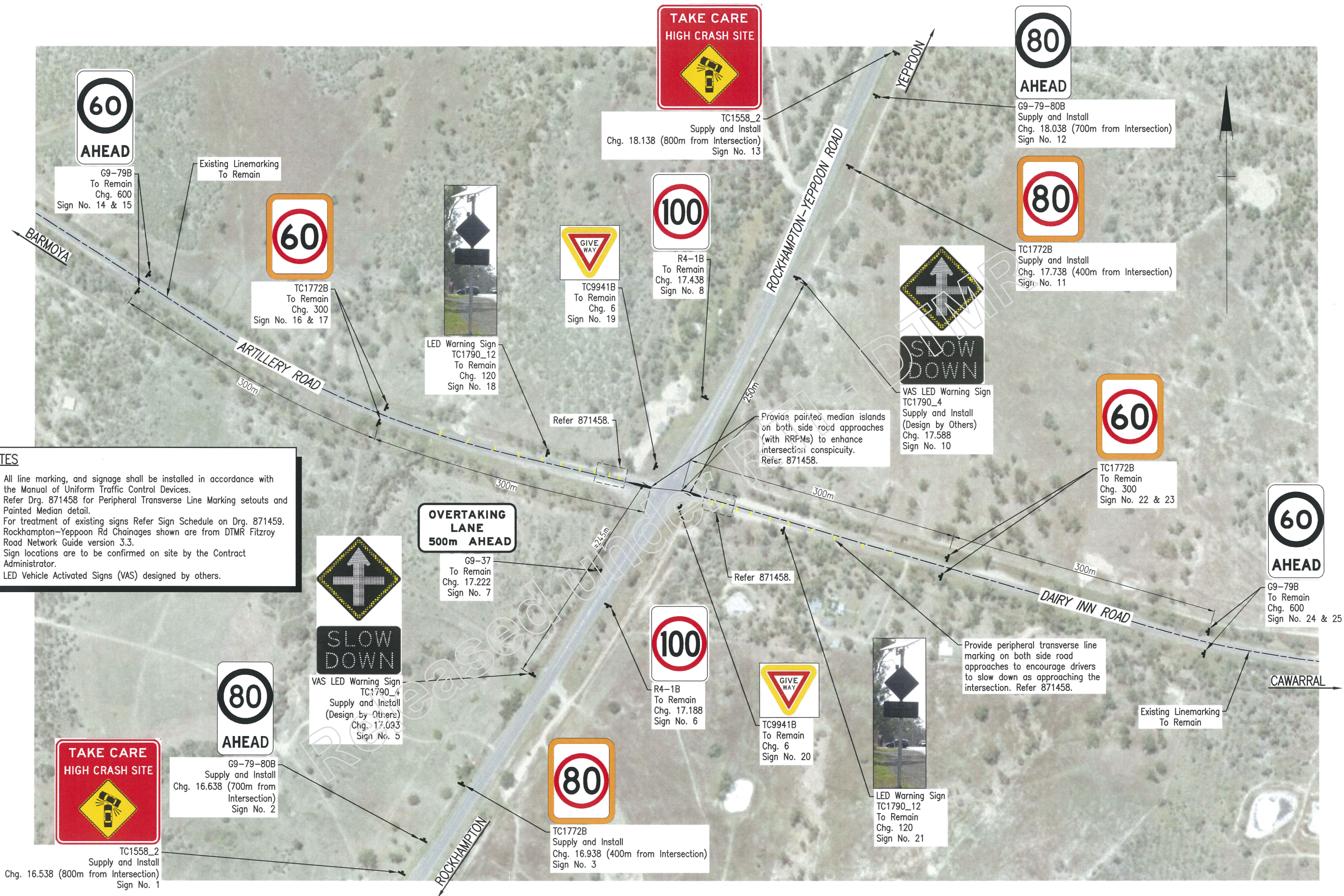
Copyright protects this publication. Reproduction by whatever means is prohibited without prior written permission of the Chief Executive Officer, Livingstone Shire Council. Livingstone Shire Council will not be held liable under any circumstances in connection with or arising out of the use of this data nor does it warrant that the data is error free. Any queries should be directed to the Customer Service Centre, Livingstone Shire Council or telephone 1300 790 919. The Digital Cadastral Data Base is current as at June 2016, copyright the State Government of Queensland 2016. All other data copyright the Livingstone Shire Council



## Overlay Map OM20-11 Road Hierarchy

Version 1



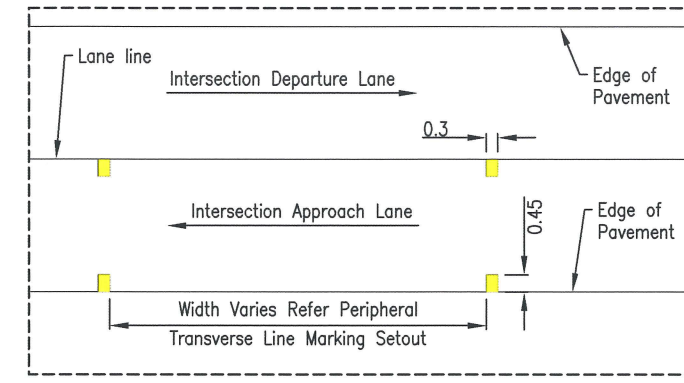
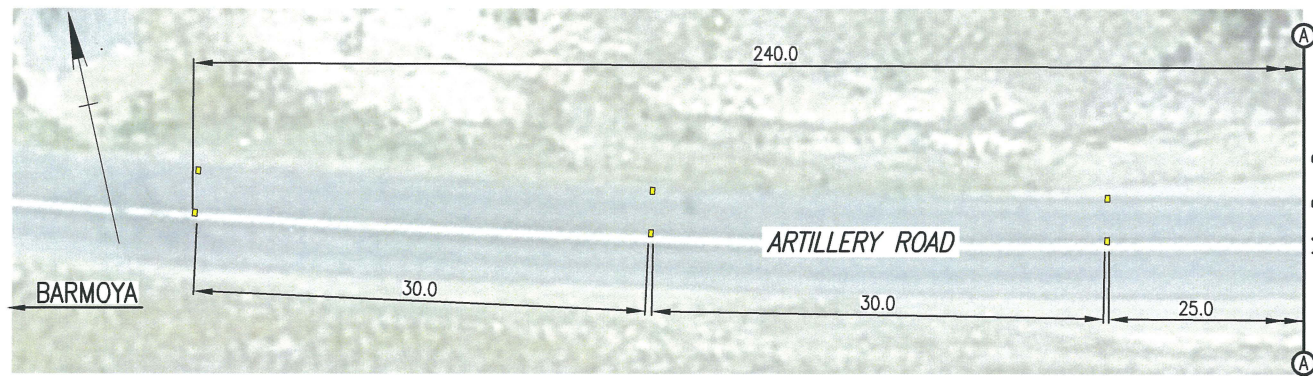


**NOTES**

- All line marking, and signage shall be installed in accordance with the Manual of Uniform Traffic Control Devices.
- Refer Drg. 871458 for Peripheral Transverse Line Marking setouts and Painted Median detail.
- For treatment of existing signs Refer Sign Schedule on Drg. 871459.
- Rockhampton-Yeppoon Rd Chainages shown are from DTMR Fitzroy Road Network Guide version 3.3.
- Sign locations are to be confirmed on site by the Contract Administrator.
- LED Vehicle Activated Signs (VAS) designed by others.

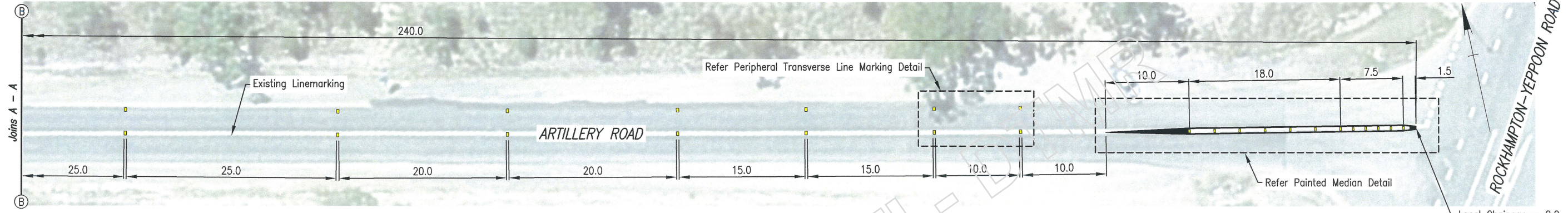
Last Modified: 18-Jan-2021 12:38pm XREFS: X\_DRN\_196.dwg

Associated Job Nos		Survey Data		Scales		LIVINGSTONE SHIRE COUNCIL		ROCKHAMPTON - YEPPOON RD, ARTILLERY RD &				
Auxiliary Drg Nos		Horiz. Datum		0 20 40 60 80m		196 ROCKHAMPTON - YEPPOON ROAD		DAIRY INN RD INTERSECTION				
		Horiz. Grid				CTL CHGE 16.4 - 18.2		SIGNAGE AND LINEMARKING				Contract No.
		Height Datum				Reference Points		Drawn		ENGINEERING CERTIFICATION (RPEQ)		Drawing No. 871457 A
		Survey Books		Dimensions shown in metres except where shown otherwise		Preceding RP Dist. to start of job (km)		R. Newton		ENG. AREA NAME SIGNATURE No. DATE		Series Number SL-01 of 02
						From start to end of job		Designed		Civil Laxman Gopali NR 14625 22/01/2024		
						From end to Following RP		G. Mctier				
						Following RP						



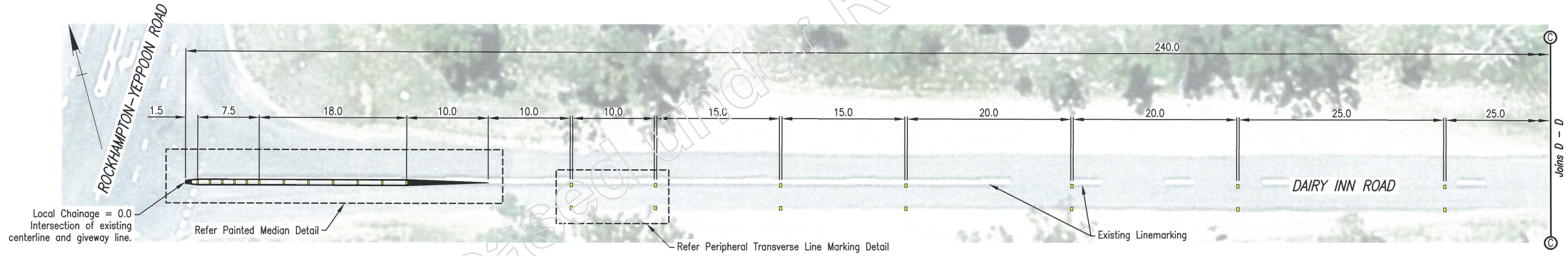
- NOTES**
1. Refer Manual of Uniform Traffic Control Devices (MUTCD) Part 2 Figure 4.1 median details.
  2. Refer Drg. 871457 for Signage Layout.
  3. Refer Drg. 871459 for Sign Schedule.

**PERIPHERAL TRANSVERSE LINE MARKING DETAIL**

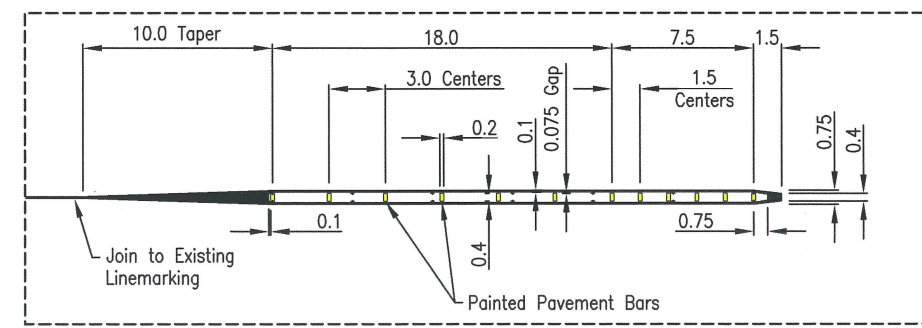


**ARTILLERY ROAD - PERIPHERAL TRANSVERSE LINE MARKING SETOUT**

Local Chainage = 0.0  
Intersection of existing centerline and giveway line.



**DAIRY INN ROAD - PERIPHERAL TRANSVERSE LINE MARKING SETOUT**



**PAINTED MEDIAN DETAIL**

Last Modified: 16 Jan 18, 2021 - 12:39pm XREFS: X:\DRL\196.dwg

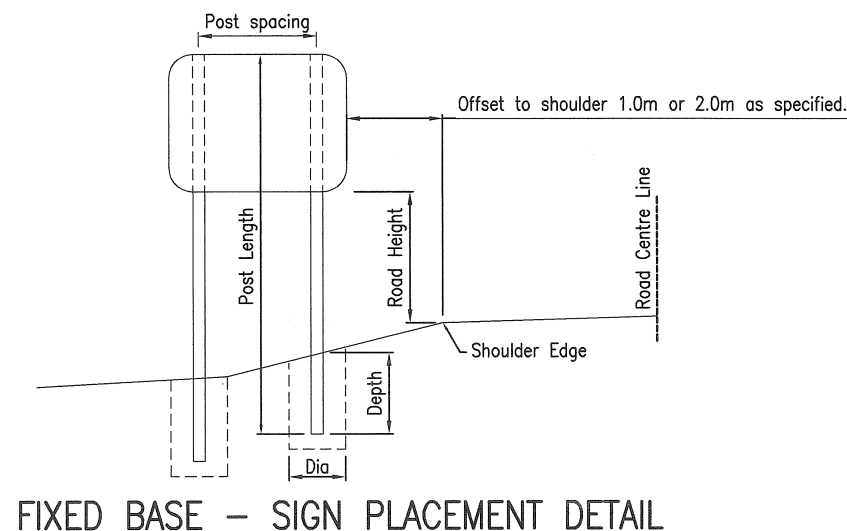
G		Associated Job Nos		Survey Data		Scales		LIVINGSTONE SHIRE COUNCIL				ROCKHAMPTON - YEPPON RD, ARTILLERY RD &				Queensland Government	
F				Horiz. Datum		0 2 4 6 8 10m		196 ROCKHAMPTON - YEPPON ROAD				DAIRY INN RD INTERSECTION					
E				Horiz. Grid		SETOUTS		CTL CHGE 16.4 - 18.2				SIGNAGE AND LINEMARKING				Job No. 276/196/394926	
D		Auxiliary Drg Nos		Height Datum		0 2 4 6 8m		Reference Points				ENGINEERING CERTIFICATION (RPEQ)				Contract No.	
C				Survey Books		DETAILS		Preceding RP Dist. to start of job (km)				Drawn R. Newton				14625	
B						Dimensions shown in metres except where shown otherwise		From start to end of job				Designed G. Mctier				22/02/2021	
A		Issued For Construction						From end to Following RP				Civil Laxman Gopali				871458 A	
		Revisions/Descriptions		Name or RPEQ No.		Signature		Following RP				NR				Series Number SL-02 of 02	

### SIGNAGE SCHEDULE

SIGN NO.	LHS OR RHS	EXISTING OR NEW	SIGN DESCRIPTION	SIGN TYPE	WORK DESCRIPTION	SIGN DETAILS						STIFFENER DETAILS			SUPPORT DETAILS			NEW SUPPORT DETAILS			NEW FOOTING DETAILS						
						Alignment	Chainage (m)	Sign Face Width (mm)	Sign Face Height (mm)	Area (m <sup>2</sup> )	Offset (from Shoulder Hinge) (m)	Height above Road Surface (m)	Type	No.	Spacing (mm)	No. of Brackets	No.	Spacing (mm)	Dimension NB (mm)	Material	(Kerb) Post 1 Length (mm)	Post 2 Length (mm)	Post 3 Length (mm)	Sleeve Length (mm)	Sleeve Diam. (mm)	Hole Diam. (mm)	Hole Depth (mm)
ROCKHAMPTON - YEPPOON ROAD																											
1	LHS	NEW	TAKE CARE HIGH CRASH SITE - TC1558_2	TC	SUPPLY AND INSTALL	DRN	16.538	1800	1800	3.24	2.0	2.1	1	5	400	10	2	1440	90	FLUTED ALUMINIUM	4740	5579	-	725	102	450	750
2	LHS	NEW	80 SPEED LIMIT AHEAD - G9-79-80B	GUIDE	SUPPLY AND INSTALL	DRN	16.638	600	1000	0.6	1.0	1.8	-	-	-	-	1	-	65	FLUTED ALUMINIUM	3567	-	-	675	76	400	700
3	LHS	NEW	80 SPEED LIMIT - TC1772B	TC	SUPPLY AND INSTALL	DRN	16.938	800	1000	0.8	1.0	1.8	-	-	-	-	1	-	65	FLUTED ALUMINIUM	3478	-	-	675	76	400	700
4	LHS	EXISTING	CROSS ROAD - W2-1C	WARNING	REMOVE AND SALVAGE	DRN	17.093	900	900	0.81																	
5	LHS	NEW	LED WARNING SIGN "CROSS ROAD" & "SLOW DOWN" - TC1790_4	TC	SUPPLY AND INSTALL	DRN	17.093	LED VEHICLE ACTIVATED SIGN (VAS) DESIGNED BY OTHERS																			
6	RHS	EXISTING	100 SPEED LIMIT - R4-1B	REGULATORY	TO REMAIN	DRN	17.188																				
7	LHS	EXISTING	OVERTAKING LANE 500M AHEAD - G9-37	GUIDE	TO REMAIN	DRN	17.222																				
8	LHS	EXISTING	100 SPEED LIMIT - R4-1B	REGULATORY	TO REMAIN	DRN	17.438																				
9	RHS	EXISTING	CROSS ROAD - W2-1C	WARNING	REMOVE AND SALVAGE	DRN	17.588	900	900	0.81																	
10	RHS	NEW	LED WARNING SIGN "CROSS ROAD" & "SLOW DOWN" - TC1790_4	TC	SUPPLY AND INSTALL	DRN	17.588	LED VEHICLE ACTIVATED SIGN (VAS) DESIGNED BY OTHERS																			
11	RHS	NEW	80 SPEED LIMIT - TC1772B	TC	SUPPLY AND INSTALL	DRN	17.738	800	1000	0.8	1.0	1.8	-	-	-	-	1	-	65	FLUTED ALUMINIUM	3591	-	-	675	76	400	700
12	RHS	NEW	80 SPEED LIMIT AHEAD - G9-79-80B	GUIDE	SUPPLY AND INSTALL	DRN	18.038	600	1000	0.6	1.0	1.8	-	-	-	-	1	-	65	FLUTED ALUMINIUM	3551	-	-	675	76	400	700
13	RHS	NEW	TAKE CARE HIGH CRASH SITE - TC1558_2	TC	SUPPLY AND INSTALL	DRN	18.138	1800	1800	3.24	2.0	2.1	1	5	400	10	2	1440	90	FLUTED ALUMINIUM	4905	5021	-	725	102	450	750
ARTILLERY ROAD																											
14	LHS	EXISTING	60 SPEED LIMIT AHEAD - G9-79-60B	GUIDE	TO REMAIN	LOCAL	600																				
15	RHS	EXISTING	60 SPEED LIMIT AHEAD - G9-79-60B	GUIDE	TO REMAIN	LOCAL	600																				
16	LHS	EXISTING	60 SPEED LIMIT - TC1772B	TC	TO REMAIN	LOCAL	300																				
17	RHS	EXISTING	60 SPEED LIMIT - TC1772B	TC	TO REMAIN	LOCAL	300																				
18	RHS	EXISTING	LED WARNING SIGN - "GIVE WAY AHEAD" - TC1790_12	TC	TO REMAIN	LOCAL	120																				
19	RHS	EXISTING	GIVE WAY - TC9941B	TC	TO REMAIN	LOCAL	6																				
DAIRY INN ROAD																											
20	RHS	EXISTING	GIVE WAY - TC9941B	TC	TO REMAIN	LOCAL	6																				
21	RHS	EXISTING	LED WARNING SIGN - "GIVE WAY AHEAD" - TC1790_12	TC	TO REMAIN	LOCAL	120																				
22	LHS	EXISTING	60 SPEED LIMIT - TC1772B	TC	TO REMAIN	LOCAL	300																				
23	RHS	EXISTING	60 SPEED LIMIT - TC1772B	TC	TO REMAIN	LOCAL	300																				
24	LHS	EXISTING	60 SPEED LIMIT AHEAD - G9-79-60B	GUIDE	TO REMAIN	LOCAL	600																				
25	RHS	EXISTING	60 SPEED LIMIT AHEAD - G9-79-60B	GUIDE	TO REMAIN	LOCAL	600																				

**NOTES**

- It is the responsibility of the contractor to determine whether a sign needs to be removed while construction takes place.
- Refer Standard Drawings
  - ~ 1363 Traffic Sign - Multiple Traffic Signs Support
  - ~ 1364 Traffic Sign - Connection Straps and Erection Cleat Details
  - ~ 1368 Traffic Sign - Single Traffic Sign Support
  - ~ 1369 Traffic Sign - Details of Sign Stiffening Extrusion
- All aluminum posts to be installed with approved restraint device with top of socket 50mm - 100mm above finished surface and not less than 28MPa (wet) concrete is to be used in footing as per manufacturer's specifications.
- Refer Drg. 871457 for signage layout.

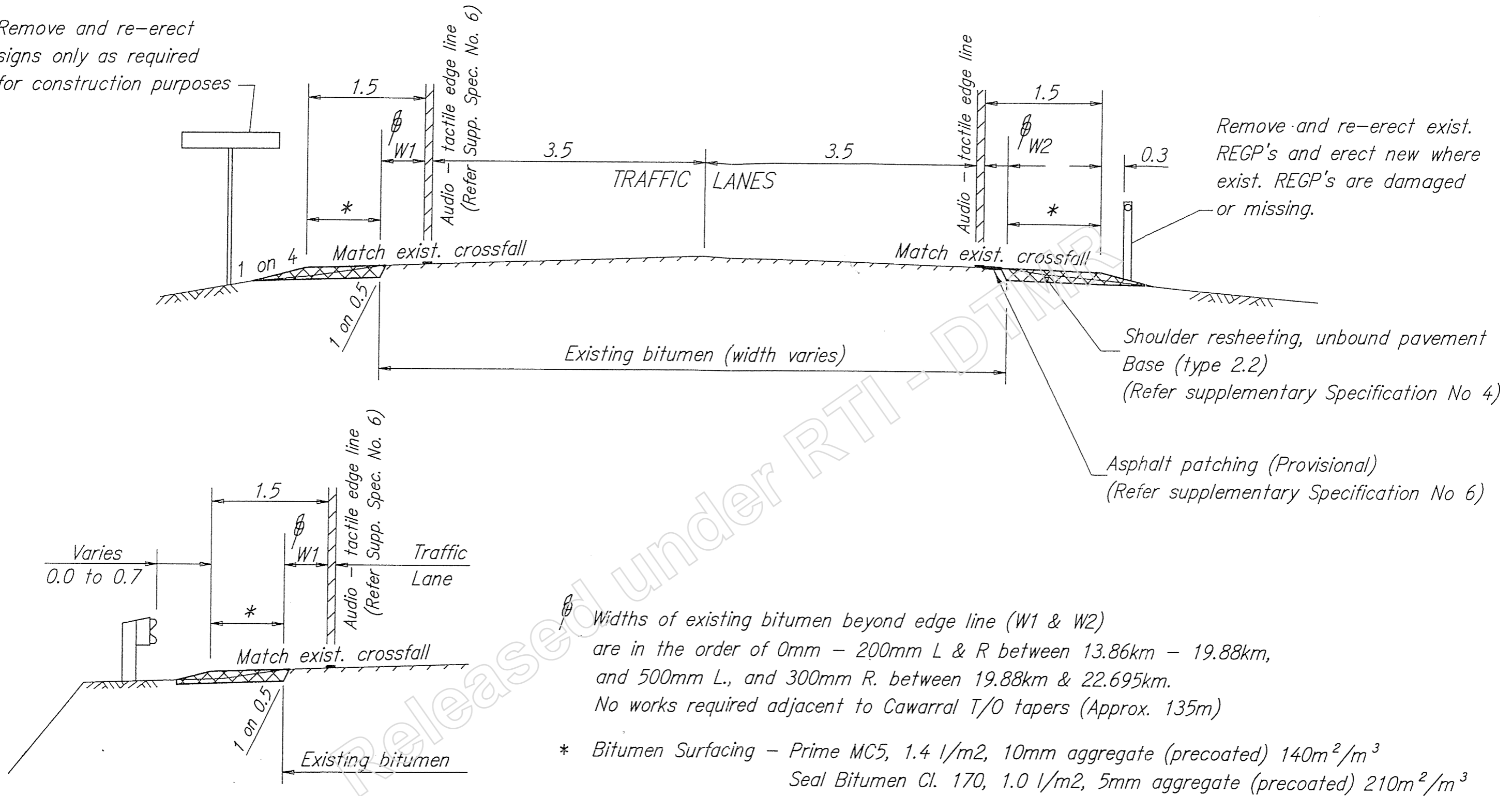


Last Modified: Jan 18, 2021 - 12:40pm XREFS :-

G		Associated Job Nos		Survey Data		Scales		LIVINGSTONE SHIRE COUNCIL				ROCKHAMPTON - YEPPOON RD, ARTILLERY RD &				Queensland Government	
F				Horiz. Datum		NTS		196 ROCKHAMPTON - YEPPOON ROAD				DAIRY INN RD INTERSECTION					
E				Auxiliary Drg Nos				CTL CHGE 16.4 - 18.2				SIGN SCHEDULE				Job No. 276/196/394926	
D				Horiz. Grid				Reference Points				ENGINEERING CERTIFICATION (RPEQ)				Contract No. 871459 A	
C				Height Datum				Preceding RP Dist. to start of job (km) From start to end of job From end to Following RP Following RP				Drawn R. Newton				No. DATE	
B				Survey Books		Dimensions shown in metres except where shown otherwise						Designed G. Mctier				14625 22/01/2021	
A		Issued For Construction		Name or RPEQ No. Signature Date								ENG. AREA NAME SIGNATURE				Series Number SF-01 of 01	

Remove and re-erect signs only as required for construction purposes

Remove and re-erect exist. REGP's and erect new where exist. REGP's are damaged or missing.



Widths of existing bitumen beyond edge line (W1 & W2) are in the order of 0mm - 200mm L & R between 13.86km - 19.88km, and 500mm L, and 300mm R. between 19.88km & 22.695km. No works required adjacent to Cawarral T/O tapers (Approx. 135m)

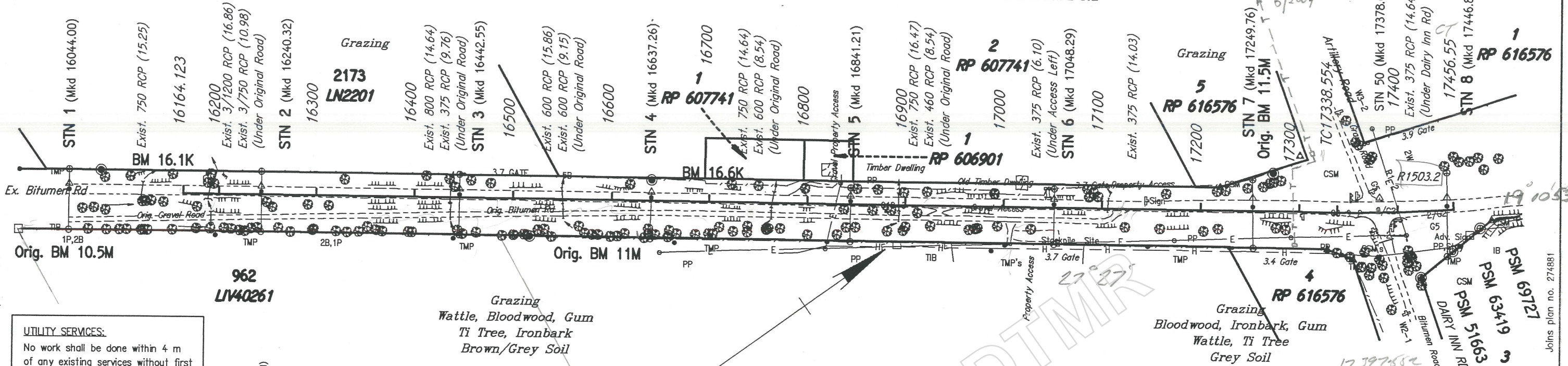
\* Bitumen Surfacing - Prime MC5, 1.4 l/m<sup>2</sup>, 10mm aggregate (precoated) 140m<sup>2</sup>/m<sup>3</sup>  
Seal Bitumen Cl. 170, 1.0 l/m<sup>2</sup>, 5mm aggregate (precoated) 210m<sup>2</sup>/m<sup>3</sup>

TREATMENT ADJACENT TO GUARDRAIL

**TYPE CROSS SECTION**

CAD DWG: JWB/KKE VIEWS: 2	Revisions	Certified	Date	Microfiled	Associated job nos	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	Survey books	LIVINGSTONE SHIRE				TYPE CROSS SECTION									
						Auxiliary plan nos	Through chainage from Rockhampton 18.3km	ROCKHAMPTON - YEPPOON ROAD				No. 1 of 1 plans									
						Scales 0 0.5 1.0 1.5 2.0 SCALE 1:50 m	Reference Points	Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Ckd	Drawn JWN	Design JWN		Examined RGC	Certified NR	Appr NR	JOB No. 77/196/722	Plan No. 274607
						(Office use only)	196/5	1.19	8.835	2.195	196/7				5/17/96		RPEQ 26005 P/C	10/11/96	Manager (I.D.) Central District		

# Parish of Cawarral



**UTILITY SERVICES:**  
No work shall be done within 4 m of any existing services without first consulting the relevant authority. Refer Supp. Condition of Contract. "Utility Services"

Existing signs affected by the construction are to be removed & re-erected. All other signs are to remain.

**RTCS (CENTRAL)**  
Design Services - Rockhampton  
Building for Transport

16164.174	17457.014	175.0 Taper RHS	Overtaking Lane RHS	Existing Road (Control Line No. 2)	Intersection RHS
Unbound Pavement Base (Type 2.1) (150mm x Widening RHS only) Unbound Pavement Subbase (Type 2.3) (125mm x Widening RHS only) Unbound Pavement Subbase (Type 2.5) (125mm x Widening RHS only) Bitumen Surfacing - 12.0 (10.5 + 1.5 shoulder RHS nom.)					

Pvt mkg		Refer to Plan Nos. 274912 & 274914																																																																	
Type		A																																																																	
Earthwork	Cut	42.0	201.8	633.1	476.7	358.9	1122.2	275.7	159.1	546.3	840.3	416.2	452.3	193.8	118.5																																																				
	Fill	0.4	2.1	0	0.5	46.1	0	6.5	53.6	8.1	0	0	0	81.9	0																																																				
Grade Height		58.824	58.772	58.703	58.670	58.668	58.708	58.784	58.907	59.080	59.263	59.420	59.622	59.841	60.030	60.248	60.447	60.640	60.856	61.030	61.205	61.345	61.425	61.446	61.402	61.335	61.232	61.098	60.939	60.742	60.554	60.354	60.119	59.906	59.702	59.462	59.276	59.084	58.880	58.669	58.470	58.320	58.116	57.907	57.696	57.528	57.341	57.125	56.879	56.549	56.125	55.639	55.090	54.529	53.907	53.254	52.666	52.118	51.518	50.923	50.301	49.732	49.189	48.687	48.239	47.854	47.516
Surface Height		58.824	58.772	58.703	58.670	58.668	58.708	58.784	58.907	59.080	59.263	59.420	59.622	59.841	60.030	60.248	60.447	60.640	60.856	61.030	61.205	61.345	61.425	61.446	61.402	61.335	61.232	61.098	60.939	60.742	60.554	60.354	60.119	59.906	59.702	59.462	59.276	59.084	58.880	58.669	58.470	58.320	58.116	57.907	57.696	57.528	57.341	57.125	56.879	56.549	56.125	55.639	55.090	54.529	53.907	53.254	52.666	52.118	51.518	50.923	50.301	49.732	49.189	48.687	48.239	47.854	47.516
Chainage		16164.174	16180	16200	16220	16240	16260	16280	16300	16320	16340	16360	16380	16400	16420	16440	16460	16480	16500	16520	16540	16560	16580	16600	16620	16640	16660	16680	16700	16720	16740	16760	16780	16800	16820	16840	16860	16880	16900	16920	16940	16960	16980	17000	17020	17040	17060	17080	17100	17120	17140	17160	17180	17200	17220	17240	17260	17280	17300	17320	17340	17360	17380	17400	17420	17440	17457.014
Horiz. curve data		R1503.2														3% Superelevation																																																			

Origin of survey(s) (For information only)  
AHD Correction: -

Revisions	Certified	Date	Microfiled	Associated job nos	Bench Marks				0 20 40 60 Plan & Horiz. Sect.		Through chainage from ROCKHAMPTON 19.2kms - 22.0kms	Survey bks: 196 - 276, 196 - 277	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	LIVINGSTONE SHIRE ROCKHAMPTON - YEPPON ROAD CTL CHGE STN1(MKD16044.00)-STN8(MKD17446.82)				JOB No. 77/196/19	No. 3 of 8 plans Plan No. 274868
A Original issue				274868, 274881, 274909 - 274914	Orig. 10.5M Star Picket 259056.830	16.1K Star Picket 259069.218	11M Star Picket 259372.507	11.5M Brass Plug 259921.647	0 2 4 6 Vertical Section				Approved: NR, NR, NR, NR 28/8/19, RPEQ 5031, 27/8/19						

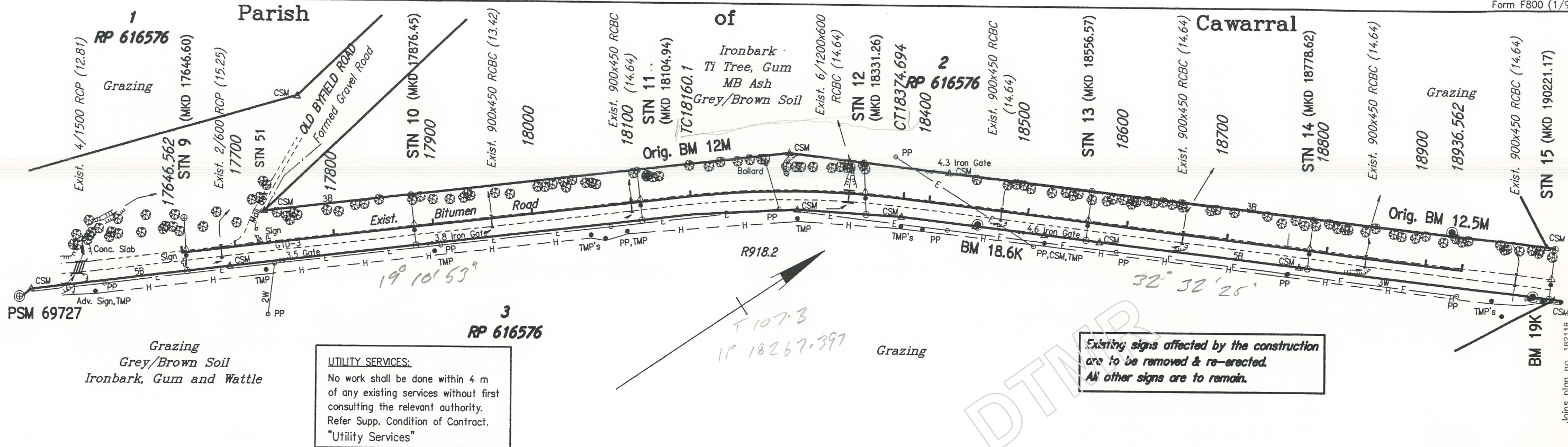
Joins plan no. 274868

PSM 69727

Origin of survey(s)  
(For information only)

AHD Correction: -

Joins plan no. 182118



**UTILITY SERVICES:**  
No work shall be done within 4 m of any existing services without first consulting the relevant authority. Refer Supp. Condition of Contract. "Utility Services"

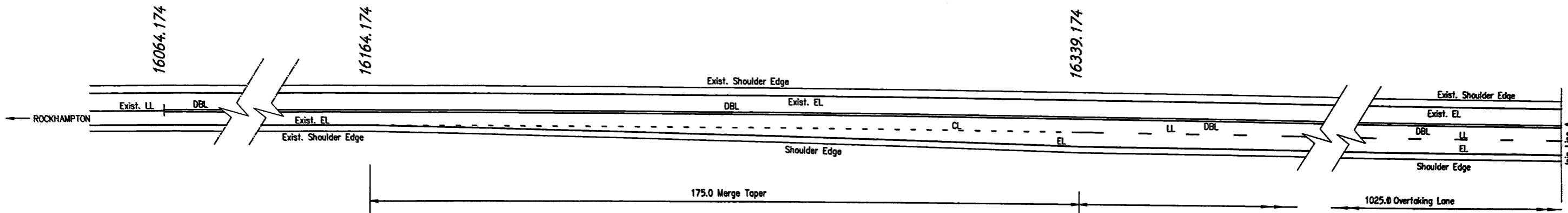
Existing signs affected by the construction are to be removed & re-erected. All other signs are to remain.



4A Exist 2/600 RCP LENGTHEN 1.22 LHS INTERSECTION L		4B Exist 900x450 RCBC LENGTHEN 2.4 LHS		4C Exist 900x450 RCBC LENGTHEN 3.6 LHS		4D Exist 6/1200x600 RCBC LENGTHEN 2.4 LHS		4E Exist 900x450 RCBC LENGTHEN 3.6 LHS		4F Exist 900x450 RCBC LENGTHEN 2.4 LHS		4G Exist 900x450 RCBC LENGTHEN 1.2 LHS	
Unbound Pavement Base (Type 2.1)(150mm x Widening LHS only) Unbound Pavement Subbase (Type 2.3) (125mm x Widening LHS only) Unbound Pavement Subbase (Type 2.5) (2+5mm Widening LHS only) Bitumen Surfacing - 12.0 (10.5 + 1.5 shoulder LHS nom.)													
DATUM 30.000 Pvf mkg Refer Plan Nos. 274913 & 274914 Type B Earthwork Cut: 104.5, 270.3, 211.5, 220.5, 205.7, 142.8, 162.7, 138.2, 158.0, 177.7, 185.7, 249.8, 212.5, 83.9 Fill: 3.2, 0, 0.2, 0.4, 12.1, 109.7, 57.9, 123.1, 24.9, 8.7, 12.3, 0, 9.2, 0 Grade Height: 46.012, 45.930, 45.821, 45.750, 45.682, 45.618, 45.580, 45.510, 45.420, 45.354, 45.266, 45.179, 45.106, 44.970, 44.873, 44.772, 44.688, 44.585, 44.493, 44.392, 44.280, 44.146, 44.024, 43.953, 43.908, 43.823, 43.767, 43.739, 43.665, 43.604, 43.538, 43.479, 43.446, 43.427, 43.437, 43.407, 43.375, 43.326, 43.260, 43.163, 43.050, 42.936, 42.853, 42.784, 42.710, 42.590, 42.490, 42.433, 42.345, 42.242, 42.132, 42.046, 41.958, 41.877, 41.795, 41.700, 41.579, 41.488, 41.435, 41.354, 41.277, 41.162, 41.054, 40.954, 40.888, 40.821 Surface Height: 46.012, 45.930, 45.821, 45.750, 45.682, 45.618, 45.580, 45.510, 45.420, 45.354, 45.266, 45.179, 45.106, 44.970, 44.873, 44.772, 44.688, 44.585, 44.493, 44.392, 44.280, 44.146, 44.024, 43.953, 43.908, 43.823, 43.767, 43.739, 43.665, 43.604, 43.538, 43.479, 43.446, 43.427, 43.437, 43.407, 43.375, 43.326, 43.260, 43.163, 43.050, 42.936, 42.853, 42.784, 42.710, 42.590, 42.490, 42.433, 42.345, 42.242, 42.132, 42.046, 41.958, 41.877, 41.795, 41.700, 41.579, 41.488, 41.435, 41.354, 41.277, 41.162, 41.054, 40.954, 40.888, 40.821 Chainage: 17646.662, 17660, 17680, 17700, 17720, 17740, 17760, 17780, 17800, 17820, 17840, 17860, 17880, 17900, 17920, 17940, 17960, 17980, 18000, 18020, 18040, 18060, 18080, 18100, 18120, 18140, 18160, 18180, 18200, 18220, 18240, 18260, 18280, 18300, 18320, 18340, 18360, 18380, 18400, 18420, 18440, 18460, 18480, 18500, 18520, 18540, 18560, 18580, 18600, 18620, 18640, 18660, 18680, 18700, 18720, 18740, 18760, 18780, 18800, 18820, 18840, 18860, 18880, 18900, 18920, 18936.662 Horiz. curve data: R918.2, 3% Superelevation, CT18374.694													

Revisions	Certified	Date	Microfiled	Associated job nos	Bench Marks				0 20 40 60 Plan & Horiz. Sect.		Through chainage from ROCKHAMPTON 20.7kms - 22.0kms	Survey bks: 196 - 278 196 - 279	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	LIVINGSTONE SHIRE ROCKHAMPTON - YEPPOON ROAD CTL CHGE STN8(MKD17446.82)-STN15(MKD19021.17)		JOB No. 77/196/19	No. 4 of 8 plans Plan No. 274881
A Original issue				274868, 274881 274909 - 274914	B.M. on X Co-ord Y Co-ord Ht.	0 2 4 6 Vertical Section	Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Bays	Drawn	Design	Examined		
					PSM69727 DDR 259915.239 428219.547 47.114								NR	NR	NR	NR	NR
					BM 12M Ironbark 260159.766 428811.360 43.358								NR	NR	NR	NR	NR
					BM 18.4K Star Pkt 260380.690 429063.464 42.556								NR	NR	NR	NR	NR
					BM 12.5M Ironbark 260645.421 429457.243 40.370								NR	NR	NR	NR	NR
					BM 19K Star Pkt 260742.480 429488.963 40.257								NR	NR	NR	NR	NR

Speed Environment km/h 110



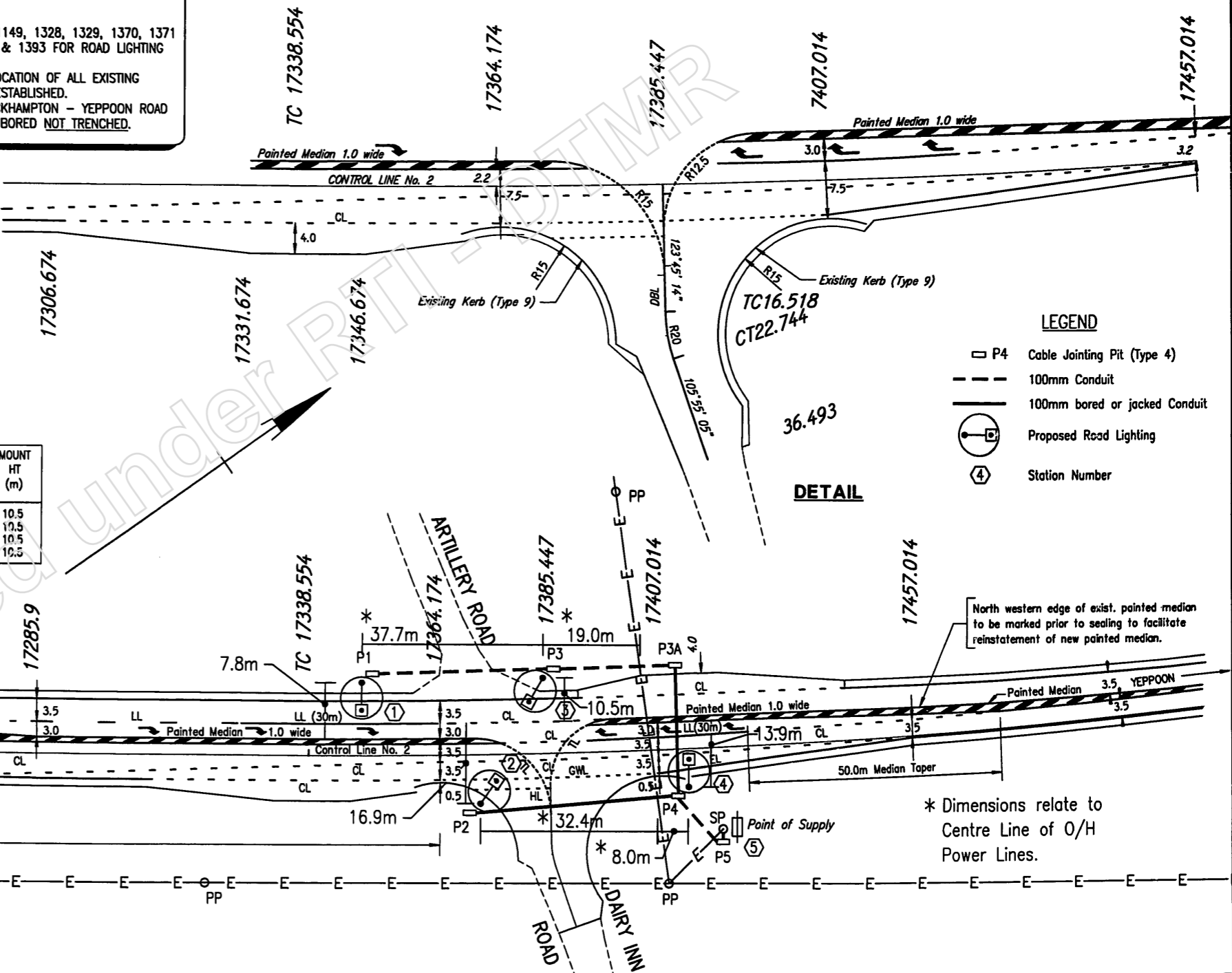
**SCHEDULE OF MAINTENANCE FOR ROAD LIGHTING**

THE ROAD LIGHTING DESIGN BASED ON THE FOLLOWING MAINTENANCE SCHEDULE

- BULK LAMP REPLACEMENT CARRIED OUT AT 36 MONTH INTERVALS. AT THIS TIME THE FOLLOWING SHALL ALSO OCCUR:
  - ALL OPTICAL SURFACES, BOTH INTERNAL AND EXTERNAL, OF THE LUMINAIRE SHALL BE CLEANED.
  - ALL GASKETS SHALL BE CHECKED FOR DETERIORATION AND REPLACED WHERE NECESSARY.
  - DAMAGED, WEATHERED VISORS SHALL BE REPLACED.
  - ALL ACCESSIBLE SCREWS, NUTS, etc SHALL BE REPLACED.
  - A VISUAL CHECK SHALL BE MADE OF THE ELECTRICAL COMPONENTS AND WIRING FOR SIGNS OF OVERHEATING.
  - IF REQUIRED, THE LUMINAIRE SHALL BE REALIGNED OR ADJUSTED TO THE DESIGN SPECIFICATION.
- IT IS RECOMMENDED THAT INSPECTION PATROLS PROVIDE SPOT LAMP REPLACEMENTS SUCH THAT THE MAXIMUM LEVEL OF LUMINAIRE OUTAGES AT ANY ONE TIME IS NOT GREATER THAN 5% OF THE LUMINAIRE POPULATION eg, SERVICE AVAILABILITY SHOULD BE AT LEAST 95% (AS PER THE REQUIREMENTS OF AS 1158.1.3)
- WHERE LUMINAIRES ARE REPLACED THE NEW LUMINAIRE SHALL BE OF EQUIVALENT TYPE SUCH THAT THE INTEGRITY OF THE ROAD LIGHTING DESIGN IS MAINTAINED.

**NOTES:**

- REFER STANDARD DRAWINGS 1149, 1328, 1329, 1370, 1371, 1372, 1380, 1381, 1382, 1392 & 1393 FOR ROAD LIGHTING INSTALLATION REQUIREMENTS.
- PRIOR TO EXCAVATION THE LOCATION OF ALL EXISTING CABLING/CONDUITING IS TO BE ESTABLISHED.
- CONDUITS CROSSING THE ROCKHAMPTON - YEPPOON ROAD AND DAIRY INN ROAD SHALL BE BORED NOT TRENCHED.

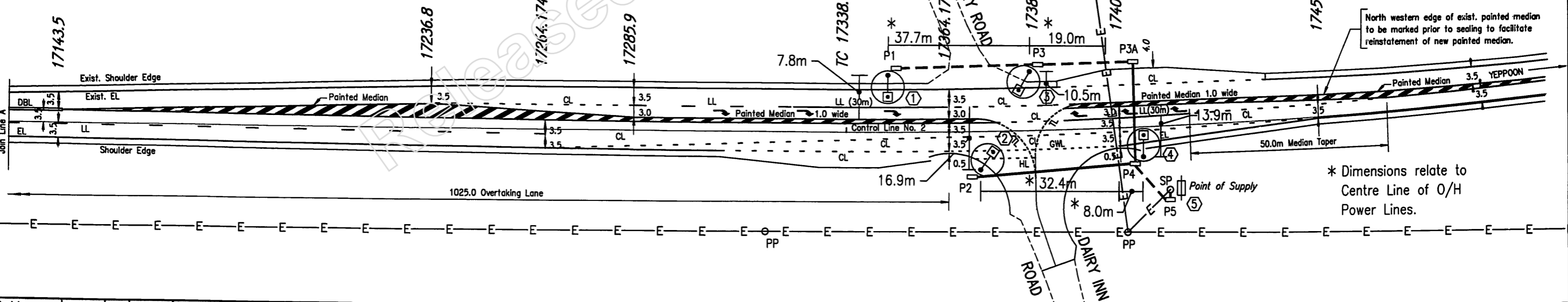


**LEGEND**

- P4 Cable Jointing Pit (Type 4)
- - - 100mm Conduit
- 100mm bored or jacked Conduit
- ⊙ Proposed Road Lighting
- ④ Station Number

**ROAD LIGHTING SCHEDULE**

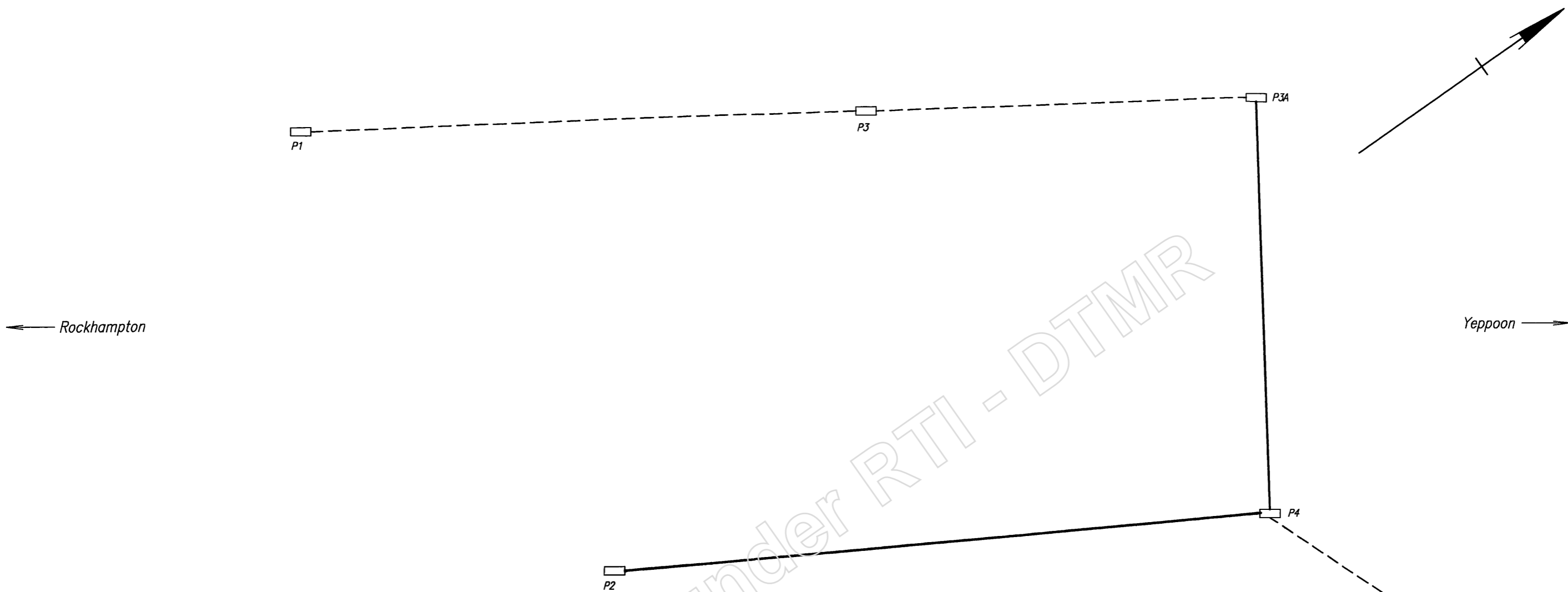
STN No	POLE				LUMINAIRE						OUTREACH BRACKET			MOUNT HT (m)
	CHAINAGE (m)	OFFSET (m)	COMP ID	EXIST	RECOVER		ERECT		IIN	EXIST (m)	ERECT (m)	IIN		
					LUMINAIRE	CUST	LUMINAIRE	CUST						
1	17349	14.5 L	PO1		8.5SB	1392	SL1			DMR	6635	4.5	3440, 2699	10.5
2	17372	9.5 R	PO1		8.5SB	1392	SL1			DMR	6635	4.5	3440, 2699	10.5
3	17385	15.0 L	PO1		8.5SB	1392	SL1			DMR	6635	4.5	3440, 2699	10.5
4	17414	7.5 R	PO1		8.5SB	1392	SL1			DMR	6635	4.5	3440, 2699	10.5



\* Dimensions relate to Centre Line of O/H Power Lines.

Revisions	Certified	Date	Microfiled	Associated job nos	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	Survey books	<b>LIVINGSTONE SHIRE</b>				<b>FLAG LIGHTING AND PAVEMENT MARKING</b>				<b>Queensland Government</b> Department of Main Roads												
				Auxiliary drawing nos	Scales	Through chainage from	<b>ROCKHAMPTON - YEPPOON ROAD</b>									<b>JOB No.</b> 77/196/203 <b>Contract No.</b>				No. 1 of 2 drgs <b>Drawing No.</b> 329724							
				329725	NOT TO SCALE	ROCKHAMPTON	<b>CTL CHGE 16164.174 - 17457.014</b>				<table border="1"> <tr> <td>Preceding RPC</td> <td>Dist. to start of job (km)</td> <td>From start to end of job</td> <td>From end to following RPC</td> <td>Following RPC</td> <td>Survey Ckd</td> <td>Bdys Ckd</td> <td>Drawn BJB Ckd</td> <td>Design BJB Ckd</td> <td>Examined Orig. plan signed by S. Price</td> <td>Certified Orig. plan signed by C. Hewitt</td> <td>Approved Orig. plan signed by T. Hill 5/11/02</td> </tr> </table>				Preceding RPC						Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Ckd	Bdys Ckd	Drawn BJB Ckd
Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Ckd	Bdys Ckd	Drawn BJB Ckd	Design BJB Ckd	Examined Orig. plan signed by S. Price	Certified Orig. plan signed by C. Hewitt					Approved Orig. plan signed by T. Hill 5/11/02												
B As Constructed				(Office use only)		19.2kms - 20.5kms																					





← Rockhampton

Yeppoon →

UNDERGROUND CABLE SCHEDULE

LOCATION	STATIONS FROM TO	VOLTS	IN	CABLE SIZE TYPE	No. CABLES IN CONDUIT	TOTAL LENGTH	REMARKS
Rockhampton - Yeppoon Road (Intersection of Dairy Inn Road & Artillery Road)	P1 - P3	240	X	16mm 2C PVC/PVC	1	35	New Cable
	P3 - P3A	240	X	16mm 2C PVC/PVC	1	23	New Cable
	P3A - P4	240	X	16mm 2C PVC/PVC	1	26	New cable
	P4 - P5	240	X	16mm 2C PVC/PVC	1	13	New Cable
	P2 - P4	240	X	16mm 2C PVC/PVC	1	40	New cable

Circuit Notes

- Maximum Voltage Drop from Supply Point - 5%
- Fault Loop Impedance calculated using the following: 4 sec disconnect time, Type C 25A Circuit Breaker, Ia of 4.5 times rated C/B current
- Installation is to comply with the requirements of AS3000 2000

Point of Supply:

- New low voltage electricity supply on separate service pole, to be located up to 20m from the existing substation pole.
- Ergon to supply and install the service pole, along with associated transformer, aerial service and service fuse.
- Pole mounted switchboard to be installed on Service Pole.

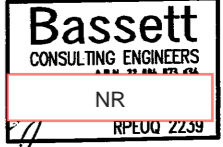
LOCATION	PHASE	CIRCUIT	STATIONS	LOAD	STARTING CURRENT(A)	RUNNING CURRENT(A)	C/B	COMMENTS
Rockhampton - Yeppoon Road (Intersection of Dairy Inn Road & Artillery Road)	A	1L1	1,2,3,4	4x150W	4.4	3.36	25A	Existing S/B New 25A C/B to be installed.

LEGEND

- P12 Cable Joining Pit (Type 4)
- 100mm Conduit
- 100mm bored or jacked conduit

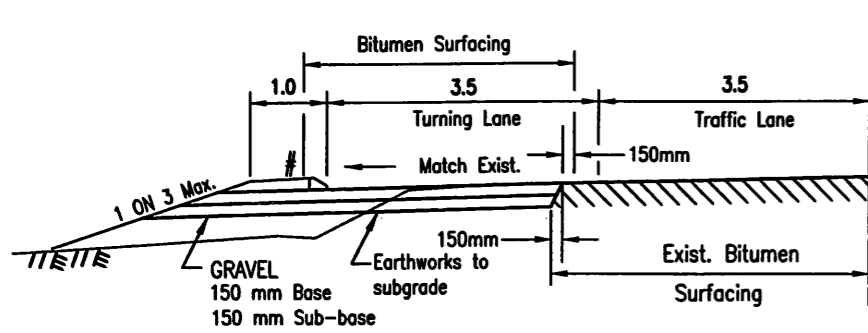
Lighting Notes:

- Luminaires are to be only as specified on these drawings.
- Any possible variations to pole locations considered necessary prior to installation or in the field, are to be checked with the Superintendent.
- Manufacture - GEC Optispan S150C luminaires to have Power Factor of 0.8 or above

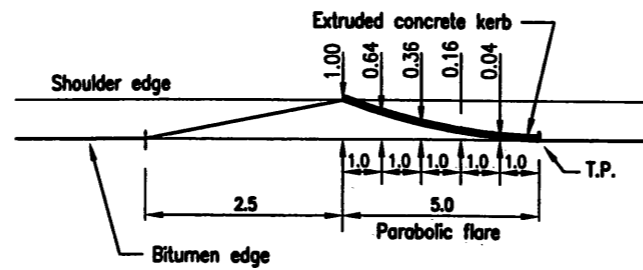


Revisions	Certified	Date	Microfiled	Associated job nos	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	Survey books	<b>LIVINGSTONE SHIRE</b>				<b>CIRCUIT DETAILS</b>				<b>Queensland Government</b> Department of Main Roads				
				Auxiliary drawing nos	Scales	Through chainage from	<b>ROCKHAMPTON - YEPPOON ROAD</b>				<b>FLAG LIGHTING</b>								
				329724	NOT TO SCALE	ROCKHAMPTON 19.2kms - 20.5kms	<b>CTL CHGE 17349 - 17414</b>				<b>CAWARRAL TURNOFF</b>				JOB No. <b>77/196/203</b> Contract No.				
A	Original issue			(Office use only)			Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Ckd	Bdys Ckd	Drawn BJB Ckd		Design BJB Ckd	Examined NR	Certified (See above) Grant Anderson	Approved NR
												RPEQ 2239		District Director Central District		CAD REF: G:\Transport\BJB\329724-329725.dwg			

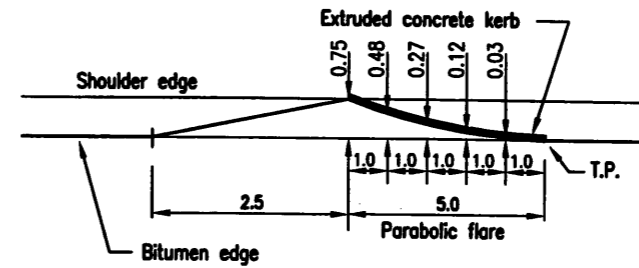
# Refer Kerbing Detail



INTERSECTION WIDENING



\* PARABOLIC FLARE DETAIL



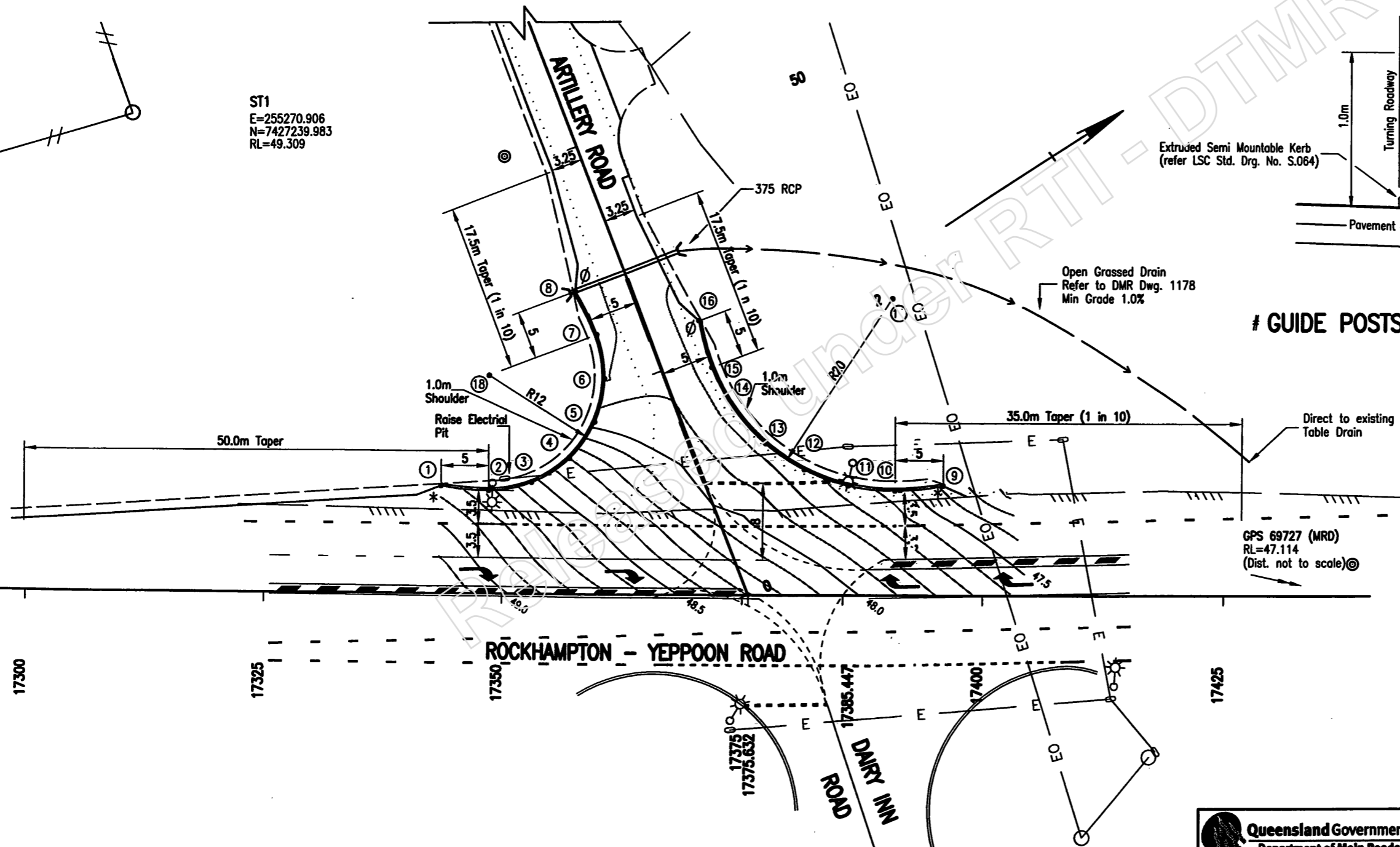
# PARABOLIC FLARE DETAIL

Pt No.	Description	RL
1	Start Flare	49.854**
2	Start R12	49.725**
3		49.534**
4		49.300
5		48.950
6		48.790
7	Start R12	48.740
8	Start Flare	48.720
9	Start Flare	48.380**
10	Start R20	48.556**
11		48.650**
12		48.730
13		48.760
14		48.752
15	Start R20	48.748
16	Start Flare	48.727
17	Centre R20	
18	Centre R12	

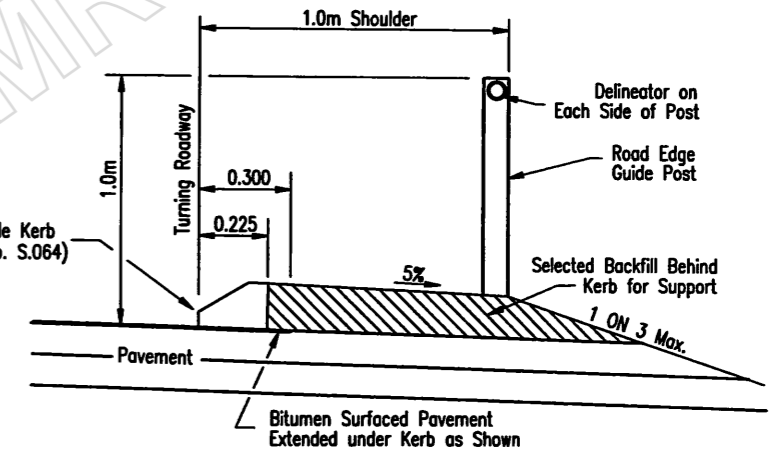
TURNOUT LEVELS

\*\* May be adjusted to match existing crossfall.

ST1  
E=255270.906  
N=7427239.983  
RL=49.309



# GUIDE POSTS IN ASSOCIATION WITH KERBS



TELSTRA CABLE : No work is to be carried out within 3.0m of cable without first consulting Telstra.

NOTE: Survey stations required for setting out of this job are indicated thus (C). Please contact Design Office if removal is necessary.

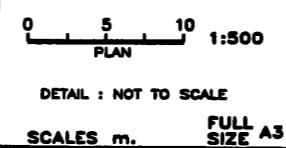
LEGEND

.....	Gravel Edge
-----	Bitumen Edge
-----	Top of Embankment
-----	Toe of Embankment
-----	Underground Electricity
-----	Aboveground Electricity
-----	Fence
-----	Surface Drain
-----	Shoulder Line (new)

Queensland Government Department of Main Roads  
Approved: NR 7/10/03  
District Director Date  
QMR JOB No. / /  
QMR PLAN No. 347355

Mark Description : GPS 69727 (MRD)
Location : Int. Yeppoon Rd & Artillery Rd
Datum : AHD RL : 47.114
Derived from : Form 6 : X
Surveyed :
Title 4/2002 LEVEL DATUM

Field Bk	AMENDMENTS DESCRIPTION	APPROVED	DATE
E/F Bk 1237GDAK			
Level Bk			
D/O File 1237GA	A Original Issue		MF-03/04/04



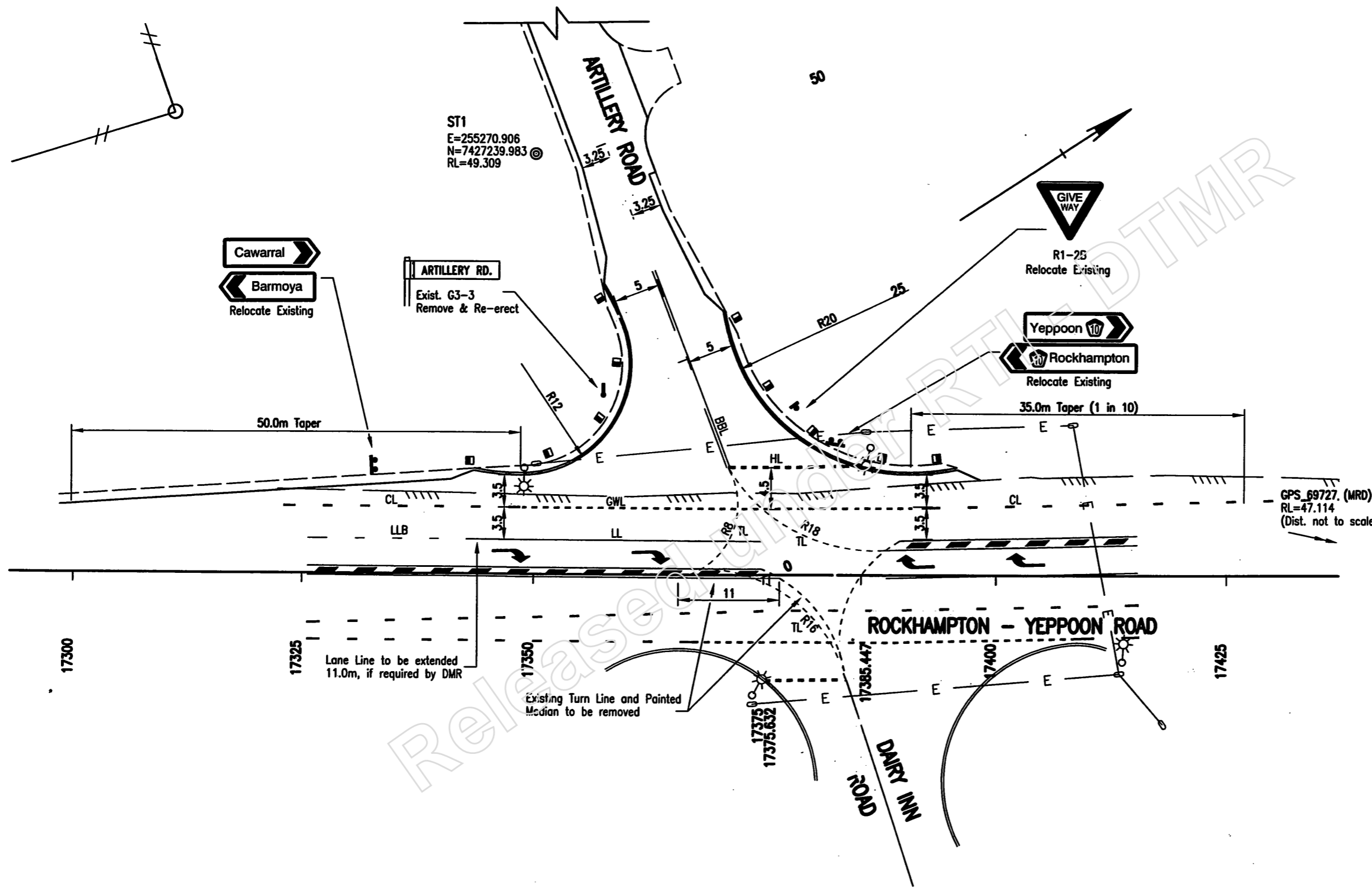
LIVINGSTONE SHIRE  
FROM THE BUSH TO THE BAYS  
70 Anzac Parade Phone (07) 4939 3388  
PO Box 600 Fax (07) 4939 3290  
Yeppoon Queensland 4703  
E-mail : general@liv'ngstone.qld.gov.au

Design/Dr G D Alexander	17/10/03
Checked S.G.S.	17.10.03
Recom'd NR	17/10/03
Approved NR	17/10/03

ROADWORKS CONSTRUCTION  
ARTILLERY ROAD  
IRON POT  
INTERSECTION DETAILS

Auxiliary Plans 8391-8396	PLAN No. 8395
Job Mnemonic 03ARTR1-9/	SHEET 5 OF 6
Construction Date 11/03	A

Parish of Cawarral



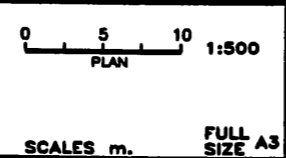
- LEGEND**
- ..... Gravel Edge
  - Bitumen Edge
  - Top of Embankment
  - Toe of Embankment
  - E--- Underground Electricity
  - EO--- Aboveground Electricity
  - Fence
  - Surface Drain
  - Shoulder Line (new)
  - Road Edge Guide Post

**TELSTRA CABLE :** No work is to be carried out within 3.0m of cable without first consulting Telstra.

**NOTE:** Survey stations required for setting out of this job are indicated thus . Please contact Design Office if removal is necessary.

Mark Description : GPS 69727 (MRD)  
 Location : Int. Yeppoon Rd & Artillery Rd  
 Datum : AHD RL : 47.114  
 Derived from : Form 6 :   
 Surveyed :  
 Title 4/2002 LEVEL DATUM

Field Bk	AMENDMENTS DESCRIPTION	APPROVED	DATE
E/F Bk 1237GDK			
Level Bk			
D/O File 1237GA	A Original Issue		MF 03/10/03



**LIVINGSTONE SHIRE**  
 FROM THE BUSH TO THE BAYS  
 70 Anzac Parade Phone (07) 4939 3388  
 PO Box 600 Fax (07) 4939 3290  
 Yeppoon Queensland 4703  
 E-mail : general@livingstone.qld.gov.au

Design/Dr	G D Alexander	17/10/03
Checked	S.G.S.	17.10.03
Recom'd		17/10/03
Approved	NR	17/10/03

Plan Description  
**ROADWORKS CONSTRUCTION**  
**ARTILLERY ROAD**  
 IRON POT  
 SIGNS AND LINEMARKING

Approved	NR	Date	17/10/03
District Director			
QMR JOB No.	/ /	QMR PLAN No.	347356
Auxiliary Plans	8391-8396	PLAN No.	8396
Job Mnemonic	03ARTR1-9/	SHEET	6
Construction Date	11/03	OF	6



MR 1  
2700103

OVERTAKING  
LANE  
300m AHEAD

Released under RTI - DTMR

04/11/2019 08:25

## Crash Site History Report

The Crash Site History report provides information about a particular crash, a crash history summary and listing of crashes at that location. The report is produced by Data Analysis for each fatal crash and is distributed within Transport and Main Roads, and on request to state and local government stakeholders.

The crash history includes fatal and hospitalisation crashes that were coded at the time of reporting. For midblock type reports the crash history includes crashes along the same road within 2.5km either direction of the subject crash, while intersection type reports include crashes within 70m of the subject crash.

### Report layout

The report layout contains two sections. The main page includes the subject crash details, location map, time series graph and crash history summary. The remainder of the report provides a listing of historic crashes for the 5 years prior to the subject crash date. These crashes are listed from north to south, or west to east for mid-block type crashes to help in finding patterns in crash types along a road. Intersection reports are ordered by crash date.

### Main page

NR

# Crash Site History

**Fatal Crash**

**9am, Sunday, 24 March 2019**

**Crash Number:** PI

**TMR: Central Queensland Region (314)**

**Intersection:** Artillery Rd & Rockhampton - Yeppoon Rd (Part 1), Ironpot, Livingstone Shire

**Lat/Long:** -23.2459380, 150.6085550

**DCA Code:** 101 (Veh'S Adjacent Approach: Thru-Thru)

**DCA Group:** Intersection, from adjacent approaches

**Nature:** Multi-Vehicle - Angle

**Feature:** Intersection - Cross

**Speed Limit:** 100 kph

**Road:** Straight, Level, Sealed - Dry

**Conditions:** Clear, Daylight

**Units Involved:**

**Casualties:**

**Road Users:**

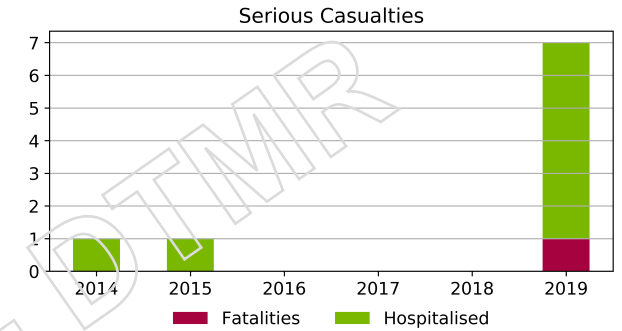
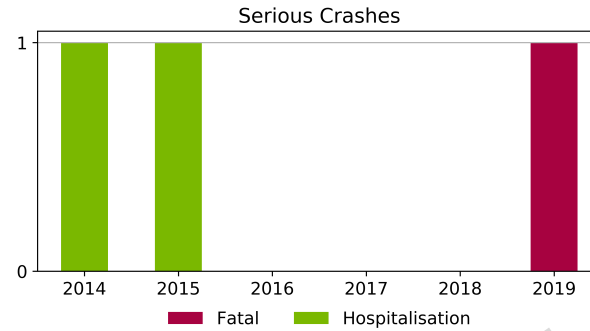
2 Cars

1 Fatality

2 Drivers

6 Hospitalised

5 Passengers



## Last 12 months - 25 March 2018 to 24 March 2019

**Serious Crashes:**

1 Fatal

**DCA Group:**

1 Intersection, from adjacent approaches

**Nature:**

1 Angle

**Behaviours/Characteristics:**

**Serious Casualties:**

1 Fatality

6 Hospitalised

## Last 3 years - 25 March 2016 to 24 March 2019

**Serious Crashes:**

1 Fatal

**DCA Group:**

1 Intersection, from adjacent approaches

**Nature:**

1 Angle

**Behaviours/Characteristics:**

**Serious Casualties:**

1 Fatality

6 Hospitalised

## Last 5 years - 25 March 2014 to 24 March 2019

**Serious Crashes:**

1 Fatal

2 Hospitalisation

**DCA Group:**

2 Intersection, from adjacent approaches

1 Rear-end

**Nature:**

2 Angle

1 Rear-end

**Behaviours/Characteristics:**

2 Road factors

**Serious Casualties:**

1 Fatality

8 Hospitalised

*Produced by Data Analysis, Land Transport Safety, Dept of Transport and Main Roads.*

*Data sourced from the Queensland RoadCrash database.*

*Fatal crash data complete to 31 October 2018.*

*Hospitalisation crash data complete to 31 August 2018.*

Department of  
Transport and Main Roads



Queensland  
Government

ID	Dist	Crash No	Date	Day	Time	Severity	DCA Group	DCA Code	Nature	Truck	Wet	Serious Casualty						
												Fatality	Hosp	Driver Passenger	Motor-cyclist	Bi-cyclist	Ped	Other
2	0.0	20190590358	24 Mar 2019	Sun	9am	Fatal	1	101	Angle	0	No	1	6	7	0	0	0	0
NR																		

Released under RTI - DTMR



OVERTAKING  
LANE  
300m AHEAD

Released under RIA/DTMP

04/11/2019 08:16



## Karen Stevens

---

**From:** Peter T Trim  
**Sent:** Wednesday, 23 October 2019 1:06 PM  
**To:** Laxman A Gopali  
**Cc:** Darren L Richardson  
**Subject:** Interim treatments at Yeppoon road/ Artillery Road intersection

Hi Laxman

As discussed – would you please review the recommendations from the investigation report for Yeppoon Road and Dairy Inn/Artillery Roads intersection and develop a proposal for the installation of the interim remedial treatments including the RAIWS on the Rockhampton – Yeppoon Road, and any other treatments that may be appropriate

I would also suggest reviewing the signs on the council roads approaching the intersection

Kind Regards

**Peter Trim PSM**

District Director (Fitzroy District) | Central Queensland Region

**Program Delivery and Operations Branch** | Infrastructure Management & Delivery Division | Department of Transport and Main Roads

---

Floor 1 | Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

(07) 4931 1540 | M: NR

[peter.t.trim@tmr.qld.gov.au](mailto:peter.t.trim@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

**Karen Stevens**

---

**From:** Vasudevan Z Ramanathan  
**Sent:** Tuesday, 29 October 2019 3:21 PM  
**To:** Sch.4 Part 4 s.6 PI  
**Cc:** Laxman A Gopali  
**Subject:** RE: ROAD SAFETY INVESTIGATION TO IDENTIFY THE DEFICIENT AT INTERSECTION OF DAIRY INN ROAD/ARTILLERY ROAD AND YEPPON ROAD  
**Attachments:** Intersection 4991 - 10yrs Crash Listing.pdf; 196 CH16.838-17.838 500m Buffer Detail.pdf; 196 CH16.838-17.838 500m Buffer Listing.pdf; Intersection 4991 - 10yrs Collision Diagram.pdf; Intersection 4991 - 10yrs Crash Detail.pdf; Crash\_Site\_History\_Notes.pdf; CSH\_F\_20190324\_20190590358\_TMR\_314\_Int\_Artillery.pdf; 329725.TIF; 347355.TIF; 347356.TIF; 274607 Type Cross Section.pdf; 274868 & 274881.pdf; 329724B.TIF

Hi [PI]

As we discussed in the meeting today, could you please investigate the intersection , Dairy Inn Road and Artillery Road and provide a report to improve the safety of traffic at this intersection.

Please find the attached document which may be useful for the investigation.

Please contact me if you need any other information to proceed this work.

Thanks.

**Vas Ramanathan**

**Senior Engineer (Road Operations)** | Fitzroy District  
Program Delivery and Operations Branch | Infrastructure Management & Delivery Division | Department of Transport and Main Roads  
Floor 1 | Rockhampton - Knight Street Complex | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
P: (07) 4931 1592 | F: (07) 4927 5020  
M [NR]  
E: [Vasudevan.z.ramanathan@tmr.qld.gov.au](mailto:Vasudevan.z.ramanathan@tmr.qld.gov.au)

196 -  
Data for ROCKHAM  
Road PTON -  
Section : YEPPON  
ROAD

District ID	TDIST_STA RT	TDIST_END	Surface Type	No Element Selected	Carriagewa y Code
6	0	0.89	SEALED	6	1
6	0	0.89	SEALED	6	3
6	0.89	1.52	SEALED	6	1
6	1.52	13.86	SEALED	6	3
6	1.52	13.86	SEALED	6	1
6	13.86	34.9	SEALED	6	1
6	34.9	35.11	SEALED	6	1
6	34.9	35.11	SEALED	6	3
6	35.11	35.39	SEALED	6	1
6	35.39	35.6	SEALED	6	3
6	35.39	35.6	SEALED	6	1

District ID	TDIST_STA RT	TDIST_END	Layer 1 Description	Surface Type	Layer 1 Type	Layer 1 Date	Layer 1 Full Description	No Element Selected	Carriagewa y Code
6	0	0.392	PMB Dense Graded Asp	Sealed	G2	23/05/2011	PMB Dense Graded Asphalt	6	3
6	0	0.392	PMB Dense Graded Asp	Sealed	G2	23/05/2011	PMB Dense Graded Asphalt	6	1
6	0.392	0.674	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	3
6	0.392	0.674	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	1
6	0.674	0.89	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	3
6	0.674	0.89	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	1
6	0.89	0.913	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	1
6	0.913	0.966	Asphaltic Concrete -	Sealed	GU	1/01/1990	Asphaltic Concrete - Quality Unknown	6	1
6	0.966	1.334	Continuous Reinforce	Sealed	E3	1/01/1952	Continuous Reinforced Cement Concrete	6	1
6	1.334	1.344	Asphaltic Concrete -	Sealed	GU	1/01/1990	Asphaltic Concrete - Quality Unknown	6	1
6	1.344	1.52	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	1
6	1.52	1.58	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	1
6	1.52	1.58	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	3
6	1.58	1.72	Bitumen Dense Graded	Sealed	G1	31/12/2017	Bitumen Dense Graded	6	3
6	1.58	1.72	Bitumen Dense Graded	Sealed	G1	31/12/2017	Bitumen Dense Graded	6	1
6	1.72	2.048	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	1
6	1.72	2.048	PMB Dense Graded Asp	Sealed	G2	1/07/2005	PMB Dense Graded Asphalt	6	3
6	2.048	2.41	Bitumen Dense Graded	Sealed	G1	28/03/2008	Bitumen Dense Graded Asphalt	6	1

6	2.048	2.71	Bitumen Dense Graded	Sealed	G1	28/03/2008	Bitumen Dense Graded Asphalt Bitumen	6	3
6	2.41	2.7	Bitumen Dense Graded	Sealed	G1	13/03/2015	Dense Graded Asphalt Bitumen	6	1
6	2.7	2.71	Bitumen Dense Graded	Sealed	G1	28/03/2008	Dense Graded Asphalt Bitumen	6	1
6	2.71	3.17	Bitumen Dense Graded	Sealed	G1	11/12/2015	Dense Graded Asphalt Bitumen	6	1
6	2.71	3.17	Bitumen Dense Graded	Sealed	G1	11/12/2015	Dense Graded Asphalt Bitumen	6	3
6	3.17	3.2	Bitumen Dense Graded	Sealed	G1	28/03/2008	Dense Graded Asphalt Bitumen	6	3
6	3.17	3.18	Bitumen Dense Graded	Sealed	G1	28/03/2008	Dense Graded Asphalt Bitumen	6	1
6	3.18	3.37	Bitumen Dense Graded	Sealed	G1	10/05/2018	Dense Graded Asphalt Bitumen	6	1
6	3.2	3.237	Bitumen Dense Graded	Sealed	G1	1/06/2009	Dense Graded Asphalt Bitumen	6	3
6	3.237	3.37	Bitumen Dense Graded	Sealed	G1	10/05/2018	Dense Graded Asphalt Bitumen	6	3
6	3.37	3.952	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	3
6	3.37	3.952	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	1
6	3.952	4.75	Bitumen Dense Graded	Sealed	G1	31/12/2017	Bitumen Dense Graded Asphalt Bitumen	6	3
6	3.952	4.75	Bitumen Dense Graded	Sealed	G1	31/12/2017	Dense Graded Asphalt PMB Spray Seal	6	1
6	4.75	4.781	PMB Spray Seal	Sealed	K2	28/05/2013	PMB Spray Seal	6	1
6	4.75	4.781	PMB Spray Seal	Sealed	K2	28/05/2013	PMB Spray Seal	6	3
6	4.781	5.421	PMB Spray Seal	Sealed	K2	17/05/2017	PMB Spray Seal	6	1
6	4.781	5.421	PMB Spray Seal	Sealed	K2	17/05/2017	PMB Spray Seal	6	3
6	5.421	5.422	Bitumen Spray Seal	Sealed	K1	23/06/2011	Bitumen Spray Seal	6	3
6	5.421	5.422	Bitumen Spray Seal	Sealed	K1	15/10/2010	Bitumen Spray Seal	6	1
6	5.422	6.07	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	3
6	5.422	6.07	PMB Dense Graded Asp	Sealed	G2	1/06/2018	PMB Dense Graded Asphalt	6	1
6	6.07	8.04	Bitumen Spray Seal	Sealed	K1	15/10/2010	Bitumen Spray Seal	6	1
6	6.07	6.805	Bitumen Spray Seal	Sealed	K1	23/06/2011	Bitumen Spray Seal	6	3
6	6.805	7	Bitumen Spray Seal	Sealed	K1	24/11/1997	Bitumen Spray Seal	6	3
6	7	13.7	PMB Spray Seal	Sealed	K2	11/04/2015	PMB Spray Seal	6	3
6	8.04	8.4	PMB Spray Seal	Sealed	K2	6/10/2005	PMB Spray Seal	6	1
6	8.4	8.47	Bitumen Spray Seal	Sealed	K1	17/09/2014	Bitumen Spray Seal	6	1
6	8.47	9.86	PMB Spray Seal	Sealed	K2	6/10/2005	PMB Spray Seal	6	1
6	9.86	9.89	PMB Spray Seal	Sealed	K2	26/02/2015	PMB Spray Seal	6	1
6	9.89	12.67	PMB Spray Seal	Sealed	K2	6/10/2005	PMB Spray Seal	6	1
6	12.67	12.691	PMB Spray Seal	Sealed	K2	30/06/2005	PMB Spray Seal	6	1
6	12.691	13.56	PMB Spray Seal	Sealed	K2	6/10/2005	PMB Spray Seal	6	1

District ID	TDIST_STA RT	TDIST_END	Lane Surface Type	No Element Selected	Carriageway Code	Lane Code	Seal Age	Pavement Age	Seal Width	Pavement Width	Lane Width	Project Completion Date	Layer 1 Date	Base Depth
6	13.56	13.86	Bitumen Spray Seal	Sealed	K1	24/11/1997	Bitumen Spray Seal	6	1					
6	13.7	13.86	Bitumen Spray Seal	Sealed	K1	24/11/1997	Bitumen Spray Seal	6	3					
6	13.86	14.055	Bitumen Spray Seal	Sealed	K1	23/12/2000	Bitumen Spray Seal	6	1					
6	14.055	21.785	PMB Spray Seal	Sealed	K2	15/10/2010	PMB Spray Seal	6	1					
6	21.785	22.345	Bitumen Spray Seal	Sealed	K1	23/12/2000	Bitumen Spray Seal	6	1					
6	22.345	22.825	Bitumen Spray Seal	Sealed	K1	31/01/2007	Bitumen Spray Seal	6	1					
6	22.825	22.86	Geotextile S	Sealed	L1	1/07/2005	Geotextile Seal	6	1					
6	22.86	24.65	Bitumen Spray Seal	Sealed	K1	17/12/2009	Bitumen Spray Seal	6	1					
6	24.65	24.99	PMB Spray Seal	Sealed	K2	17/12/2009	PMB Spray Seal	6	1					
6	24.99	25.3	Bitumen Spray Seal	Sealed	K1	17/12/2009	Bitumen Spray Seal	6	1					
6	25.3	26.6	PMB Spray Seal	Sealed	K2	26/03/2012	PMB Spray Seal	6	1					
6	26.6	28.088	PMB Spray Seal	Sealed	K2	1/07/2008	PMB Spray Seal	6	1					
6	28.088	28.77	Bitumen Spray Seal	Sealed	K1	18/12/2003	Bitumen Spray Seal	6	1					
6	28.77	28.806	Bitumen Dense Graded	Sealed	G1	1/01/1975	Bitumen Dense Graded	6	1					
6	28.806	29.935	Bitumen Spray Seal	Sealed	K1	18/12/2003	Bitumen Spray Seal	6	1					
6	29.935	30.03	Bitumen Spray Seal	Sealed	K1	30/11/2001	Bitumen Spray Seal	6	1					
6	30.03	30.738	Bitumen Spray Seal	Sealed	K1	10/06/2008	Bitumen Spray Seal	6	1					
6	30.738	31.36	Bitumen Spray Seal	Sealed	K1	31/01/2007	Bitumen Spray Seal	6	1					
6	31.36	32.11	Bitumen Dense Graded	Sealed	G1	30/06/2015	Bitumen Dense Graded	6	1					
6	32.11	34.9	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	1					
6	34.9	34.923	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	1					
6	34.9	34.925	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	3					
6	34.923	34.925	Skid Treat - Tyregri	Sealed	F1	20/04/2018	Skid Treat - Tyregri	6	1					
6	34.925	35.067	PMB Dense Graded Asp	Sealed	G2	2/04/2018	PMB Dense Graded Asp	6	1					
6	34.925	34.95	Bitumen Dense Graded	Sealed	G1	22/09/2003	Bitumen Dense Graded	6	3					
6	34.95	35.067	PMB Dense Graded Asp	Sealed	G2	2/04/2018	PMB Dense Graded Asp	6	3					
6	35.067	35.08	Bitumen Dense Graded	Sealed	G1	22/09/2003	Bitumen Dense Graded	6	3					
6	35.067	35.08	Bitumen Dense Graded	Sealed	G1	22/09/2003	Bitumen Dense Graded	6	1					
6	35.08	35.11	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	1					
6	35.08	35.11	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	3					
6	35.11	35.37	PMB Spray Seal	Sealed	K2	22/01/2016	PMB Spray Seal	6	1					
6	35.37	35.39	Bitumen Spray Seal	Sealed	K1	1/06/2004	Bitumen Spray Seal	6	1					
6	35.39	35.4	Bitumen Spray Seal	Sealed	K1	1/06/2004	Bitumen Spray Seal	6	1					
6	35.39	35.4	Bitumen Spray Seal	Sealed	K1	1/06/2004	Bitumen Spray Seal	6	3					
6	35.4	35.6	Bitumen Dense Graded	Sealed	G1	1/06/2004	Bitumen Dense Graded	6	1					
6	35.4	35.6	Bitumen Dense Graded	Sealed	G1	1/06/2004	Bitumen Dense Graded	6	3					



6	8.391	8.406 Sealed	6 M		1	31.2	31.2	5.6	5.6	5.6	21/04/2015	1/01/1988	345
6	8.4	8.47 Sealed	6	1	1	4.5	4.5	3.5	3.5	3.5	1/06/2018	17/09/2014	289
6	8.47	8.531 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	408
6	8.531	8.6 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	408
6	8.6	8.601 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	408
6	8.601	8.82 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	8.82	8.847 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	8.847	9.107 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	9.107	9.32 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	9.32	9.86 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	173
6	9.86	9.89 Sealed	6	1	1	4.1	4.5	3.5	3.5	3.5	1/06/2018	26/02/2015	299
6	9.89	11.387 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	173
6	10.001	10.016 Sealed	6 M		1	31.2	31.2	5.7	5.7	5.7	21/04/2015	1/01/1988	345
6	10.941	11.011 Sealed	6 M		1	31.2	31.2	5.1	5.1	5.1	21/04/2015	1/01/1988	345
6	11.387	11.564 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	173
6	11.564	12.67 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	173
6	12.67	12.68 Sealed	6	1	1	13.8	61.2	3.5	3.5	3.5	1/06/2018	30/06/2005	316
6	12.68	12.691 Sealed	6	1	1	13.8	61.2	3.5	3.5	3.5	1/06/2018	30/06/2005	316
6	12.691	12.86 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	173
6	12.86	12.975 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	12.975	12.985 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	12.985	13.56 Sealed	6	1	1	13.5	13.8	3.5	3.5	3.5	1/06/2018	6/10/2005	407
6	13.341	13.361 Sealed	6 M		1	31.2	31.2	6.4	6.4	6.4	21/04/2015	1/01/1988	345
6	13.56	13.68 Sealed	6	1	1	21.4	31.2	3.5	3.5	3.5	1/06/2018	24/11/1997	451
6	13.56	13.58 Sealed	6 M		1	31.2	31.2	4.8	4.8	4.8	21/04/2015	1/01/1988	345
6	13.68	13.76 Sealed	6	1	1	21.4	31.2	3.5	3.5	3.5	1/06/2018	24/11/1997	387
6	13.76	13.86 Sealed	6	1	1	21.4	39.2	3.5	3.5	3.5	1/06/2018	24/11/1997	387

Not Relevant

Not Relevant

Released under RTI - DTMR



Not Relevant

6	34.923	34.925 Sealed	6	1	1	3.2	15.5	3.5	3.5	3.5	1/06/2018	20/04/2018	540
6	34.925	34.96 Sealed	6	1	1	1	15.5	3.5	3.5	3.5	1/06/2018	2/04/2018	555
6	34.96	34.963 Sealed	6	1	1	1	15.5	6.8	6.8	6.8	1/06/2018	2/04/2018	555
6	34.963	35.023 Sealed	6	1	1	1	15.5	6.8	6.8	6.8	1/06/2018	2/04/2018	555
6	35.023	35.053 Sealed	6 W		1	1	15.5	6.8	6.8	6.8	1/06/2018	2/04/2018	555
6	35.023	35.03 Sealed	6	1	1	1	15.5	6.8	6.8	6.8	1/06/2018	2/04/2018	515
6	35.03	35.06 Sealed	6	1	1	1	15.5	3.5	3.5	3.5	1/06/2018	2/04/2018	515
6	35.06	35.067 Sealed	6	1	1	1	15.5	3.5	3.5	3.5	1/06/2018	2/04/2018	515
6	35.067	35.08 Sealed	6	1	1	15.5	15.5	3.5	3.5	3.5	1/06/2018	22/09/2003	491
6	35.08	35.11 Sealed	6	1	1	3.2	15.5	3.5	3.5	3.5	1/06/2018	22/01/2016	515

Not Relevant

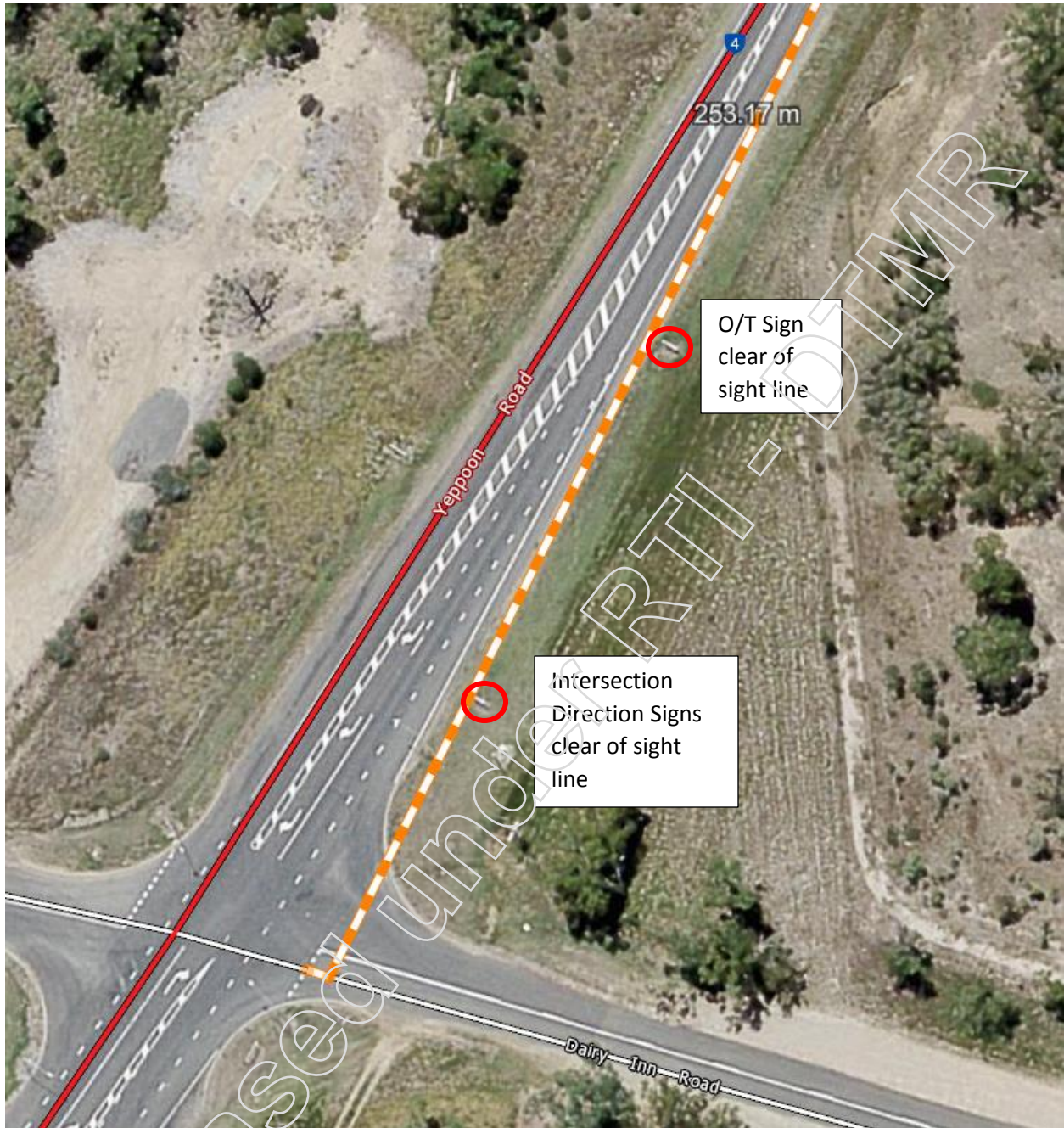


Figure 1 – Dairy Inn Road Sight Visibility

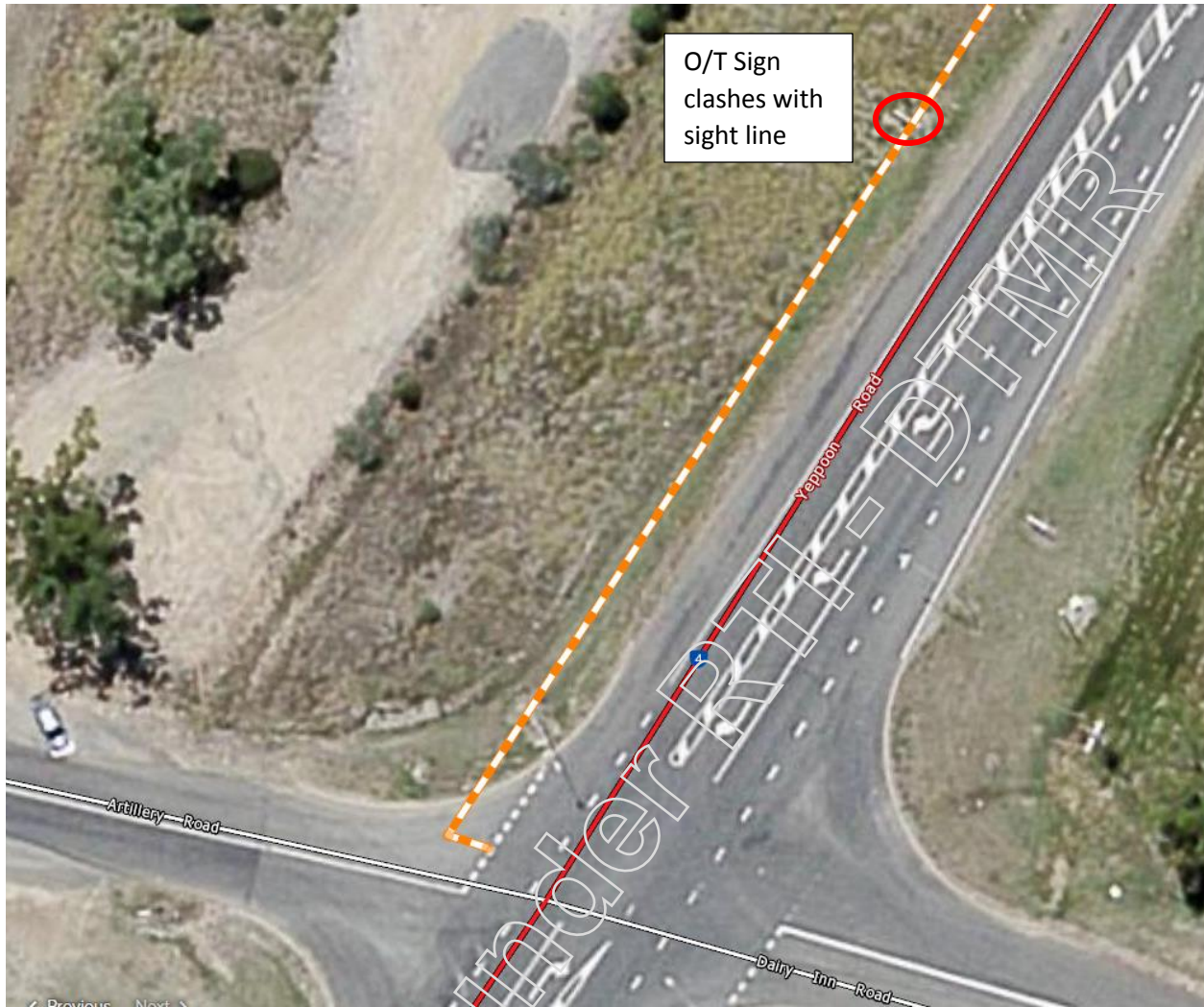


Figure 2 – Artillery Road Sight Visibility

Released Under PII-DMR

## Karen Stevens

---

**From:** Fitzroy District Road Safety  
**Sent:** Monday, 24 August 2020 12:15 PM  
**To:** Colin J Edmonston; Laxman A Gopali; Colleen M Williams; Garry I Patterson;  
Findlater.EwanG[RPC]; Pimm.RayJ[RPC]; amal.meegahawattage@livingstone.qld.gov.au; PI  
Sch.4 Part 4 s.6 PI Ashish Baral; Sch.4 Part 4 s.6 PI Adithya.Anilkumar@rrc.qld.gov.au  
**Cc:** Brett A Skyring  
**Subject:** Minutes of 3E & Speed Management Committee meeting held 5/08/2020  
**Attachments:** 3E Minutes August 2020 final.pdf

Good afternoon,

Please find attached the final version of the minutes of the Fitzroy District 3E Committee and Speed Management Committee meeting held Wednesday 5 August 2020.

The next meeting is proposed for Tuesday 15 September 2020.

Kind regards,

**Kath Ferguson**

Operations Officer (Road Safety) | Central Queensland Region

**Program Delivery & Operations Branch** | Infrastructure Management and Delivery Division | Department of Transport and Main Roads

-

---

Floor 1 | 31 Knight Street | North Rockhampton Qld 4701  
PO Box 5096 | Red Hill Rockhampton Qld 4701  
(07) 49311527  
[www.katharine.l.ferguson@tmr.qld.gov.au](mailto:www.katharine.l.ferguson@tmr.qld.gov.au)  
[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

# Minutes: Fitzroy District 3E Committee and Speed Management meeting, July 2020

**Date:** Wednesday 5 August 2020      **Time:** 10am – 11:50am  
**Place:** 31 Knight Street, Ground Floor Theatre  
**Chair:** Colin Edmonston      **Minute taker:** Kath Ferguson

**Invitees**

DTMR Colin Edmonston (CE)      DTMR Laxman Gopali (LG)      QPS Ewan Findlater (EF)  
LSC Reece Sainsbury (RS)      LSC Ashish Baral (AB)      LSC Amani Meegahawattage (AM)  
RRC Sch.4 Part 4 s.6 PI      RRC Adi Aninkumar (AA)

**Apologies**

DTMR – Colleen Williams  
QPS – personal information  
RRC Sch.4 Part 4 s.6 PI

Ongoing Business		
Location / Topic	Issue / Commentary	Actions / Next Steps
NR		

Released under RTI - DTMR

Location / Topic	Issue / Commentary	Actions / Next Steps
<p>Not Relevant</p>		

Location / Topic	Issue / Commentary	Actions / Next Steps
<b>General Business</b>		
<p>Yeppoon Road i/s with Dairy Inn Road</p>	<p>Yeppoon Road intersection with Dairy Inn Road – The Coroner has requested an update on the recommendations that we made to change speed signs on Dairy Inn and Artillery Roads and in relation to when those changes were made. I inspected the scene and found the new 60 signs leading to the intersection, upgraded Give Way signs. My recommendations were for Stop signs to be installed instead of Give Way signs.</p> <p>The court matter against the driver of unit 1 from the fatal last year where the young child died was to be heard yesterday but the charge had to be withdrawn due to a defence submission about visibility and in certain conditions, traffic on Yeppoon Road would not be visible to a driver on</p>	

Location / Topic	Issue / Commentary	Actions / Next Steps
<b>General Business</b>	<p>Dairy Inn Road. The defence submission was that their driver did look but due to the angle of the road, angle of the A-pillar in her car and her slow speed caused a blind spot where she didn't see the other car until she was out onto Yeppoon Road. We couldn't negative that it was a mistake of fact and had to withdraw the charge. We felt that if there was a Stop sign at the intersection, the defendant would have stopped (instead of rolling through) and there would be no doubt about the visibility. I don't know if DTMR will conduct a further review of that intersection or will change the signs but the Coroner might ask about why a Stop sign isn't installed at that intersection?</p> <p>TMR are looking at reducing the speed on Rockhampton – Yeppoon Road through the intersection from 100km/h to 80km/h zone as well as improvements to signage. Discussion on traffic and usage of the intersection.</p>	
<p>Released under RIPA DTMR</p> <p>NR</p>		



Not Relevant

Released under RTI - DTMR

**Crash Types**

Crash Dates  -  Alignment: Vertical

Owner  Horizontal

DCA Code  Feature

Group  Traffic Ctrl

Speed Limit

Fatalities =  Contrib Circ.

Severity  Unit Type

Nature  Risk Factor

**Area** LGA  SLA  Police Division

**Road Sections**

All Road Sections  S Include Crashes on:  Y Thru road Mid-block  Y Thru roads at Intersections  Y Intersecting roads at Intersections

Road Section	Cway	Start		End		Tdist		Number of Crashes					Total
		RPC	Dist	RPC	Dist	Start	End	Fatal	Hosp.	Medical	Minor	PDO	
196 ROCKHAMPTON - YEPPOON ROAD		5D	4.330	5D	4.830	17.000	17.500	1	1	0	0	0	2

**Intersections**

All Intersections  N



Crash No.	Date	Day	Hour	DCA	No. Units	Street/s	Nature
Sch.4 Part 4 s.6 PI	24-MAR-2019	Sun	09	101 VEH'S ADJA	2	Artillery Rd Yeppoon Rd	02 Angle
R Sect	196 Rockhampton - Yeppoon Road		RPC	5D		Alignment: Vertical	1 Level
Cway	1	Direction	W		Dist from RPC	4.668	
Inter.			Tdist	17.338		Horizontal	1 Straight
Road Surface	Sealed - dry				Feature	10 Cross	
						Traffic Control	03 Give Way

Units	Age	Gender	Unit Type	Dirn.	Intended Action	BAC
1			01 Car, Station Wagon	W	01 Go Straight Ahead	0
2		PI	01 Car, Station Wagon	N	01 Go Straight Ahead	0

**Description**

Description of accident / position and travel of units At approximately 0930hrs on Sunday the 24th day of March 2019 U1 was travelling West on Dairy Inn Road and has entered the intersection at Yeppoon Road, U1 has proceeded straight through this intersection intending to continue West onto Artillery Road. U2 has been travelling North-East on Yeppoon Road intending to continue straight towards Yeppoon. U2 has crashed head on into the left side of U1. There is a give way sign at Dairy Inn Road as you enter Yeppoon Road. Description of location The incident location is a very busy section of road. It is a cross intersection with Dairy Inn Road and Artillery Road intersecting through Yeppoon Road. it is a section of highway with 2 lanes travelling in the same direction then 2 lanes travelling in the other direction split in the middle by a median strip. Yeppoon Road also has 2 turning lanes from both directions at this intersection. The speed limit is 100km/h. Road/Weather conditions At the location of the crash the roadway is sealed bitumen. The road was dry and the conditions were fine and clear. Unit Description / Damage Unit 1 is a 2015 Blue Toyota Corolla bearing Qld registration xxxxxxx. Major damage to entire left side of vehicle. Seized for mechanical inspection authorised by DDO and FCU. Unit 2 is a 2010 Grey Toyota Corolla bearing Qld registration xxxxxxx. Major damage to entire front of vehicle. Seized for mechanical inspection authorised critical condition ' admitted to hospital. (entrapped in vehicle upon Police attendance, QFRS assisted and removed child from vehicle). Unit 2- ' xxxxxxxxxxxxxxxx ' driver ' minor injuries ' admitted to hospital. ' injuries ' admitted to hospital. Witnesses xxxxxxxxxxxxxxxx - observed U1 overtake her vehicle a short time before the crash along Dairy Inn Road, observed U1 enter intersection but did not observe time of impact. xxxxxxxxxxxxxxxx - at intersection at time of crash, did not observe crash, heard impact then observed aftermath. xxxxxxxxxxxxxxxx - (in vehicle with witness xxxxxxx) at intersection at time of crash, did not observe crash, heard impact then observed aftermath. xxxxxxxxxxxxxxxx - council worker working in close proximity to crash location, did not observe crash but heard it and assisted. xxxxxxxxxxxxxxxx - passing motorist, did not

**Contributing Circumstances**

- 2 NOT APPLICABLE
- 1 VIOLATION - DISOBEY GIVE WAY SIGN
- 1 VIOLATION - DRIVING WITHOUT DUE CARE A



Crash No.	Date	Day	Hour	DCA	No. Units	Street/s	Nature
Sch.4 Part 4 s.6 PI	24-MAR-2019	Sun	09	101 VEH'S ADJA	2	Artillery Rd Yeppoon Rd	02 Angle
R Sect	196 Rockhampton - Yeppoon Road		RPC	5D		Alignment: Vertical	1 Level
Cway	1	Direction	W		Dist from RPC	4.668	
Inter.			Tdist	17.338		Horizontal	1 Straight
Road Surface	Sealed - dry				Feature	10 Cross	
						Traffic Control	03 Give Way

Description

observe crash but attended a very short time after crash and assisted. Outcome All drivers and passengers have been transported by QAS to RBH for treatment. 1 confirmed fatality - xxxxxxxxxxxxxxxxxxxxxxx. 1 other child in critical condition - xxxxxxxxxxxxxxxxxxxxxxx Rockhampton Crew attended RBH to collect bloods from both drivers. FCU attended and investigating.

Released under RTI - Draft



Crash No.	Date	Day	Hour	DCA	No. Units	Street/s	Nature
Sch.4 Part 4 s.6 PI	08-JUL-2020	Wed	15	101 VEH'S ADJA	2	Artillery Rd Yeppoon Rd	02 Angle
R Sect	196 Rockhampton - Yeppoon Road		RPC	5D		Alignment: Vertical	1 Level
Cway	1	Direction	E		Dist from RPC	4.689	
Inter.	4991 Artillery Rd / Dairy Inn Rd, Ironpot				Tdist	17.359	
Road Surface	Sealed - dry				Traffic Control	03 Give Way	
Severity	2 ADMITTED TO HOSPITAL						
Horizontal	1 Straight						
Feature	10 Cross						

Units	Age	Gender	Unit Type	Dirn.	Intended Action	BAC
1			01 Car, Station Wagon	E	01 Go Straight Ahead	0
2		PI	02 Utility, Panel Van	S	01 Go Straight Ahead	0

Description

Time: 1535hrs Date: 08/07/2020 Location: Yeppoon Road IRON POT, QLD, 4701 at the intersection of Artillery Road and Dairy Inn Road. Police Division: Rockhampton Road  
 Conditions: Dry/Sealed Weather Conditions: Clear/afternoon/no glare from sun. Traffic Flow: Medium Report Method: Communications At approximately 1535hrs on the 8 th of July 2020 a two-vehicle traffic crash has occurred on Yeppoon Road, IRONPOT at the intersections of Artillery Road and Dairy Inn Road. Yeppoon Road runs in a westerly direction out-bound from Yeppoon. It is a sealed bitumen road with one lane travelling in both directions, surrounded by open table drains to allow for natural water runoff. The cross intersection is controlled by give-way signs on Artillery Road and Dairy Inn Road with Yeppoon Road being the straight through road. The intersection has designated turning lanes on both sides of Yeppoon Road to allow traffic to safely wait for a break in traffic. The roadway is marked with solid white lines on the outer lanes and separated with double white line in the centre. There are street lights at the intersection. At the time of the traffic crash there was medium traffic flow in both directions. Only two units involved and four nominated witness to the accident. Unit 1 is white Suzuki Swift hatch with Queensland registration xxxxxx driven by xxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxx. Juvenile female passenger xxxxx xxxxxxxxxxxxxxxxxxxxxxxx Unit 1 has travelled from Mackay in a south-bound direction towards Brisbane. Unit 1 has driven south east on Artillery Road in a safe manner. Unit 1 has intended on travelling straight ahead through the intersection of Yeppoon Road onto Dairy Inn Road onto Cawarrall. Unit 1 has come to a complete stop at the intersection for no more than a minute. A large truck travelling East on Yeppoon Road has passed through the intersection on front of Unit 1. Unit 1 has entered the intersection immediately after the truck passed through. Unit 1 has failed to give-way to unit 2 who has collided with the passenger side of Unit 1. Unit 1 has spun out of control on impact and has travelled about 20 metres before parting the roadway on the far-left hand side. Unit 1 has then collided with a metal light pole causing it to fall to the ground. Unit 1 has continued on that path without deviation until it came to rest on the grassy

Contributing Circumstances

2 NOT APPLICABLE  
 1 VIOLATION - DISOBEY GIVE WAY SIGN



Crash No.	Date	Day	Hour	DCA	No. Units	Street/s	Nature
Sch.4 Part 4 s.6 PI	08-JUL-2020	Wed	15	101 VEH'S ADJA	2	Artillery Rd Yeppoon Rd	02 Angle
							Severity 2 ADMITTED TO HOSPITAL
							Alignment: Vertical 1 Level
R Sect	196 Rockhampton - Yeppoon Road					RPC 5D	Horizontal 1 Straight
C way	1		Direction E			Dist from RPC 4.689	Feature 10 Cross
Inter.	4991 Artillery Rd / Dairy Inn Rd, Ironpot					Tdist 17.359	Traffic Control 03 Give Way
Road Surface	Sealed - dry						

Description

verge. Unit 1 driver and juvenile female passenger were entrapped in the vehicle and required QFRS to cut them free. Unit 1 passenger was believed to be unconscious as a result of the impact and [PI] [PI] Unit 1 was assessed by QAS and taken to Rockhampton Hospital to receive treatment for injuries to the [PI] Unit 1 provided a brief version to Police and provided a negative RBT result. Unit 2 is a white Toyota Hilux utility bearing QLD registration xxxxxx driven by xxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxx Unit 2 was travelling in a westerly direction on Yeppoon Road in the direction of Rockhampton. Unit 2 has continued straight ahead as he approached the intersections of Artillery and Dairy Inn Roads. Unit 2 was travelling at about 100 km/h when Unit 1 entered the intersection on the right-hand side. Unit 1 applied the brakes immediately however was only about 10 metres from Unit 1. Unit 2 has collided head on into the passenger side of Unit 1 causing both vehicles to spin out of control. Unit 2 has come to a stop about 50 metres on the western side of the intersection. Unit 2 exited the vehicle and rendered assistance to Unit 1 until help arrived to take over provided a negative RBT. The reporting officer attend the scene, obtained details, versions and observed damage to the vehicle. Nil action taken due to the nature of the injuries sustained by unit 1. [PI] [PI] Offered and accepted a juvenile caution. Administered by RO and authorised by DSC xxxxx. CAUTION FORM ADDED TO REPORT.

## Karen Stevens

---

**From:** Laxman A Gopali  
**Sent:** Thursday, 2 June 2022 2:39 PM  
**To:** Fitzroy District Road Safety; Karen Stevens  
**Subject:** FW: Crash 8/07/2020 on 196 Rockhampton - Yeppoon Road, intersection Artillery Road/Dairy Inn Road  
**Attachments:** 10-7-2020 Rd 196.pdf

FYI

Kind Regards

**Laxman Gopali**

Principal Engineer (Civil) RPEQ CPEng NER MIEAust (Fitzroy District) | Central Queensland Region  
**Program Delivery and Operations Branch** | Infrastructure Management & Delivery Division | Department of Transport and Main Roads

Floor 1 | Knight Street Office Complex | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

P: 074931 1568 | M: NR

E: [Laxman.A.Gopali@tmr.qld.gov.au](mailto:Laxman.A.Gopali@tmr.qld.gov.au)

W: [www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)

---

**From:** Katharine L Ferguson <katharine.l.ferguson@tmr.qld.gov.au>

**Sent:** Friday, 10 July 2020 11:16 AM

**To:** Dave J Grosse <Dave.J.Grosse@tmr.qld.gov.au>

**Cc:** Peter T Trim <Peter.T.Trim@tmr.qld.gov.au>; Darren L Richardson <darren.l.richardson@tmr.qld.gov.au>;

Laxman A Gopali <Laxman.A.Gopali@tmr.qld.gov.au>

**Subject:** Crash 8/07/2020 on 196 Rockhampton - Yeppoon Road, intersection Artillery Road/Dairy Inn Road

Dave,

As requested, I spoke with the Yeppoon Qld Police officer investigating the crash.

The crash occurred on Wednesday 8 July at 3:30pm

Crash # QP2001415874

- Unit 1 was travelling on Artillery Road towards R'ton-Yeppoon Road intersection
- Unit 1 stopped at the intersection with the intention of travelling through the intersection into Dairy Inn Road
- A truck travelling towards Yeppoon on the R'ton-Yeppoon Road went through the intersection at this time
- Unit 1 has entered the intersection after the truck
- Unit 2, travelling towards Rockhampton on the R'ton-Yeppoon Road entered the intersection after the truck and collided with the back passenger side door of Unit 1

It is believed that the driver of Unit 1 did not see the oncoming unit 2 due to view obscured by the truck

Unit 2 was unable to take evasive action before collision as the driver did not see unit 2 approaching due to view obscured by the truck

No atmospheric factors (no glare) or driver distraction

QPS Investigating officer did not identify the intersection as a contributing factor

QPS Investigating officer sees it as possible inexperience/mistake of Unit 1 – infringement: failure to give way

Other comments by QPS Investigating officer

- This is a very busy road and a very busy intersection
- This location is very flat and fog can be an issue (obviously not in relation to this crash)
- The father of the unit 1 driver was very vocal believing the intersection was at fault in this crash and spoke about pursuing his concerns further



Kind regards,

**Kath Ferguson**

Operations Officer (Road Safety) | Central Queensland Region

**Program Delivery & Operations Branch** | Infrastructure Management and Delivery Division | Department of Transport and Main Roads

-

---

Floor 1 | 31 Knight Street | North Rockhampton Qld 4701

PO Box 5096 | Red Hill Rockhampton Qld 4701

(07) 49311527

[www.katharine.l.ferguson@tmr.qld.gov.au](mailto:www.katharine.l.ferguson@tmr.qld.gov.au)

[www.tmr.qld.gov.au](http://www.tmr.qld.gov.au)



**Fitzroy District**

**Crash No.** Sch.4 Part 4 s.6 PI

**Sunday 24 March 2019**

**196 Rockhampton – Yeppoon Road**

**Chainage 17.338**

**(Intersection with Dairy Inn Road / Artillery Road)**

**Stage 1 Report**

**Stage 1 Investigation of Road Safety and Road Environment  
Conditions**

## Document control sheet

### Contact for enquiries and proposed changes

If you have any questions regarding this document or if you have a suggestion for improvement, please contact:

**Contact officer** Ishara Jayasinghe  
**Title** Engineer (Civil), Fitzroy District, Central Queensland Region  
**Phone** (07) 4931 1691

### Version history

Version no.	Date	Changed by	Nature of amendment
1	2/04/2019	Ishara Jayasinghe	First Draft
2	4/04/2019	Ishara Jayasinghe	Second Draft
3	5/04/2019	Ishara Jayasinghe	Final Draft

### Document sign off

#### Report prepared by:

Name Ishara Jayasinghe

Position Engineer (Civil)

Signature \_\_\_\_\_ Date \_\_\_\_\_

#### Submitted by:

Name Vasudevan Ramanathan

Position Senior Engineer (Civil)

Signature \_\_\_\_\_ Date \_\_\_\_\_

#### Endorsed by:

Name Laxman Gopali

Position Senior Engineer (Civil)

Signature \_\_\_\_\_ Date \_\_\_\_\_

### Purpose of Report

The purpose of this report is to collect road-based information at and on the approaches to crash sites, to determine any road infrastructure factors which, if treated, could reduce the risk of serious crashes happening at the site in the future.

## Contents

1	Crash Details .....	4
2	Police Report Information .....	4
3	Site Details: Site Inspection.....	4
4	Crash History and Analysis .....	7
5	Attachments.....	8
6	Investigating Officer’s Statement.....	9
7	Review.....	9
7.1	Immediate Interim Remedial Measures .....	9
7.2	Actions that Permanently Reduce Road Safety Risk.....	9
8	Warrant for Stage 2 Investigation .....	10
	<i>APPENDIX A - QPS CRASH REPORT .....</i>	<i>11</i>
	<i>APPENDIX B - CRASH SITE INVESTIGATION CHECKLIST.....</i>	<i>12</i>
	<i>APPENDIX C - SITE PHOTOGRAPHS .....</i>	<i>13</i>
	<i>APPENDIX D - ROADCRASH2 10-YEAR CRASH HISTORY .....</i>	<i>18</i>
	<i>APPENDIX E – SITE PLANS .....</i>	<i>19</i>
	<i>APPENDIX F – MEDIA CLIPPINGS .....</i>	<i>20</i>

## 1 Crash Details

**Crash No** Sch.4 Part 4 s.6 PI

**Pursuit No.** To be advised

**Time and date of crash:** Sunday, 24 March 2019, time 9:45am

**DCA code:** 101 vehicles from adjacent approaches: through-through

**Tdist and road name:** Chainage 17.633km, 196 Rockhampton-Yeppoon Road (ironpot)

**Severity:** 1 fatality, multiple injuries/hospitalisations

**Victim age and gender:** Nine-year-old female passenger of Unit 1

**Unit information:** Unit 1 – Car/Station Wagon (hatchback), travelling West

Unit 2 – Car/Station Wagon (hatchback), travelling north

**No of units:** 2

## 2 Police Report Information

Summary of the Information as of the 29<sup>th</sup> March in email from QPS: The occupants of the blue Toyota Corolla (Unit 1) were travelling from Emu Park to the Capricorn Caves for the day and was travelling (west) on Dairy Inn Road towards Yeppoon Road. Information from personal information was that the driver slowed and indicated to turn left onto Yeppoon Road, then the daughter in the front passenger seat advised the driver to go straight ahead in order to reach their destination quicker.

The driver appears to have then driven straight across the intersection and into the path of the grey Toyota Corolla (Unit 2) which was travelling (north-east) towards Yeppoon. There were no pre-impact brake marks from either vehicle and Unit 2 collided with the back-left door area of Unit 1. The collision occurred in the (north-) eastbound lane of Yeppoon Road.

A 6yo boy was seated in a child booster seat on the left rear side and the 9yo deceased girl was seated in the middle rear seat.

QPS have mapped and photographed the scene and conducted friction testing across both directions (QPS advise that extra skid marks on the road can be observed by the road crash investigators onsite).

Refer to **Appendix A** for Police Report.

## 3 Site Details: Site Inspection

On Monday 1 April 2019 a crash site investigation was conducted by Vasudevan Ramanathan, a Senior Engineer (Civil) in the Road Operations Unit and Ishara Jayasinghe, Engineer (Civil) from DTMR, Fitzroy District. The investigation commenced at around 10:40 am, in dry and sunny conditions. Note the crash occurred at around 9:45 am in dry, clear conditions according to the QPS report.

The crash site is located in a rural area on the Rockhampton-Yeppoon Road (196) at the approximate chainage of 17.338 km. At the time of the investigation, initial crash clearing had already been completed. Yellow paint marks left by the police indicated the point at which Unit 1 had left the roadway on the north-west corner of the intersection of Artillery Road and Rockhampton-Yeppoon Road after attempting to cross from Dairy Inn Road to Artillery Road. Photography from Media reports confirm this as well as the final location of Unit 2 in the left-hand lane of Rockhampton-Yeppoon Road.

The investigated section of road includes an unsignalised intersection and its approaches, in a 100km/hr zone, with an AADT (through Rockhampton-Yeppoon Road) of 7,215 vpd (2018). The section under investigation has the following configuration:

- One through lane heading north-east towards Yeppoon
- A right turn lane from the north-east bound direction into Dairy Inn Road,
- An auxiliary lane allowing left-into and left-out movements from Artillery Road,
- One through lane heading south-west to Rockhampton (north of the intersection),
- An auxiliary lane allowing left-into and left-out of movements from Dairy Inn Road.
- Just south of the intersection there is a transition to one through lane and one overtaking lane heading south-west and an overtaking lane is available 380 metres north of the intersection in the north-east-bound direction.

As determined from the working plans and intersection drawings, the traffic lane widths were 3.5m wide on the through lane and 3.5m on the left hand turn in and turn out auxiliary lane heading north-east towards Yeppoon, 3m wide on the right-hand turn lane into Artillery Rd (from south-west bound direction) and shoulders were 1.5m wide in each direction on the approaches.

The section covering 500m either side of the intersection has a downhill grade of 2-3% from Rockhampton to Yeppoon. There is a crest approximately 250m away on the south-western approach on the LHS. The crossfalls of the lanes and shoulders in both directions looked reasonable and to be within standard. According to the drawings there is a 3% superelevation at the intersection.

The surface of the carriageway is a spray-sealed surface, across the full width including shoulders, found in good dry conditions. Some oxidation of the bitumen was evident but the aggregate (upon visual inspection) was in good condition and not overly embedded and overall the surface appeared to be in a satisfactory condition.

Very minor potholing was observed closer to the centre-line on the Cawarral-bound lane of Dairy Inn Road, as well as on the turning lane from Yeppoon Rd (south-west bound) into Dairy Inn road (noting this is Council-owned road). No rutting was observed, however, isolated but minor areas of bitumen flushing were observed as well as a minor quantity of loose stone on the left-turning lane from Yeppoon Rd into Dairy Inn Rd.

What appears to be a minor structural failure of an underlying layer was observed to cover several square metres of the pavement at the left turn from Dairy Inn Road onto the south-west bound lane of Rockhampton-Yeppoon Road (note, however, that this is out of the path of vehicles travelling

between Dairy Inn Road and Artillery Road, across Rockhampton-Yeppoon Road). A small patch had been applied in this area.

Skid marks were observed on the surface of the intersection, made by vehicles travelling in a number of different paths including fresh skid marks between Yeppoon Road and Artillery Road and across Yeppoon Road. Note that the QPS report mentioned that extra skid marks would be found so any skid marks made during the incident could not be easily distinguished from skid marks made by QPS during their own investigation. Yellow paint marks (applied by QP) tracking the movements of the vehicles on the intersection through to the point of impact with the kerb and the final locations were also observed. Gouge marks/damaged concrete, highlighted by yellow paint, were observed on the kerb, indicating where Unit 1 had struck the kerb and left the roadway after the collision.

The intersection and its approaches are in relatively flat terrain, with the road sloping gently downwards towards Yeppoon as mentioned, and batters, no steeper than 1 in 4 on the approaches. The grass while growing was not high while fence lines on the adjacent rural properties are well back from the road shoulders. The nearest property access is 200 metres south of the intersection on the RHS.

There weren't any drainage issues noticed.

There weren't any obstructions or foreign objects within the clear zone found on the day of the investigation of the crash area apart from a shredded tire to the side of road immediately after the left-out turn from Dairy Inn Road southbound onto Yeppoon-Rockhampton Road.

Sight distance appears to be adequate in both directions with vehicles coming over the crest from the south-west being visible at Chainage 17.065km, a distance of over 270m from the centre of the intersection, and advertising signs being visible, north-east of the sight at Chainage 17.765km a distance of over 420 metres from the intersection.

Line marking is in place: Give way lines are on the minor road approaches, edge lines are present, 1 metre wide painted medians, broken lines and continuous lines are in place, demarcating the through lanes and two right turning lanes from Rockhampton-Yeppoon Road into Dairy Inn Road and Artillery Road. Going further away on the approaches the two directions are separated by double barrier lines. Retro-reflective Raised Pavement Markers (RRPMs) have been applied to the painted medians, edge lines and the continuous line delineating the turn lanes. Line marking and RRPMs are in good condition.

Permanent road edge guide posts are in place, in good condition and placed appropriately at curves.

Light poles with outreach brackets are installed on all four corners of the intersection but these present no obstructions to visibility and the poles are marked with road edge guide posts.

Permanent signage has been installed at the intersection and on its approaches and is in relatively good condition.

Cross-road warning signs (W2-1) have been installed at the approximate (DVR) chainage of 17.124km, i.e. approximately 210m prior to the intersection on the north-east bound direction (from Yeppoon) and at Chainage 17.61km on the south-west bound direction, i.e. 274m.

“Overtaking Lane Ahead...” and “Keep Left Unless Overtaking” signs are installed on the approaches to the intersection with adequate horizontal clearance from the edge of the road shoulder.

Pole mounted street signs for Dairy Inn Road and Artillery Road are in place but do not present a hazard. Road information signs indicating the directions to Rockhampton and Yeppoon are installed on the Dairy Inn side of Rockhampton-Yeppoon Road, facing motorists leaving Artillery Road.

The investigators also inspected both Dairy Inn Road and Artillery Roads, to get an understanding of the conditions of the approach to the intersection. It was confirmed that, on the approaches from both minor roads, “Give way” signs were in place, preceded by LED “GIVEWAY SIGN AHEAD” with supplementary “SLOW DOWN” warning, preceded by “GIVEWAY SIGN AHEAD” signs and “REDUCE SPEED” signs. Much further away from the intersection, floodway warning signs are in place as well as “Crest” warning signs, reduce speed warning signs, regulatory speed signs at bends and 60km/h, 80km/hr and 100km/h regulatory speed signs were in place along Dairy Inn Road, reverting to 100km/h approaching the intersection. Contractors conducting slashing works for Council on Artillery Road at the time of the inspection had appropriate signage in place.

In conclusion, this section of the road was found in good dry conditions with respect to the road surface, with the road’s geometric elements appearing to comply with standards, upon initial inspection, and all safety devices being in place, not obstructing the road environment, during the road crash investigation and at the time of the accident.

The information obtained on the road’s conditions and the road’s environment during the inspection can be found in **Appendix B**, Crash Site Investigation Checklist.

Photographs of the roads conditions and roads environment at the intersection can be found in **Appendix C**

#### **4 Crash History and Analysis**

Crash histories for both the Rockhampton-Yeppoon Rd/Dairy-Inn Road Intersection as well as the segment of road spanning from 500 metres upstream to 500 metres downstream of the intersection was obtained from the RoadCrash 2 database. The check covers 9 years and 9 months of validated data

The report showed that there were 16 other crashes in the 1km long stretch of road Rockhampton-Yeppoon Road, 500m either side of the crash site. All these crashes occurred in the validated data period.

The DCA code of this crash is 1-1 vehicles adjacent approach: through-through. The DCA codes of the other crashes are:

1 x 107 Intersection vehicles from adjacent approaches: Thru-Left – Admitted to hospital

1 x 201 Vehicles from opposing directions: Head-on

Received medical treatment

3 x 202 Vehicles from opposing directions: Thru-right

Admitted to hospital

- Property damage only
- Admitted to hospital
- 4 x 301 Vehicles from one direction: Vehicles in the same lane
  - Admitted to hospital
  - Admitted to hospital
  - Property damage only
  - Admitted to hospital
- 1 x 303 Vehicles from one direction: Right-Rear – Property damage only
- 1 x 305 Vehicles from one direction: Vehicles in parallel lanes: Lane side swipe
  - Minor injury – first aid or ...
- 1 x 408 Manoeuvring: From footway – Admitted to Hospital
- 2x 703 Off Path on Straight: Left off carriageway into object
  - Received medical treatment
  - Minor injury – first aid or ...
- 2 x 705 Off Path on Straight: Out of control on carriageway
  - admitted to hospital
  - admitted to hospital

The crash history shows that this is the first validated crash for DCA code 101 (through-through movements of two vehicles) at this intersection. The data, when analysed closely, does not show an emerging pattern of crashes. While there are 6 crashes involving vehicles travelling in one direction, most of these can be explained by driver inattention and poor driving, such as drivers following too closely. Looked at in a different way, the data may indicate some difficulty for some road users in judging oncoming traffic and subsequently negotiating movements in a safe manner across Rockhampton-Yeppoon Road, whether it be while travelling between Dairy Inn Road and Artillery Road or making right turn movements onto Dairy Inn Road or Artillery Road. Again, this can be put down to poor attention and not following give-way rules or failure of the driver to take adequate caution in darker-conditions or with the glare of oncoming vehicles' headlights as happened in one case.

## 5 Attachments

- Appendix A: Crash Site Investigation Checklist
- Appendix B: Site Photographs
- Appendix C: QPS Crash Report
- Appendix D: RoadCrash2 10-year history
- Appendix E: Site Plans



## 6 Investigating Officer's Statement

This Crash Investigation Report was carried out by a regional Crash Investigation Officer, using observations and information made available to them. Every effort was made to ensure that all relevant safety issues were considered.

### Crash Investigation Officer

Name: Vasudevan Ramanathan – Senior Engineer (Civil), Fitzroy District

Signature:

Date:

Contact Details: (07) 4931 1592, Vasudevan.Z.Ramanathan@tmr.qld.gov.au

## 7 Review

### 7.1 Immediate Interim Remedial Measures

In the course of this road crash investigation, no safety issues were identified with the existing road infrastructure on this section of road that contributed to this accident or that present an imminent danger in the immediate future. As such, no immediate interim remedial measures are recommended for this intersection.

### 7.2 Actions that Permanently Reduce Road Safety Risk

The crash investigation did not identify any safety-related issues with the road infrastructure that require long term remedial work or any minor works in addition to what is already being planned and implemented as part of the ongoing maintenance program.

## 8 Warrant for Stage 2 Investigation

Trigger	Yes/NO (If answer is Yes to any one of the triggers, then stage 2 investigation is needed)	Explanation
Investigation finds that road infrastructure at the site poses significant road safety risks, even though these may not have contributed to the crash	No	
Investigation finds that there is a possibility that the road infrastructure contributed to the likelihood and/or the severity of the crash	No	
Investigation finds that the crash occurred at an emerging crash site	No	
Investigation identifies the crash site as a high risk location	No	

**APPENDIX A - QPS CRASH REPORT**

Released under RTI - DTMR

Date Extracted: 25 March 2019

### Crash

**Crash Number** : Sch.4 Part 4 s.6 PI  
**Date** : 24-Mar-2019  
**Units Inv** : 2  
**Crash Scope** : In scope  
**Lat (GDA94)** : -23.245938  
**Street** : Artillery Rd (Preliminary QPS Location)  
**Street Int** : Yeppoon Rd (Preliminary QPS Location)  
**Landmark** :  
**Suburb** : Ironpot  
**Police Div** : Yeppoon  
**Roads Dis** : 404 [Fitzroy]  
**Road Section** :  
**Thru dist** :  
**Crash Nature** : Angle  
**Speed Limit** : 100  
**Surface Cond** : Sealed - Dry  
**DCA Group** : 1 [Intersection from adjacent approaches]  
**Severity** : Fatal  
**Day** : Sunday  
**Casualties** : 7  
**Validation** : Not started  
**Long (GDA94)** : 150.608555  
**LGA** : Livingstone Shire  
**Police Dis** : Capricornia  
**Roads Reg** : 314 [Central Queensland]  
**Authority** : Not Entered  
**Trans Reg** : Central  
**Police Reg** : Central  
**ARMIS Dis** :  
**Traffic Cont** : Give way sign  
**Atmo Cond** : Clear  
**Horiz Align** : Straight  
**Roadway Feat** : Inter - Cross  
**Light Cond** : Daylight  
**Vert Align** : Level  
**DCA Code** : 101 [VEH'S ADJACENT APPROACH: THRU-THRU]

### Description

### Unit 1

**Unit Type** : Car/Station wagon  
**Status** : Unknown  
**Intended Act** : Go straight ahead  
**Direction** : West  
**# Occupants** : 0  
**State Reg** : QLD  
**Danger Goods** : Unknown  
**On Street** : Invalid Street  
**# Casualties** : 4  
**Towing** : No  
**Overall Dmg** : Major - towed away  
**Controlled? Licence** : Sch.4 Part 4 s.6 PI  
**Gender** :  
**State** : Unknown  
**Age** : Sch.4 Part 4 s.6 PI

**Contributing Circumstances**  
NOT APPLICABLE

-----  
**Casualty - Unit 1 - Occupant 1**

**Severity** : Hosp  
**Road User** : Driver

**Gender** :   
**Restraint** : Restrained

**Age** :   
**Helmet** : Not applicable

-----  
**Casualty - Unit 1 - Occupant 2**

**Severity** : Fatality  
**Road User** : Passenger

**Gender** :   
**Restraint** : Not determined

**Age** :   
**Helmet** : Not applicable

-----  
**Casualty - Unit 1 - Occupant 3**

**Severity** : Hosp  
**Road User** : Passenger

**Gender** :   
**Restraint** : Restrained

**Age** :   
**Helmet** : Not applicable

-----  
**Casualty - Unit 1 - Occupant 4**

**Severity** : Hosp  
**Road User** : Passenger

**Gender** :   
**Restraint** : Restrained

**Age** :   
**Helmet** : Not applicable

=====  
**Unit 2**

**Unit Type** : Car/Station wagon  
**Status** : Unknown  
**Intended Act** : Go straight ahead  
**Direction** : North

**# Occupants** : 3  
**State Reg** : QLD  
**Danger Goods** : Unknown  
**On Street** : Invalid Street

**# Casualties** : 3  
**Towing** : No  
**Overall Dmg** : Major - towed away

**Controlled? Licence** :

**Gender** :   
**State** :

**Age** :

**Contributing Circumstances**  
NOT APPLICABLE

-----  
**Casualty - Unit 2 - Occupant 1**

**Severity** : Hosp  
**Road User** : Passenger

**Gender**   
**Restraint** : Restrained

**Age**   
**Helmet** : Not applicable

-----  
**Casualty - Unit 2 - Occupant 2**

**Severity** : Hosp  
**Road User** : Driver

**Gender**   
**Restraint** : Not determined

**Age**   
**Helmet** : Not applicable

-----  
**Casualty - Unit 2 - Occupant 3**

**Severity** : Hosp  
**Road User** : Passenger

**Gender**   
**Restraint** : Restrained

**Age**   
**Helmet** : Not applicable

Released under RTI - DTMR

**APPENDIX B - CRASH SITE INVESTIGATION CHECKLIST**

Released under RTI - DTMR

**APPENDIX C - SITE PHOTOGRAPHS**



***Looking toward the intersection from Dairy Inn Road***



***Looking north-east from the southern corner of Dairy Inn Road and Rockhampton-Yeppoon Road, note fatigue cracking and patching in the pavement in the left-out turning area of the intersection***





*Looking left from Dairy Inn Road, south-west towards Rockhampton*



*What appears to be a structural failure of an underlying pavement (left turn out from Dairy Inn Road to Rockhampton-Yeppoon Road), looking north-east toward Yeppoon*



*Looking back down the road towards the south-west, at the turnoff onto Artillery Road. Note linemarking is in good condition*



*Just south of the turnoff onto Artillery Road, looking towards the north-east*



*Looking back south from the northern lane of Artillery Road*



*Looking left out of Artillery Road towards the north-east direction of Rockhampton-Yeppoon Road at the final crash site*



***Looking Straight ahead from Artillery Road across Rockhampton-Yeppoon Road towards Dairy Inn Road***

**APPENDIX D - ROADCRASH2 10-YEAR CRASH HISTORY**

Released under RTI - DTMR



**Crash Types**

Crash Dates  -  Alignment: Vertical

Owner  Horizontal

DCA Code  Feature

Group  Traffic Ctrl

Speed Limit

Fatalities =  Contrib Circ.

Severity  Unit Type

Nature  Risk Factor

**Area** LGA  SLA  Police Division

**Road Sections**

All Road Sections  Include Crashes on:  Thru road Mid-block  Thru roads at Intersections  Intersecting roads at Intersections

**Intersections**

All Intersections

Intersection	Number of Crashes					Total
	Fatal	Hosp.	Medical	Minor	PDO	
<input type="text" value="4991 Yeppoon Rd &amp; Dairy Inn Rd, Artillery Rd"/>	<input type="text" value="0"/>	<input type="text" value="3"/>	<input type="text" value="0"/>	<input type="text" value="0"/>	<input type="text" value="2"/>	<input type="text" value="5"/>

Not Relevant

Released under RTI - DTMR

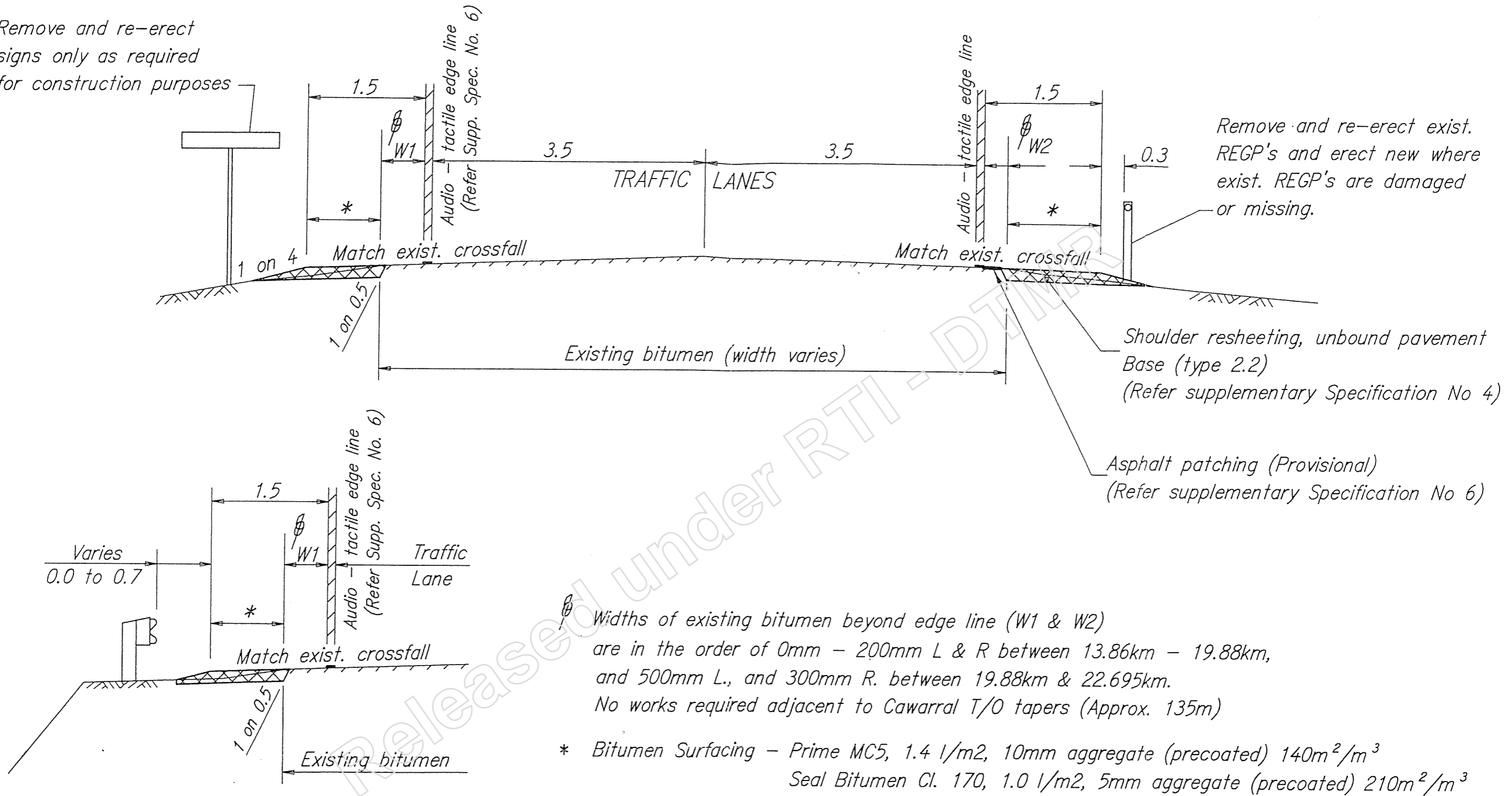
**APPENDIX E – SITE PLANS**

Released under RTI - DTMR



Remove and re-erect signs only as required for construction purposes

Remove and re-erect exist. REGP's and erect new where exist. REGP's are damaged or missing.



TREATMENT ADJACENT TO GUARDRAIL

**TYPE CROSS SECTION**

CAD DWG: JWBKKE VIEWS: 2	Revisions	Certified	Date	Microfiled	Associated job nos	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	Survey books	LIVINGSTONE SHIRE				TYPE CROSS SECTION						
						Auxiliary plan nos	Through chainage from Rockhampton 18.3km	ROCKHAMPTON - YEPPOON ROAD				JOB No. 77/196/722			No. 1 of 1 plans			
						Scales 0 0.5 1.0 1.5 2.0 SCALE 1:50 m	Reference Points	Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Bds		Drawn	Desian	Examined	Plan No. 274607
						(Office use only)	196/5	1.19	8.835	2.195	196/7	Ckd	Ckd		NR	NR	Manager (I.D.) 10/11/96 Central District	A



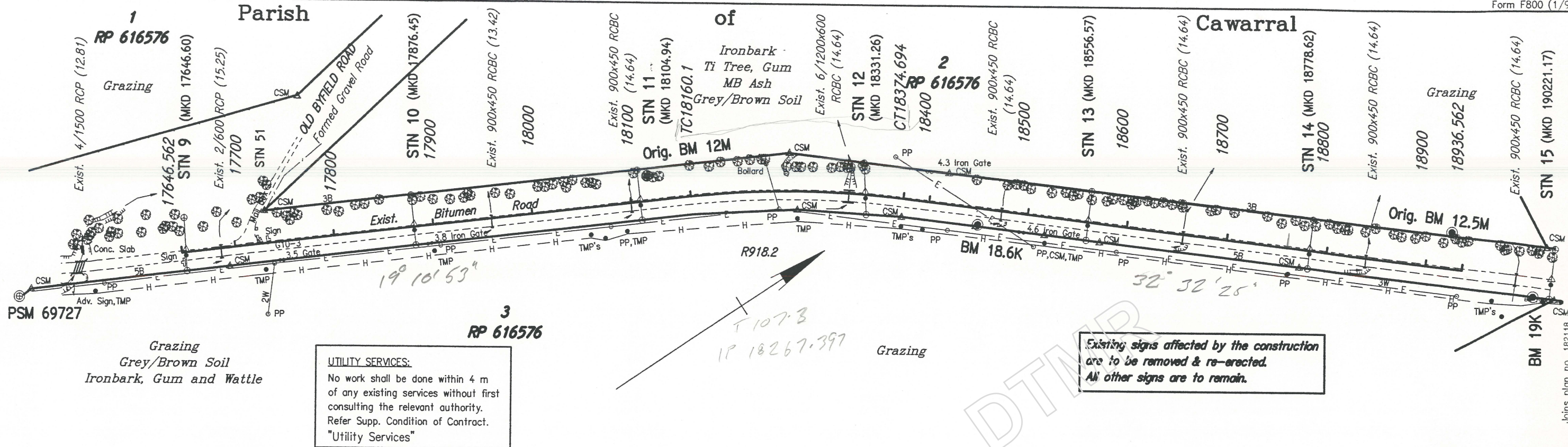
Joins plan no. 274868

PSM 69727

Origin of survey(s)  
(For information only)

AHD Correction: -

Joins plan no. 182118



**UTILITY SERVICES:**  
No work shall be done within 4 m of any existing services without first consulting the relevant authority. Refer Supp. Condition of Contract. "Utility Services"

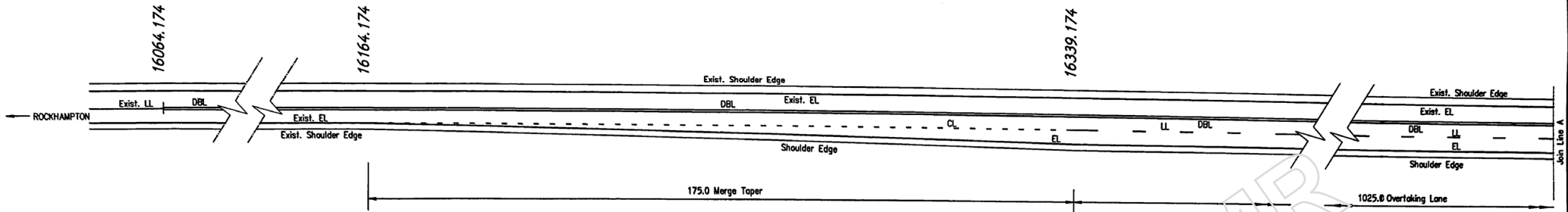
Existing signs affected by the construction are to be removed & re-erected. All other signs are to remain.



4A Exist 2/600 RCP LENGTHEN 1.22 LHS INTERSECTION L		4B Exist 900x450 RCBC LENGTHEN 2.4 LHS		4C Exist 900x450 RCBC LENGTHEN 3.6 LHS		4D Exist 6/1200x600 RCBC LENGTHEN 2.4 LHS		4E Exist 900x450 RCBC LENGTHEN 3.6 LHS		4F Exist 900x450 RCBC LENGTHEN 2.4 LHS		4G Exist 900x450 RCBC LENGTHEN 1.2 LHS																																																						
Unbound Pavement Base (Type 2.1)(150mm x Widening LHS only) Unbound Pavement Subbase (Type 2.3) (125mm x Widening LHS only) Unbound Pavement Subbase (Type 2.5) (2+5mm Widening LHS only) Bitumen Surfacing - 12.0 (10.5 + 1.5 shoulder LHS nom.)																																																																		
Existing Road (Control Line No. 3)																																																																		
Intersection LHS																																																																		
Overtaking Lane LHS																																																																		
175.0 Taper LHS																																																																		
DATUM 30.000																																																																		
Pvt mkg Refer Plan Nos. 274913 & 274914																																																																		
Type B																																																																		
Earthwork	Cut	104.5	270.3	211.5	220.5	205.7	142.8	162.7	138.2	158.0	177.7	185.7	249.8	212.5	83.9																																																			
	Fill	3.2	0	0.2	0.4	12.1	109.7	57.9	123.1	24.9	8.7	12.3	0	9.2	0																																																			
Grade Height	46.012	45.930	45.821	45.750	45.682	45.618	45.580	45.510	45.420	45.354	45.266	45.179	45.106	44.970	44.873	44.772	44.688	44.585	44.483	44.392	44.280	44.146	44.024	43.953	43.908	43.823	43.767	43.739	43.665	43.604	43.538	43.479	43.446	43.427	43.437	43.407	43.375	43.326	43.260	43.163	43.050	42.936	42.853	42.784	42.710	42.590	42.490	42.433	42.345	42.242	42.132	42.046	41.958	41.877	41.795	41.700	41.579	41.488	41.435	41.354	41.277	41.162	41.054	40.954	40.888	40.821
Surface Height	46.012	45.930	45.821	45.750	45.682	45.618	45.580	45.510	45.420	45.354	45.266	45.179	45.106	44.970	44.873	44.772	44.688	44.585	44.483	44.392	44.280	44.146	44.024	43.953	43.908	43.823	43.767	43.739	43.665	43.604	43.538	43.479	43.446	43.427	43.437	43.407	43.375	43.326	43.260	43.163	43.050	42.936	42.853	42.784	42.710	42.590	42.490	42.433	42.345	42.242	42.132	42.046	41.958	41.877	41.795	41.700	41.579	41.488	41.435	41.354	41.277	41.162	41.054	40.954	40.888	40.821
Chainage	17646.662	17660	17680	17700	17720	17740	17760	17780	17800	17820	17840	17860	17880	17900	17920	17940	17960	17980	18000	18020	18040	18060	18080	18100	18120	18140	18160	18180	18200	18220	18240	18260	18280	18300	18320	18340	18360	18380	18400	18420	18440	18460	18480	18500	18520	18540	18560	18580	18600	18620	18640	18660	18680	18700	18720	18740	18760	18780	18800	18820	18840	18860	18880	18900	18920	18936.562
Horiz. curve data	R918.2 3% Superelevation													CT18374.694																																																				

Revisions	Certified	Date	Microfiled	Associated job nos	Bench Marks				0 20 40 60	Through chainage from	Survey bks:	Dimensions in metres	LIVINGSTONE SHIRE		JOB No. 77/196/19	No. 4 of 8 plans Plan No. 274881
					B.M.	on	X Co-ord	Y Co-ord	Ht.	0 2 4 6	ROCKHAMPTON	except where shown	ROCKHAMPTON - YEPPOON ROAD			
					PSM69727	DDR	259915.239	428219.547	47.114	Vertical Section	20.7kms - 22.0kms	otherwise.	CTL CHGE STN8(MKD17446.82)-STN15(MKD19021.17)			
					BM 12M	Ironbark	260159.766	428811.360	43.358	Reference Points	196 - 278	Culvert sizes in	Approved			
					BM 18.4K	Star Pkt	260380.690	429063.464	42.556	Preceding	196 - 279	millimetres.	Approved			
					BM 12.5M	Ironbark	260645.421	429457.243	40.370	Dist. to start			Approved			
					BM 19K	Star Pkt	260742.480	429488.963	40.257	of job (km)			Approved			
A Original issue					Instrument Stations				Survey Bays				Approved			
					No.	X Co-ord	Y Co-ord	Ht.	Ckd	Ckd			Approved			
					8	259870.821	428209.871						Approved			
					9	259968.706	428384.089						Approved			
					51	260002.521	42850.868						Approved			
					10	260072.543	428589.152						Approved			
					11	260175.763	428792.997						Approved			
					12	260292.304	428987.007						Approved			
					13	260437.746	429159.084						Approved			
					14	260581.084	429328.673						Approved			
					15	260737.655	429513.918						Approved			

Speed Environment km/h 110



**SCHEDULE OF MAINTENANCE FOR ROAD LIGHTING**

THE ROAD LIGHTING DESIGN BASED ON THE FOLLOWING MAINTENANCE SCHEDULE

1. BULK LAMP REPLACEMENT CARRIED OUT AT 36 MONTH INTERVALS. AT THIS TIME THE FOLLOWING SHALL ALSO OCCUR:

- ALL OPTICAL SURFACES, BOTH INTERNAL AND EXTERNAL, OF THE LUMINAIRE SHALL BE CLEANED.
- ALL GASKETS SHALL BE CHECKED FOR DETERIORATION AND REPLACED WHERE NECESSARY.
- DAMAGED, WEATHERED VISORS SHALL BE REPLACED.
- ALL ACCESSIBLE SCREWS, NUTS, etc SHALL BE REPLACED.
- A VISUAL CHECK SHALL BE MADE OF THE ELECTRICAL COMPONENTS AND WIRING FOR SIGNS OF OVERHEATING.
- IF REQUIRED, THE LUMINAIRE SHALL BE REALIGNED OR ADJUSTED TO THE DESIGN SPECIFICATION.

2. IT IS RECOMMENDED THAT INSPECTION PATROLS PROVIDE SPOT LAMP REPLACEMENTS SUCH THAT THE MAXIMUM LEVEL OF LUMINAIRE OUTAGES AT ANY ONE TIME IS NOT GREATER THAN 5% OF THE LUMINAIRE POPULATION eg. SERVICE AVAILABILITY SHOULD BE AT LEAST 95% (AS PER THE REQUIREMENTS OF AS 1158.1.3)

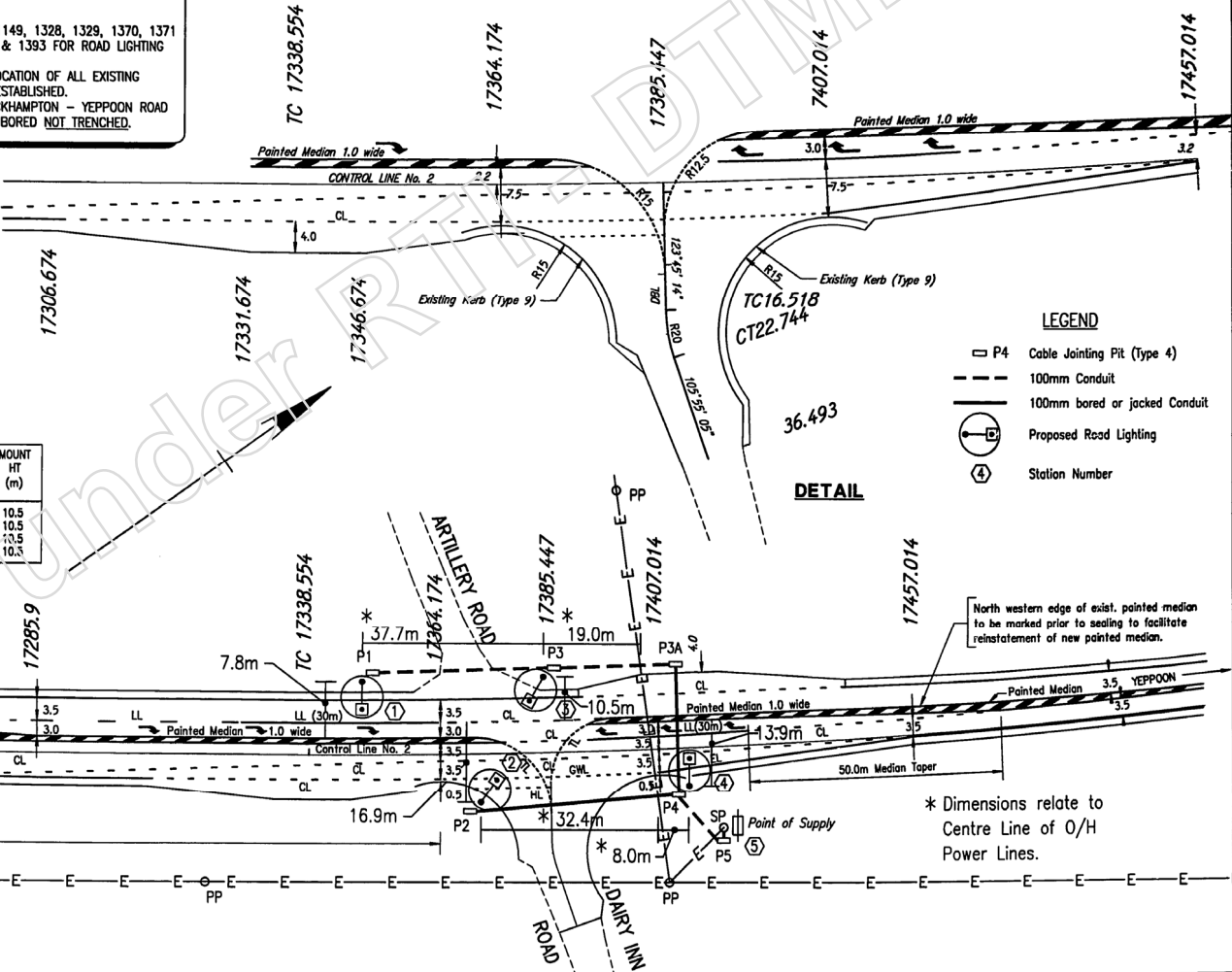
3. WHERE LUMINAIRES ARE REPLACED THE NEW LUMINAIRE SHALL BE OF EQUIVALENT TYPE SUCH THAT THE INTEGRITY OF THE ROAD LIGHTING DESIGN IS MAINTAINED.

**NOTES:**

1. REFER STANDARD DRAWINGS 1149, 1328, 1329, 1370, 1371, 1372, 1380, 1381, 1382, 1392 & 1393 FOR ROAD LIGHTING INSTALLATION REQUIREMENTS.
2. PRIOR TO EXCAVATION THE LOCATION OF ALL EXISTING CABLING/CONDUITING IS TO BE ESTABLISHED.
3. CONDUITS CROSSING THE ROCKHAMPTON - YEPPOON ROAD AND DAIRY INN ROAD SHALL BE BORED NOT TRENCHED.

**ROAD LIGHTING SCHEDULE**

STN No	POLE						LUMINAIRE						OUTREACH BRACKET			MOUNT HT (m)
	CHAINAGE (m)	OFFSET (m)	COMP ID	EXIST	ERECT (m)	IIN	RECOVER		ERECT		IIN	EXIST (m)	ERECT (m)	IIN		
							LUMINAIRE	CUST	LUMINAIRE	CUST						
1	17349	14.5 L	PO1		8.5SB	1392	SL1		S150C	DMR	6635	4.5	3440, 2699	10.5		
2	17372	9.5 R	PO1		8.5SB	1392	SL1		S150C	DMR	6635	4.5	3440, 2699	10.5		
3	17395	15.0 L	PO1		8.5SB	1392	SL1		S150C	DMR	6635	4.5	3440, 2699	10.5		
4	17414	7.5 R	PO1		8.5SB	1392	SL1		S150C	DMR	6635	4.5	3440, 2699	10.5		



**LEGEND**

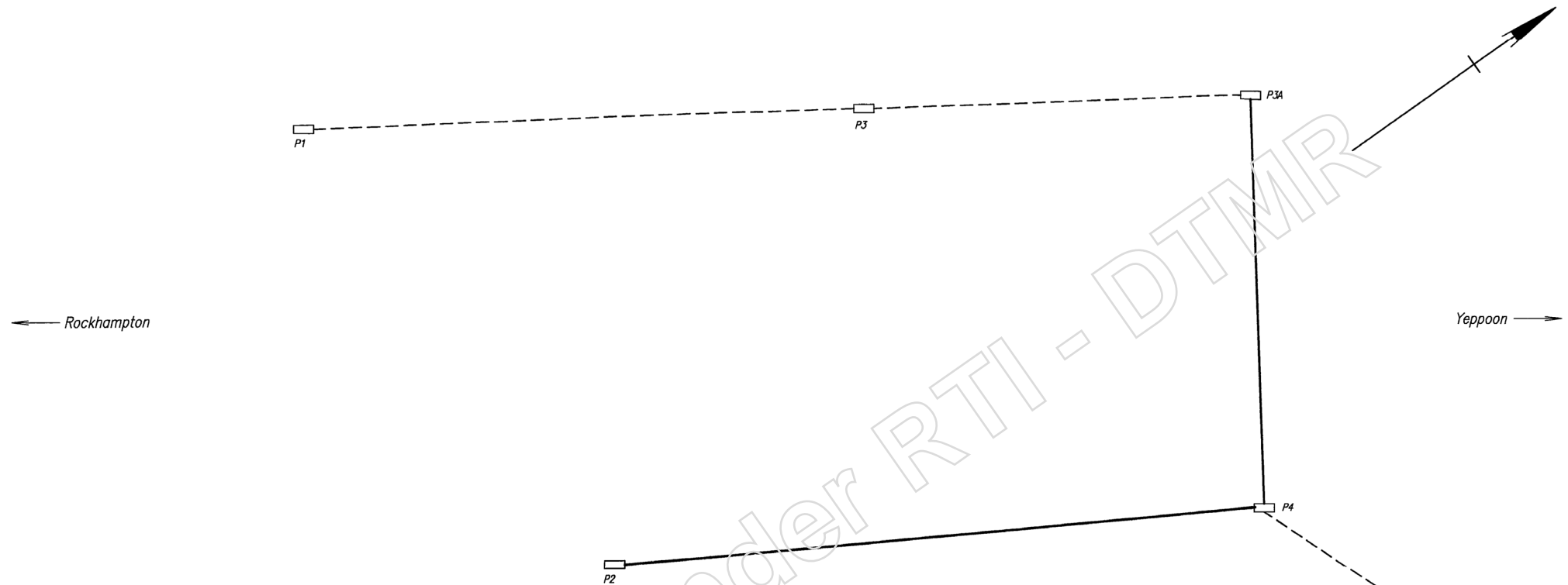
- P4 Cable Jointing Pit (Type 4)
- 100mm Conduit
- 100mm bored or jacked Conduit
- Proposed Road Lighting
- ④ Station Number

**DETAIL**

North western edge of exist. painted median to be marked prior to sealing to facilitate reinstatement of new painted median.

\* Dimensions relate to Centre Line of O/H Power Lines.

Revisions	Certified	Date	Microfilmed	Associated job nos	Dimensions in metres except where shown otherwise	Survey books	LIVINGSTONE SHIRE			Queensland Government								
					Culvert sizes in millimetres.		ROCKHAMPTON - YEPPOON ROAD			Department of Main Roads								
				Auxiliary drawing nos	Scales	Through chainage from	FLAG LIGHTING AND PAVEMENT MARKING			JOB No. No. 1 of 2 drgs								
				329725	NOT TO SCALE	ROCKHAMPTON	CTL CHGE 16164.174 - 17457.014			77/196/203 Drawing No. 329724								
B As Constructed		14/06/03				19.2kms - 20.5kms	Reference Points			Approved Orig. plan signed by T. Hill 5/11/02								
A Original issue				(Office use only)			Preceding RPC	Dist. to start of job (km)	From start to end of job	From end to following RPC	Following RPC	Survey Bkds	Drawn BJB	Design BJB	Examined	Certified	Approved	
												Ckd	Ckd	Ckd	Ckd	Ckd	Ckd	Contract No.
																		77/196/203
																		329724



**UNDERGROUND CABLE SCHEDULE**

LOCATION	STATIONS FROM TO	VOLTS	IN	CABLE SIZE TYPE	No. CABLES IN CONDUIT	TOTAL LENGTH	REMARKS
Rockhampton - Yeppoon Road (Intersection of Dairy Inn Road & Artillery Road)	P1 - P3	240	X	16mm 2C PVC/PVC	1	35	New Cable
	P3 - P3A	240	X	16mm 2C PVC/PVC	1	23	New Cable
	P3A - P4	240	X	16mm 2C PVC/PVC	1	26	New Cable
	P4 - P5	240	X	16mm 2C PVC/PVC	1	13	New Cable
	P2 - P4	240	X	16mm 2C PVC/PVC	1	40	New Cable

**Circuit Notes**

- Maximum Voltage Drop from Supply Point - 5%
- Fault Loop Impedance calculated using the following: 4 sec disconnect time, Type C 25A Circuit Breaker, Ia of 4.5 times rated C/B current
- Installation is to comply with the requirements of AS3000 2000

**Lighting Notes:**

- Luminaires are to be only as specified on these drawings.
- Any possible variations to pole locations considered necessary prior to installation or in the field, are to be checked with the Superintendent.
- Manufacture - GEC Optispan S150C luminaires to have Power Factor of 0.8 or above

**LEGEND**

- P12 Cable Joining Pit (Type 4)
- 100mm Conduit
- 100mm bored or jacked conduit

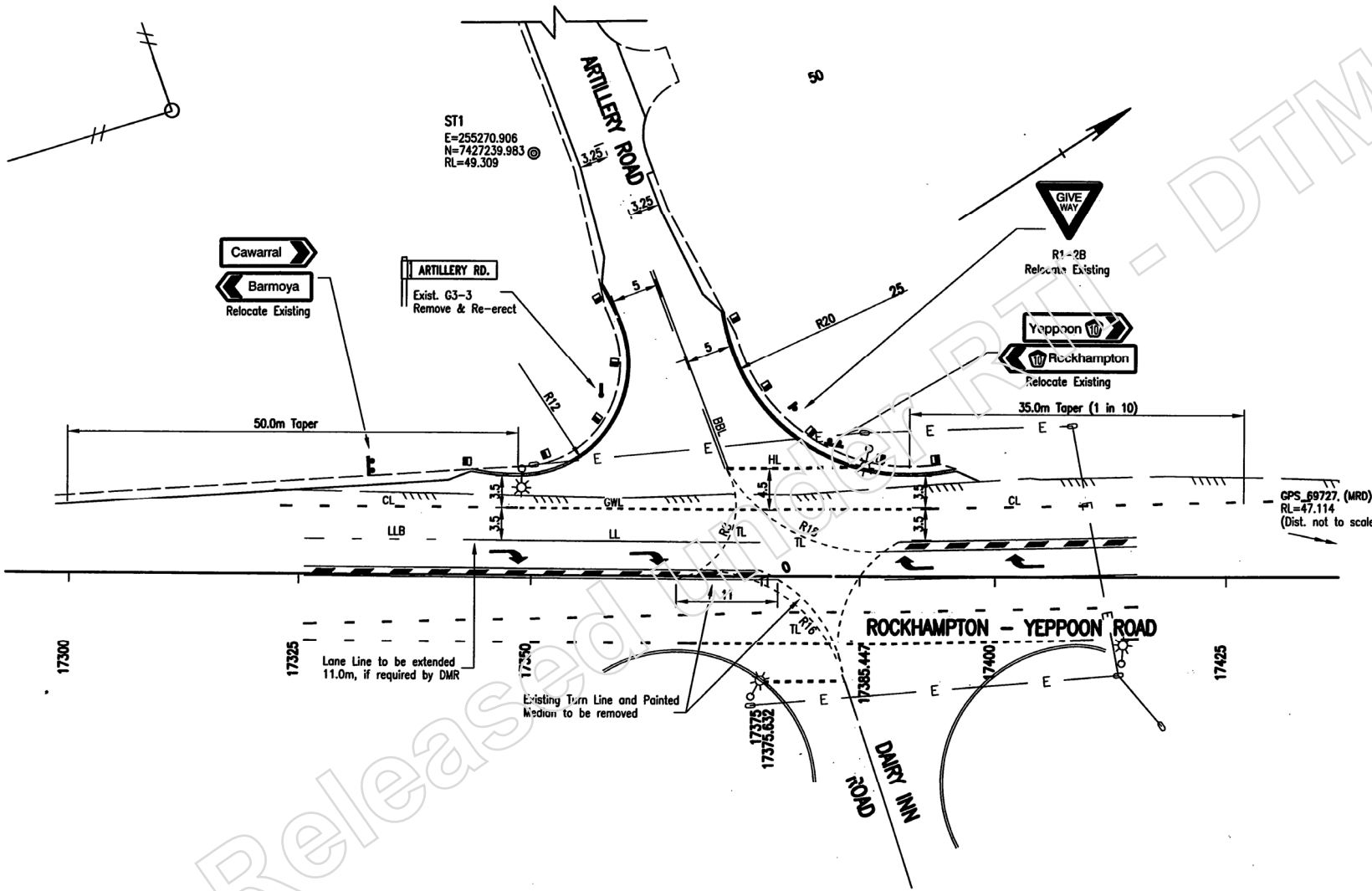
LOCATION	PHASE	CIRCUIT	STATIONS	LOAD	STARTING CURRENT(A)	RUNNING CURRENT(A)	C/B	COMMENTS
Rockhampton - Yeppoon Road (Intersection of Dairy Inn Road & Artillery Road)	A	1L1	1,2,3,4	4x150W	4.4	3.36	25A	Existing S/B New 25A C/B to be installed.



Revisions	Certified	Date	Microfiled	Associated job nos	Dimensions in metres except where shown otherwise. Culvert sizes in millimetres.	Survey books	<b>LVINGSTONE SHIRE</b>	<b>CIRCUIT DETAILS</b>	<b>Queensland Government</b>
				Auxiliary drawing nos	Culvert sizes in millimetres.	Through chainage from ROCKHAMPTON 19.2kms - 20.5kms	<b>ROCKHAMPTON - YEPPOON ROAD</b>	<b>FLAG LIGHTING</b>	<b>Department of Main Roads</b>
				329724	Scales		<b>CTL CHGE 17349 - 17414</b>	<b>CAWARRAL TURNOFF</b>	<b>JOB No. 77/196/203</b>
				(Office use only)	NOT TO SCALE		<b>Reference Points</b>	<b>Certified (See above)</b>	<b>No. 2 of 2 drgs</b>
A	Original issue						Preceding RPC	Disl. to start of job (km)	From start to end of job
							From end to following RPC	Following RPC	Survey Bkds
							Ckd	Ckd	Drawn BJB
							Design BJB	Examined	Certified (See above)
							Ckd	NR	Grant Anderson
								RPEUO 2239	NR
									Contract No.
									A
									CAD REF: G:\Transp\B&M\329724-329725.dwg



Parish of Cawarral



**LEGEND**

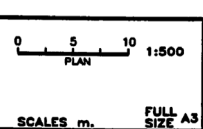
.....	Gravel Edge
-----	Bitumen Edge
———	Top of Embankment
———	Toe of Embankment
—E—	Underground Electricity
—EO—	Aboveground Electricity
———	Fence
———	Surface Drain
———	Shoulder Line (new)
■	Road Edge Guide Post

**TELSTRA CABLE : No work is to be carried out within 3.0m of cable without first consulting Telstra.**

**NOTE: Survey stations required for setting out of this job are indicated thus (⊙). Please contact Design Office if removal is necessary.**

Mark Description : GPS 69727 (MRD)  
 Location : Int. Yepoon Rd & Artillery Rd  
 Datum : AHD RL : 47.114  
 Derived from : Form 6 :   
 Surveyed :  
 Title 4/2002 LEVEL DATUM

Field Bk	AMENDMENTS DESCRIPTION	APPROVED	DATE
E/F Bk	12370DAK		
Level Bk			
D/O File	1237GA		



**LIVINGSTON SHIRE**  
 FROM THE BUSH TO THE BAYS  
 70 Anzac Parade Phone (07) 4939 3388  
 PO Box 600 Fax (07) 4939 3290  
 Yepoon Queensland 4703  
 E-mail : general@livi.ignition.qld.gov.au

Design/Dr	G D Alexander	17/10/03
Checked	S.G.S.	17.10.03
Recom'd		17/10/03
Approved	NR	17/10/03

**Queensland Government**  
 Department of Main Roads  
 District Director Date  
 Plan Description  
**ROADWORKS CONSTRUCTION**  
**ARTILLERY ROAD**  
 IRON POT  
 SIGNS AND LINEMARKING

QMR JOB No.	NR	1/10/03	QMR PLAN No.	347356
Auxiliary Plans	8391-8396		PLAN No.	8396
Job Mneumonic	Q3ARTR1-9/		SHEET	6
Construction Date	11/03		OF	6

**APPENDIX F – MEDIA CLIPPINGS**

Released under RTI - DTMR



# FAST-TRACK FOR \$64M YEPPOON ROAD UPGRADE

Horror accident sparks urgent Government funding promise to fix dangerous road following Labor's campaign pledge, P2



CRASH SITE: Flowers left at the intersection of Artillery Rd, Dairy Inn Rd and Yeppoon Rd. Photo: Allan Reinikka ROK280319aflowers

## LNP promises funding

### Yeppoon Rd secures \$64 million

MADDELIN MCCOSKER

[Maddelin.McCosker@capnews.com.au](mailto:Maddelin.McCosker@capnews.com.au)

**EXCLUSIVE:** THE Federal Government has bolstered its recently announced Roads of Significant Importance initiative by dedicating an extra \$64 million for an upgrade to the Rockhampton to Yeppoon Road following a tragic fatal crash last week.

LNP's Member for Capricornia Michelle Landry yesterday said she had lobbied Deputy Prime Minister Michael McCormick for the funding last week, saying urgent action was needed on the road after 20 people had been killed in the past 20 years.

"I do believe we have to do some serious upgrades to that intersection, sooner rather than later," she said.

"It is getting busier all the time and I think now the time has come for us to move forward on it and get this done."

The LNP commitment locks in the much-needed improvement for the stretch on the back of Labor's long-standing commitment to upgrade the busy road in three stages.



GETTING ATTENTION: Glenda Mather at the intersection of Artillery Rd, Dairy Inn Rd and Yeppoon Rd. Inset top left: Federal Member for Capricornia Michelle Landry. Inset bottom right: Labor candidate Russell Robertson. Photo: Allan Reinikka ROK280319ayeppoon

wards, would focus on creating a dual carriageway between Ironpot and Limestone.

It is in this section of the road where many of the 20 lives have been lost, including a nine-year-old girl last weekend. Labor candidate for Ca-

portance (ROSI) initiative, which has already promised \$190 million for the Yeppoon to Mount Isa road, the extra \$64 million for the Yeppoon Rd upgrade will see the Federal Government put \$254 million into the Roads of Significant

serious upgrades so it is safer for the amount of people that travel on it everyday."

Mr Robertson, however said he couldn't see any reason to not prioritise the upgrade until now.

"They have been in for six

this last time and we didn't win government so we were unable to carry that out."

The Rockhampton to Yeppoon road carries 12,000 vehicles every day, including 1200 heavy vehicles along the 40km

# Fast-track Yeppoon upgrade



MY SAY

FRAZER PEARCE

Editor

I KNOW we are heading into a Federal election campaign but the urgent attention for upgrading the Rockhampton Yeppoon Road is appropriate.

The \$64m commitment to improving the busy stretch was announced yesterday by Deputy Prime Minister Michael McCormick as part of the Australian Government's investment in the corridor to Mt Isa under its Roads of Strategic Importance initiative.

LNP's Member for Capricornia Michelle Landry successfully lobbied Deputy Prime Minister Michael McCormick for the funding last week, saying urgent action was needed on the road after 20 people had died in the past 20 years including the terrible crash last week that killed nine-year-old Zara Pakiappa.

This funding pledge follows Labor's long-held commitment to upgrade the road carried over from the 2016 election.

So the upshot is we will see action on this road no matter

who wins government.

That is good news for the people who travel the section that carries 12,000 vehicle movements a day including 1200 heavy vehicles.

I thought at the time in 2016 that the pledge by the Labor candidate of that election, Leisa Neaton, was a popular one - but history shows not as much as the Rockhampton Hospital carpark.

What I hope now, that politics can be put aside, is that the State Government takes immediate action on this project rather than wait for the Australian Government to disburse funds.

The cheque is in the mail and this community is crying out for action.

# Jamie still in critical condition

ROCKHAMPTON boy Jamie Pakleppa is still in an induced coma at Queensland Children's Hospital in Brisbane after sustaining injuries in a fatal car crash last on March 24.

The crash occurred about 9.40am between two cars at the Yeppoon Rd and Artillery Rd intersection.

Tragically, Jamie's sister Zara, 9, lost her life in the crash.

A Queensland Children's Hospital representative confirmed that as of 4.30pm Friday, Jamie was still in a critical but stable condition.

"We thank you for your continuous support and the generosity of our community is truly overwhelming," Pakleppa Automotive's Carissa Young posted on Facebook.

A Transport and Main Roads representative said the department was separately investigating the circumstances following Zara's death and the circumstances surrounding the crash.

**MB Yeppoon.** Speed cameras on the Rocky/Yeppoon road would be a better investment than double lanes. Seems Left lanes are for drivers within the speed limit and Right lanes are for speedsters!

**PM YEPPON** Cawarra Turnoff Intersection needs an independent expert investigation. Poor geometry angles and poor lane marking contribute to poor visibility of some vehicles.

# Funds raised for crash family



Jamie Pakleppa remains in a critical condition after the crash which claimed the life of his sister Zara.  
Photo: Facebook

THE Nerimbera Football Club has raised about \$1600 for a Capricorn Coast family devastated by a fatal car accident.

Zara Pakleppa, 9, died in the two-car crash on Yeppoon Rd on March 24 while her brother Jamie, 6, remains in a critical but stable condition in the Queensland Children's Hospital in Brisbane.

Nerimbera's head coach Scott Thomson, whose mother Lee knows the family, hatched the plan for the fundraiser which was organised around the club's home games at Pilbeam Park on Saturday.

Money was raised through a gold coin entry, a multi-draw raffle, a \$100 board and from items sold at the canteen.

Mr Thomson said Nerimbera's opponents that day, Central Football Club from Gladstone, also donated \$100 to the cause.

"It was a very successful day. I think the rain might have scared a few people away but we still had a large turnout," he said.

"It was really pleasing to see the support, and it's great that a small community can get together and do so much.

"About 95 per cent of people associated with our club would not know the family but they still dug deep to help them."

Mr Thomson said arrangements would be made to get the funds to the family as soon as possible.

# Yeppoon to Rocky road: Your say

THE Federal Government announced it was dedicating \$64 million to upgrade the Rockhampton to Yeppoon Rd and locals had a lot to say about the decision.

Read more to hear what the community had to say:

**Michael Chadbone:** Glad to see the LNP has finally come on board with this. Labor promised \$47.5 back in February.

**Jeff Philippi:** There are sections of Yeppoon Rd that need attention. It is not the section where the tragedy took place!

**Stacey Malinoo:** All our roads need upgrading...our roads are disgusting...Caves

to Rocky for starters...

**Kerl Henderson:** I don't understand how these accidents happen. I've driven that road sooo many times and have never had an issue.

**Garry Jensen:** Isn't it a state road, why would the federal government have to fund it?

**Jody Bayntur:** I think that Yeppoon road is ok, what I know is that there are some drivers out there that need to take more care. Same as it's not the gun that kills people, but the person using it. The same can be said for cars and blaming the state of the road is not fixing the problem.

**Brad Paton:** Hasn't this

been promised for the last two parliamentary terms?

**Willow Dunning:** People need to learn how to drive properly and slow down it's not a race to get from a to b. Well done to everyone who does drive decently

**Rebecca Irene Meg Thomasson:** Hopefully they do it this time. It's disgusting that a fatality had to happen for them to do something if it does happen. It's been a dangerous intersection for years.

**Keith Whittaker:** It always takes a tragedy to wake the polities up.

**Heather Morler:** Pity it took someone losing their

life before Michelle Landry got money to fix the road being a local she should have been chasing money well before this tragedy.

**Megan Cosgrove:** It's in Brittany Lauga's state electorate and within Bill Ludwig's council boundaries. Given that Labor has been governing Queensland for so long and it's Brittany Lauga's second term as the Labor representative for that electorate shouldn't your comments be directed to her? I say thank God Michelle Landry is getting something done.

**Albert Zarb:** Why not an overpass with adequate merging lanes?

# Danger was known, no action until a tragedy

RECEIVED a statement on my mobile put out by Brittany Lauga regarding the recent fatal accident in Yeppoon where a nine year old tragically lost her life.

In this statement Brittany Lauga claims she regularly travels this road and knows how dangerous this road can be and that she will bring pressure to authorities to upgrade this road to make it safe for motorists.

This is a question that I would like to ask Brittany - You have been aware of the dangerous situation this road represents for several years now.

Why does it have to take the loss of the life, a beautiful young girl, before you start jumping up and down over this road's condition?

**NWH Timms  
Bungundarra**



**DANGER ZONE:** Iron Pot resident Peg Payne lays flowers on the intersection where Zara Pakleppa lost her life.

PHOTO: JANN HOULEY

## We're committed to cutting the danger

RE NWH Timms' letter on April 2, I want to pass on my sincere condolences to both families involved in the recent horrible accident near Rockhampton that cut the life of a nine-year-old-girl far too short.

Since the accident I have spoken with both Queensland Police Service and the Department of Transport and Main Roads because I want a safety audit undertaken to improve the safety of this intersection. I also spoke in State Parliament just last week about this road.

Mr Timms can rest assured that since I was elected in 2015 I have been fighting for improvements to the Yeppoon-Rockhampton road.

In 2016, after lobbying the Minister for Main Roads, I secured \$250,000 to start the engineering for the Yeppoon-Rockhampton road to be turned into a four-lane corridor.

The preliminary indicative cost to four-lane the remaining 20km section of this road is estimated at \$200 million.

In addition to this, each year millions of dollars have been spent on this

road by the State Government, including signage improvements, line markings, and vegetation from the road corridor to improve safety.

Yeppoon Rd to Rockhampton is a busy road with more than 12,000 vehicles using it every single day, with almost 1200 of those being heavy vehicles.

More heavy vehicles are using that road.

As well, a growing number of tourists to the region are driving on it.

That extra traffic has slowed travel times and increased the potential for head-on crashes as more people try to overtake.

In February 2018, I was incredibly pleased when federal Labor made a \$475 million election commitment to the first stage of turning the Yeppoon-Rockhampton road into four lanes.

It's disappointing that it's taken the LNP six years and a looming election to finally find some money for the road – a full year after federal Labor pledged \$47 million to help deliver the first stage.

**Brittany Lauga MP**  
**Member for Keppel, Assistant**  
**Minister for Education**



# Pedestrian hit on Yeppoon Rd

EMERGENCY services responded to a report of a pedestrian injured after being struck by a vehicle travelling at 100km/h near Ironpot on the Yeppoon Rockhampton Rd yesterday.

The accident happened on the corner of Artillery Rd and was reported about 5.10pm.

Initial reports indicated one person was unconscious with head injuries.

# Main Roads reveals locations for Yeppoon Road's upgrade

MADDELIN MCCOSKER  
[Maddelin.McCosker@capnews.com.au](mailto:Maddelin.McCosker@capnews.com.au)

Both the major parties have committed \$64m for the project

THE Department of Transport and Main Roads has revealed the three sections of the Rockhampton to Yeppoon road that could be upgraded to dual carriageways.

After 20 fatalities in 20 years on the busy stretch, both major parties have committed \$64 million to upgrade the road.

Currently 10km of the road, between the Bruce Highway intersection and Ironpot, already has four lanes.

A Transport and Main Roads representative said in the upgrade the remaining 20km of the road would be turned into dual carriageways.

"In the past 12 months we have completed strategic planning to determine options to four-lane the remaining 20km section of Rockhampton-Yeppoon Rd, between Ironpot and Yeppoon," the representative said.

The planning identified three stages to the upgrade.

Stage one has been identified as the stretch of road between Tanby Rd and Neils Rd, about 5km.

Stage two is from Neils Rd to Mulara, about 2.3km.

Stage three, the largest of the three, runs from Mulara to Ironpot, about 14.2km.

Stages one and two already have a business case under way, and the department said it would start work on a business case for stage three this month.



# Man killed in traffic incident

POLICE are investigating a fatal traffic incident after a man was struck by a vehicle traveling 100km/h near Ironpot on the Yeppoon Rockhampton Rd on Sunday.

The man, aged in his 30s, was being transported to the Rockhampton Hospital in a critical condition when he died.

The incident happened on the corner of Artillery Rd and was reported about 5.10pm.

# Yeppoon Rd commitment won't even be enough for the first 5km

**EXCLUSIVE**

**MADDELIN MCCOSKER**

[Maddelin.McCosker@capnews.com.au](mailto:Maddelin.McCosker@capnews.com.au)

A *MORNING Bulletin* investigation has revealed that the \$64 million promised by both major parties to upgrade Yeppoon Rd will not be enough – it won't even be enough for the first five kilometres.

After 12 months of strategic planning, the Department of Transport and Main Roads found it would cost about \$95 million to make the road between the Tanby Rd roundabout and Neils Rd four lanes.

TMR have said in a previous statement to *The Morning Bulletin* the upgrade would happen in three stages, the first being Tanby Rd to Neils Rd, a stretch of approximately 5km.

The second stage is from

Neils Rd to Mulara, a stretch of approximately 2.3km, and the third is from Mulara to Ironpot, a stretch of approximately 14.2km.

The Department said in the statement that further planning is required to identify the potential cost for stage two and three.

As it is a state-owned road, funding is normally split 50/50 between the two levels of government and both Brittany Lauga and Michelle Landry have said they will gladly work with each other to see the upgrade become a reality.

With a reported estimate of \$200 million to upgrade the entire stretch of road, Member for Keppel Brittany Lauga said any money committed federally would be welcome.

"Any money from the Federal Government or Federal Labor is great and I will work with



**BLACK SPOT:** Motorists face traffic chaos after a horror crash on Yeppoon Rd in 2017.

Photo: 7 News CQ

whoever is in government to get those upgrades completed," she said.

"It is expensive to upgrade and I have already been in discussion with the Minister for Main Roads about funding for the State Government's share in the road upgrade."

Member for Capricornia Michelle Landry said she would work with the State Government to get the road upgraded but the upgrade needs to be more focused on the danger spots, rather than fixing the whole road.

"I always work with the De-

partment of Transport and Main Roads," she said.

"Obviously they are the ones that control this.

"I believe we need to be very focused on where the major accidents are happening and use that as a priority.

"It is not necessarily about the duplication the whole way through, it is about making some of those side roads and artillery roads safer for people coming onto the main section of the highway."

The Federal Government's commitment of \$64 million, according to Ms Landry, was a starting point to get the most pressing upgrades done first.

"I actually thought the \$64 million would assist in making the road safer in the areas that need it, like the areas with black spots and the side roads that are coming onto the main road," she said.

# Hospital after Ironpot smash

A WOMAN was taken to hospital after a crash on Thursday night.

The QAS reported a female patient with neck pain was transported in a stable condition to Rockhampton Hospital following a road traffic incident on Yeppoon Road and Dairy Inn Road at Ironpot at 6.35pm.

# Horror crash on Yeppoon Rd

## One girl killed, one boy critical

KERRI-ANNE MESNER AND MICHELLE GATELY

A GIRL aged nine has died after receiving fatal injuries in a two-vehicle crash on Yeppoon Rd yesterday morning.

She was one of seven people injured in the crash, including a six-year-old boy the Royal Flying Doctor Service transported to the Children's Hospital in Brisbane with life-threatening injuries yesterday afternoon.

Messages of condolences were left for her family as well as prayers for the little boy critically injured in the crash.

Rachel Hermann posted this message on *The Morning Bulletin's* Facebook post about the tragic news: "RIP young one. My heart breaks for both families right now.

"My condolences to the family who lost their girl. My thoughts are with the other family suffering from knowing the loss as well. It's going to be a



FATAL CRASH: One girl, 9, was killed and one boy, six, was critically injured in a serious crash on Yeppoon Rd at Ironpot.

Photo: Michelle Gately

## GIRL, 9, DIES IN HORROR CRASH, ANOTHER CHILD CRITICAL

Emergency services praise passing motorists who stopped

# Community rallies to support CQ family

THE community has rallied behind a grieving Livingstone Shire family after Sunday morning's tragic crash on Yeppoon Road.

The Pakleppa family, whose daughter Zara died after the incident, were yesterday praying for the recovery

of their son Jamie, who was in a critical but stable condition on Monday afternoon.

Zara, 9, and Jamie, 6, were in a sedan when it and another car collided at the Yeppoon and Artillery Rd intersection about 9.40am.

A 33-year-old female driv-

er, a 40-year-old female driver, and three girls aged 8, 10 and 12, were taken to the Rockhampton Base Hospital, all in stable condition.

Pakleppa Automotive posted on Facebook today asking

**CONTINUED ON PAGE 5**

## Support for grieving family

### **FROM PAGE 1**

for the public's support of the children's father Brendan (Wookie) and the Pakleppa family and that the business was to be closed until further notice.

But when other local mechanics stepped in to help out the grieving dad's business, it was announced the doors would stay open.

"Wookie is always the first one to put up his hand to help anyone whenever he can," Pakleppa Automotive's Carissa Young said.

"We are asking if anyone could help with the financial difficulties that the family are going to face in the near future at this devastating time and it would be greatly appreciated by all affected.

"There will be a donation box at the workshop."

**Steph Allen**

# 'IT'S LIKE RUSSIAN ROULETTE'



**DANGER ZONE:** Iron Pot resident Peg Payne lays flowers on the intersection where Zara Pakleppa lost her life. Photo: Jann Houley

“THE DANGEROUS INTERSECTION NEEDS TO BE UPGRADED WITH APPROPRIATE CONTROLS SO A TRAGEDY LIKE THIS DOESN'T HAPPEN AGAIN.

LIVINGSTONE MAYOR BILL LUDWIG

## Residents and Mayor call for urgent upgrade to Yeppoon Rd after little girl dies in horror crash at notorious intersection

### Community plea to fix Yeppoon Rd death trap

Livingstone mayor demands action

STEPH ALLEN  
Stephanie.Allen@capnews.com.au

THERE was an eerie silence on Sunday morning at Peg Payne's home on Artillery Rd – no sound of screeching tyres or crushed metal. It was just another weekend.

Then the wail of sirens punctured the calm and spelled out a devastating tragedy that would cause shockwaves throughout the community.

"I didn't hear the crash," Mrs Payne said.

"I can't believe I didn't. Normally I do when something happens on that intersection."

What occurred was the unthinkable – a nine-year-old girl lost her life at the intersection of Yeppoon and Artillery Rd where Mrs Payne has lived 1km away from for the past eight years.

One of the vehicles was travelling east on Yeppoon Rd

when it entered the middle of the intersection.

It then collided with the side of another car heading north on Artillery Rd.

"I could hear the screech of the police car's tyres when they were doing the skid test that afternoon after they had cleared the scene," Mrs Payne said.

"A couple of my friends were heading to the beach yesterday and they saw what happened, before the ambulance showed up.

"There were so many people pulled over trying to help resuscitate. Something needs to be done."

It was not the first accident to happen on this "dangerous" intersection, but it would be one to make the community stand up and take notice, Mrs Payne said.

"I've seen and heard of probably 10 near misses and accidents at that intersection (over the past eight years).



**DEVASTATING:** Several children were seriously injured and a girl died in a two-vehicle crash at the intersection of Yeppoon Rd and Arterial Rd at Ironpot. **INSET:** Local resident Peg Payne. Photo: Michelle Gately

"I have had a couple of near misses. One where I was nearly side-swiped and had to speed backwards really fast to avoid being hit. Some where I was just at the intersection waiting to turn.

"On a foggy morning, you can't see 15 feet in front of you. It's very busy, traffic is flowing and people are still doing 100km/h.

"My husband (Alex) says it's like Russian roulette."

After the outpouring of grief from the community on Facebook, many started to address what could be done to stop another tragedy happening.

Traffic lights, a roundabout and a speed limit reduction to 80km/h were some of the suggestions.

"They definitely need traffic lights there. If there were lights, there would be no way it would have happened," Mrs Payne said.

"Even if it's turned into an 80km/h zone, there's still not enough time to avoid an incident. It would make no difference. You need to stop.

"If you're coming from Yeppoon and turning right on to

**CONTINUED ON PAGE 4**

## Mayor's had enough of empty promises to fix intersection

FROM PAGE 3

Artillery Rd and a car from Yeppoon is turning right to Ca-warral, they're both going the opposite way and they can't see what's coming down on the inside lane at 100km/h," Mrs Payne said.

"You can't see what's coming and you're risking your life." Livingstone Mayor Bill Ludwig expressed his condolences yesterday morning.

"On behalf of the community, our condolences go out to all those impacted by the terrible tragedy," he said.

"It highlights just how im-

portant it is to have that entire road, with the volumes of traffic that are now on it, fully upgraded to four lanes all the way.

"The dangerous intersection needs to be upgraded with appropriate traffic controls so a tragedy like this doesn't happen again.

"It's up to the government to realise there's an issue there. Those warning signs they've put up telling people to slow down are really just band-aids and aren't good enough."

Cr Ludwig said investment in safety measures such as reduced speed limits, traffic lights or a roundabout were

“THE DANGEROUS INTERSECTION NEEDS TO BE UPGRADED WITH APPROPRIATE TRAFFIC CONTROLS SO A TRAGEDY LIKE THIS DOESN'T HAPPEN AGAIN.

CR BILL LUDWIG

"absolutely critical" for keeping the community safe.

"The Capricorn Coast is continuing to grow. It's the fastest growing area in the region and that comes with a responsibility by the state and federal governments to put in structures required to make it safe," he said.

"It has to start at the very

dangerous intersection.

"We've had promises over the years from politicians that the two-lane issue will be assessed and it's happened in dribs and drabs. It's not good enough.

"This has to be one of the highest priorities. It has to happen now."

Cr Ludwig said the number

of accidents on Yeppoon Rd was directly related to the lack of investment from successive governments.

"We've got a situation where cars from both directions are either coming out of or back into lanes," he said.

"There needs to be four lanes put in. We need the proper controls.

"This has to be one of the most frightening intersections on a state-controlled road in Australia.

"It's so easy to get distracted and if you're coming from Artillery Rd, it feels like a drive-through road."

As a grandfather, Cr Ludwig empathised with the devastation of Sunday's accident.

"I know how heart-wrenching it must be to have a tragedy like this," he said.

"I don't think anybody in the community will not be getting behind this cause. The state and federal governments must come together and help this appalling situation."

A Department of Main Roads and Transport spokesman said the department was aware of the tragic death and the circumstances surrounding the incident were being investigated.

## Supporters step forward to help grieving Pakleppa family

THE community has rallied behind a grieving Livingstone Shire family after Sunday morning's tragic crash on Yeppoon Rd.

The Pakleppa family, whose daughter Zara died after the accident, were yesterday praying for the recovery of their son Jamie who was in a critical but stable condition yesterday afternoon.

Zara, 9, and Jamie, 6, were in a sedan when it and another car collided at the Yeppoon and Artillery Rd intersection about 9.40am.

A 33-year-old female driver, a 40-year-old male driver, and three girls aged 8, 10 and 12, were taken to the Rockhampton Base Hospital, all in stable condition.

Pakleppa Automotive posted on Facebook yesterday asking for the public's support of the children's father Brendan (Wookie) and the Pakleppa family and that the business was to be closed until further notice. But when other local mechanics stepped up to help out the grieving dad's business, it was announced the doors would stay open. "Wookie is always the first one to put up his hand to help anyone whenever he can," Pakleppa Automotive's Carissa Young said.

"We are asking if anyone

could help with the financial difficulties that the family are going to face in the near future at this devastating time and it would be greatly appreciated by all affected.

"There will be a donation box at the workshop.

"After so many phone calls offering help to support the workshop in this time, the doors will remain open for all your mechanical needs with very trusted hand-picked mechanics and will guarantee to uphold Brendan's quality workmanship.

"The new operating hours for the workshop until further notice is as follows: Mondays 9am-2.30pm, Thursdays 9am-2.30pm and Fridays 9am-3.30pm."

A donation box can also be found at Little Barber & Hair Co, Rockhampton.

"Our hearts go out to Brendan, Kate, Jamie, Charlie and all the Pakleppa family at this terrible time," the Little Barber's Kerry Smith said.

"Nothing anyone says will help you feel any better but know you are all surrounded with so much love and support. "We all hold our breath for a good outcome for Jamie. Charlie needs him home. Sending so much love."

Lee's Family Daycare – Ca-

### Messages of support

**Lara Carter:** "So very sad, prayers to all those involved especially to Brendan who lost his little girl, my heart goes out to everyone."

**Paulette Flint:** "There are too many accidents on the Rockhampton Yeppoon road. Thinking of those involved and wishing them all a swift recovery."

**All Crosby:** "RIP little one. Praying your family and friends find the strength to get through this. I hope the little boy improves."

**Robyn Louisa:** "Condo-

lences and love sent to all involved in the accident and to families. Praying the young boy pulls through and makes a full recovery. My heart aches for you all."

**Diane Bianchi:** "Condolences to you all, thinking of you in this sad time."

**Jaz McKee:** "All our love and support, if you need anything don't be afraid to ask!"

**Donna Bateman:** "Our thoughts in prayers are with you in your family Brendan, from team at Easylift."

**Angie Fay:** "Oh Brendan!!!! Words just can't describe .... I am heartbroken

for your family! My deepest deepest sympathy."

**Nathan Jung:** "So sorry to hear, mate, no one deserves this, just unimaginable."

**Scott Lynch:** Wow Wooki, words can't express it really. My deepest condolences mate."

**Adam Mackle:** "Thinking of you and the family mate, if there is anything I can do you only have to let me know."

**Chris Irvine:** "Mate, I'm heart broken for you."

**Rohana Gorman:** "Words cannot express how sad we are for you, Kate and Charlie. Prayers for Jamie."

“WE ALL HOLD OUR BREATH FOR A GOOD OUTCOME FOR JAMIE. CHARLIE NEEDS HIM HOME. SENDING SO MUCH LOVE.

warral posted a tribute to the family yesterday morning.

"Please keep young Jamie in your thoughts, wishing him a speedy recovery and my deepest condolences go out to all the Pakleppa family on the loss of Jamie's sister, Zara," the page said. "My heart is breaking for them."

Member for Keppel Brittany Lauga posted on Facebook on

Sunday night about the devastating accident.

"I'm so terribly sorry to hear about the horrible crash that has already claimed a life today on Yeppoon-Rockhampton Road," she said.

"I'm hoping those fighting for their lives can hold on. "My sincerest wishes to the family and friends of those involved. Thank you to our

amazing emergency services personnel who not only responded quickly to the incident but also those who provide ongoing emergency medical care to everyone involved."

Charlotte and Annaleese Hermann were in the other car with their mother.

The eldest daughter, Anna-leese, was still at the Rockhampton Hospital as of 5pm yesterday afternoon and was reported to be in a stable condition.

The girls' mother was discharged yesterday. "[My] daughter broke down

when they notified her about the little girl in the other vehicle had passed away," the girls' grandfather Graeme Hermann said.

"Eldest granddaughter has a big bruise on her hip and was unconscious for a while and can't remember the accident.

"Going to be a long, slow healing process."

Emu Park State School principal Wayne McMurtrie released a statement on the loss of his student.

"The Emu Park State School community is deeply shocked and saddened by the death of one of our students," Wayne said. "The school community's thoughts and sincere sympathies go out to the family, caregivers, friends and loved ones during this difficult time.

"Support is being provided to students and staff, and counsellors will remain at the school to provide support in any way they can and for as long as it is needed."

**DONATE TO THE PAKLEPPA FAMILY**

Brendan Pakleppa  
BSB: 062692  
A/C: 29232418  
Commonwealth Bank



● **LB ROCKY.** After tragic road accident Sunday morning with Rocky Rd closed all traffic diverted to Emu Park Rd. Very disappointing to see speed camera just past Lakes Creek meatworks with so much traffic using road and a very long time since I have seen speed camera on that road.

● **RM FRENCH.** Two kids under 10 taken on our roads, how many families destroyed. We will never know why, all we can do is cry for them.

● **GUNNER.** The horrific and tragic accident at the dangerous, artillery road, dairy in road, and yeppoon road black spot intersection, highlights the urgent need to upgrade and make safe this killer crossing on a 100kmh highway it requires the two secondary intersecting roads to be zigzagged as with the Gavial Grace mere and Burnett Highway crossroads, that slows traffic before crossing the highway, the terrible truth is, it will most likely involve more tragic accidents before action is taken.

# Tragedy spurs on funding

JACK EVANS

[jack.evans@stimson.com.au](mailto:jack.evans@stimson.com.au)

## Horror Yeppoon-Rocky stretch needing urgent attention

AS THE community remains saddened by Sunday's tragic car accident that took the life of nine year-old Zara Pakleppa, the calls for upgrading the Yeppoon to Rockhampton stretch have intensified.

Yesterday, *The Morning Bulletin* reported a \$190-million dollar upgrade to the corridor between Yeppoon and Mount Isa from the Federal Government under the Roads of Strategic Importance Program.

Following yesterday's publication, Capricornia MP Michelle Landry told media that she was lobbying heads of government, including the Deputy Prime Minister to attract more of the funding to the dangerous stretch.

"The works will come down to advice from the Main Roads department, but I have been asked about the Yeppoon to Rockhampton stretch," she said.

"I'd love to see the whole Yeppoon to Rockhampton stretch four lanes, but I'm not

quite sure on what the cost factor of that would be.

"We know there are dangerous areas there and we saw a tragedy there on the Sunday.

"There needs to be some major work done there ASAP," Ms Landry said.

"I've spoken to Michael McCormack yesterday about the importance of this and he agreed."

It remains unknown exactly where, on the 1373 kilometre stretch, would receive the funding or whether extra funding would be allocated to the stretch.

This signifies attempts from both sides of the political divide to have the stretch upgraded due to safety concerns.

Labor doubled down on their commitments to duplicate the stretch of road earlier this year when Opposition Leader Bill Shorten visited the region.

They estimated the duplication of the stretch would cost \$475 million.



**URGENT:** Funding of \$190 million has been allocated to the ROSI program but some say more must be directed at Rockhampton-Yeppoon.

"We want to put dual lanes between the Rockhampton and Yeppoon (stage one)—that's \$475 million," Mr Shorten said.

At the same town hall meet-

ing, Labor candidate for Capricornia, Russell Robertson, expressed his concerns regarding the stretch.

"There have been at least 20 fatalities on the Yeppoon-

Rockhampton road in the last 20 years," Mr Robertson said.

"By duplicating the Rockhampton-Yeppoon road, we will improve road safety and create 150 local jobs."

Calls for action have also been received from Rockhampton Regional Council mayor Margaret Strelow and Livingstone Shire Council mayor Bill Ludwig.

● PM YEPPON. Cawarral Turnoff Intersection needs stop signs on the Artillery and Diary-Inn legs as interim treatments until full redesign is built.

**Page Number: 210 of 221**

# Calls to action on deadly section

Politicians respond unanimously

CHRISTINE MCKEE christine.mckee@capnews.com.au

THERE'S no dispute among political leaders who have put politics aside to call for action on a notorious intersection on Yeppoon Rd, which claimed the life of a nine-year-old girl on Sunday.

Zara Pakleppa died after a crash at the crossroad where the main connection road between Rockhampton and Yeppoon meets Artillery Rd.

Locals are calling for an 80km/h zone to be imple-



**DANGEROUS: Glenda Mather at the intersection of Artillery Rd, Dairy Inn Rd and Yeppoon Rd.**

mented at the intersection while investigations into the cause of the crash continue.

“Crossroad intersections like this are inherently the most dangerous intersections

to have on any major commuter route,” Livingstone mayor Bill Ludwig said.



## Jamie still in critical condition

JAMIE Pakleppa is still in an induced coma at Queensland Children's Hospital in Brisbane after sustaining injuries in a fatal car crash last Sunday.

The crash occurred about 9.40am Sunday between two cars at the Yeppoon Rd and Artillery Rd intersection.

Tragically, Jamie's sister Zara, 9, lost her life in the crash.

A Queensland Children's Hospital representative confirmed that as of 4.30pm yesterday, Jamie was still in a critical but stable condition.

"We thank you for your continuous support and the generosity of our community is truly overwhelming," Pakleppa Automotive's Carissa Young posted on Facebook.

A Transport and Main Roads representative said the department was separately investigating the circumstances

following Zara's death and the circumstances surrounding the crash.

"We will collect road-based information from the approaches to the crash sites to determine if any remedial measures or road improvements are required," the representative said.

"The initial fatal crash investigation is expected to be completed and recommendations provided by early next month."

The representative pressed the importance of motorists taking the "fatal five" seriously.

These include drink and drug-driving, fatigue, inattention, not using a seat belt and speeding.

"We all have a role to play in road safety, and motorists are reminded to obey the speed limits, abide by the road rules and drive to conditions," the representative said.

Released under RTI - DTMR

Created on	Type	Marker Number	Comment	Email	Address	Suburb	Postcode	Firstname	Lastname	Receipt	Latitude	Longitude	IP Address	Visit IP	View on map	Sentiment
			Not Relevant													
2020-08-30 15:35:58 +1000	Flooding	8	Drainage considerations' road closures - floodwaters back up onto road - area Artillery Road to the Oaks. South side of road verge water backup from heavy rains / other issues													
			Not Relevant													
2020-08-11 22:07:33 +1000	Local access & local connections	8	have the acceleration lane at least 800m long heading towards rocky so trucks can get up ace before merging into traffic as they have to climb the incline. Have the access road go from dairy inn Rd to iron pot Creek to service the properties on the eastern side of the road. Also have a overpass at dairy inn Rd artillery rd for traffic flowing east west and cars turning east towards Yeppoon.													
			Not Relevant													
2020-08-04 18:55:14 +1000	Local access & local connections	3	Link Old Byfield Road with Artillery Road to overcome the 2nd intersection in close proximity. Proper turning lanes to gain access and leave yeppoon road from mackays road and other side roads is needed. It can regularly take time to get onto yeppoon road and this brings frustration which leads to accidents.													
2020-08-01 13:48:34 +1000	Local access & local connections	1														

Released under RTI - DTMR

Sch.4 Part 4 s.6 PI

2020-08-01 15:06:18 +1000	Local access & local connections	2	Separation of laneways particularly for night driving conditions to allow longer turning lane approaches into both Artillery Rd and Dairy Inn Rd from the Rockhampton-Yeppoon road, together with dual lane access turning left (west) from Dairy Inn Rd and (east) from Artillery Rd with suitable trail length for merging traffic. For traffic turning right (east) from Dairy Inn Rd and (west) from Artillery Rd, or for those crossing directly between Artillery Rd and Dairy Inn Rd, road painted warnings
2020-08-04 19:58:03 +1000	Local access & local connections	5	Generally the connections leading onto the main road is a little dangerous, mainly because the side roads are lower in some places to the main road causing lack of sight distance. I would recommend upgrading all side road connections to ensure safety to residents.
2020-08-11 22:25:54 +1000	Local access & local connections	9	this is the road intersection that needs to be as the same as Neil's rd for access for cawarral second access.
2020-08-13 22:30:00 +1000	Local access & local connections	11	Upgrade intersection of Dairy Inn & Artillery Rd to make safer access to and crossing over Rockhampton-Yeppoon Rd.
2020-08-14 04:17:54 +1000	Local access & local connections	12	Ensure that the overtaking lane here is joined to Artillery Road so slow moving traffic have time and distance to merge with Yeppoon bound traffic without the need for moving traffic to reduce speed

Not Relevant

2020-08-22 11:23:36 +1000	Local access & local connections	17	During peak times (7.30am-8.30am and 4.45pm-6.00pm), the traffic travelling along Yeppoon Road makes it almost impossible for any vehicles on Dairy Inn Road or Artillery Road to either cross the intersection or join Yeppoon Road safely. This intersection needs an overpass / roundabout / traffic lights, especially once additional lanes are added. Making the speed limit lower will not assist in any way as the traffic volume does not change.
2020-08-22 06:58:29 +1000	Local access & local connections	16	as per comment prior

Not Relevant

2020-08-30 15:59:48 +1000	Local access & local connections	24	Water sheeting on road various spots, dangerous when hit at high speed by the inexperienced. Road camber / tracks subsided Artillery Road to Hedlow Creek
---------------------------	----------------------------------	----	---

Not Relevant

2020-09-04 10:25:25 +1000	Local access & local connections	26	A high percentage of traffic which utilises the Artillery Rd / Yeppoon Road (direction) seems to be CCC employees, general commuters, and travellers in camper/"hippy" vans/travellers towing camper trailers. (A distinct increase of traffic was observed after the left turn lane into Greenlake Rd off the Bruce Hwy was constructed.) Given the high percentage of users traversing out of Artillery Rd, perhaps a left-turn slip lane to East bound Yeppoon Road would be advantageous.
---------------------------	----------------------------------	----	---

2020-08-02 22:14:54 +1000	Network efficiency & reliability	5	Very difficult for local traffic from Cawarral, coastal traffic from Scenic highway areas, Emu Park, Tanby, Zilizie, Keppel Sands, Mt Chalmers and all areas in between as well as tourists towing vans that traverse Dairy Inn Rd to access either Yeppoon Road or Artillery Rd trying to go about daily activities.
---------------------------	----------------------------------	---	---

Not Relevant

Sch.4 Part 4 s.6 PI



Not Relevant

2020-08-26 10:26:18 +1000 Other 5 Road surfacing that reduces the noise may help. We have no problem with a four lane highway between Iron Pot and the Old Rockhampton Road turnoff at Yeppoon. It would save lives to have the intersection at Artillery Rd/Dairy Inn Rd/Yeppoon Rockhampton Rd reduced to 80k. Thankyou for your consideration.

Not Relevant

2020-08-22 23:24:17 +1000 Safety 77 Continuation of the dual carriageway from Ironpot Rd to Artillery Rd will remove a dangerous section of Yeppoon Rd improving safety and increasing traffic flow. Proper treatment of the major intersection will also help

Not Relevant

2020-09-04 10:30:52 +1000 Safety 105 Perhaps ripple-strips could be applied to the section of Dairy Inny Road approaching the Intersection would ensure that road users were completely aware of the imminent intersection ahead.

Not Relevant

Sch.4 Part 4 s.6 PI This intersection is a major concern. At peak hours traffic from Rockhampton turning right to Carwaral obstructs view of traffic coming from the right, and traffic merging in from Artillery road on the left adds to limited reaction times. The traffic on the Yeppoon/ Rocky road needs to be slowed and the traffic from Artillery /Carwaral needs to STOP. The turning lane to Carwaral needs to be clearly zoned. Painted caution stripes, Slow and Stop signs

2020-08-02 15:51:10 +1000 Safety 12

Not Relevant

This intersection is very busy particularly at peak traffic times of the day and is difficult to cross the current 2 lanes as well as deal with traffic turning into Dairy Inn Road and Artillery Road and Yeppoon Road traffic travelling over 100km and itching to overtake on overtaking lanes near the intersection. To safely cross when it becomes 4 lanes on Yeppoon Road will require either a overpass (preferred option) roundabout or traffic lights. Tourists also cross from Artillery to Dairy Inn Rd.

2020-08-02 22:03:55 +1000 Safety 13

The Artillery Road intersection should have first priority. Speed restricted to 80ks with flashing warning signs and large traffic islands to safely divide turning traffic from oncoming through traffic.

2020-08-03 07:23:54 +1000 Safety 14

Not Relevant

Sch.4 Part 4 s.6 PI

Released under RTI - DTMR

2020-08-14 06:01:32 +1000 Safety 40 the intersection of Yeppoon Road and Dairy Inn road has proved to be a dangerous intersections with deaths and serious accidents. The traffic is increasing and trying to turn right into incoming traffic is becoming increasingly difficult and dangerous. It needs upgrading urgently. Perhaps a very big round about??

Not Relevant

2020-08-05 10:25:11 +1000 Safety 18 Cars often pull in here from Dairy Inn Road in an attempt to merge with traffic. However, if there isn't an opportunity to do so, they end up sitting on the side of the road or in the left-hand lane, sometimes with multiple vehicles waiting. I have see several times where cars are travelling at speed on Yeppoon Road and have had to swerve into the centre of the road to avoid rear ending a car on the left.

Not Relevant

2020-07-31 21:09:09 +1000 Safety 5 Sch.4 Part 4 s.6 PI Sch.4 Part 4 s.6 PI its so scary trying to cross the intersection off Dairy Inn rd to turn right against all the Rockhampton bound traffic to head to Yeppoon in the mornings. The flow going to Rockhampton is sometimes uninterrupted. Speed changes wont help. It needs traffic lights or maybe a large roundabout

Sch.4 Part 4 s.6 PI

Not Relevant

2020-08-01 13:41:32 +1000 Safety 10 These roads need to be separated by overpass. Left hand turns only permitted with merging lanes. A u-turn facility each side of yeppoon road on artillery / dairy inn road would solve right turning. Having a normal intersection will see major accidents happen.

2020-08-01 15:14:00 +1000 Safety 11 Extension of the dual carriageway (four lanes) to past Cabbage Tree Creek Rd, or even to the Artillery Rd/Dairy Inn Rd intersection would improve traffic flow and safety as passing through the curvy and graded sections of the road.

Not Relevant

Released under RTI - DTMR

2020-08-12 07:47:08 +1000 Safety 26 Dangerous crossing from Dairy Inn Road to Artillery Road which is used by trucks, Caravans etc as a short cut to Bruce Highway.

Not Relevant

2020-08-12 18:28:52 +1000 Safety 29 The existing westbound overtaking lanes are not overtaking standard due to inadequate length and are incorrect entry lane marked. They are supposed to be merging lanes with one westbound through-lane for Yeppoon Rd and a merging lane for Dairy Inn Rd turning west onto Yeppoon Rd. An immediate fix is a small amount of remarking to convert the overtaking lanes back to be merging. The overtaking lane entry marking overlaps the intersection causing a conflict.

2020-08-12 18:43:11 +1000 Safety 30 Duplication of this Iron Pot section would be more desirable than the Mulara section due to the high traffic volumes of the combined counts from Yeppoon and Dairy Inn Rds.

2020-08-12 18:09:50 +1000 Safety 28 The new design intersection needs to be 'grade separated' type to achieve an uncontrolled standard. Traffic light gap controls would be an unacceptable delays to Yeppoon Rd through traffic and eastbound turning traffic Yeppoon Rd to Dairy Inn Rd. A double lane roundabout may not achieve a desirable LOS to cross traffic. High through traffic peak hour counts on the Yeppoon Rd does not allow adequate uncontrolled gaps for the high through & merg traffic counts on Dairy Inn Rd to Artillery Rd.

Not Relevant

2020-08-13 22:51:22 +1000 Safety 34 The 4 way intersection of Yep-Rton Rd, Artillery Rd and Dairy Inn Rd needs to be upgraded first to allow safe crossing, turning and merging. It's already a death trap with only 2 lanes, let alone the 4 that are being proposed. At peak hour, turning right from either Dairy Inn Rd Or Artillery Rd onto Yep-Rton Rd or crossing that intersection like I do each day is scary at best. Particularly with the cars and trucks travelling from Rton to Yeppoon speeding over the crest towards the intersection.

Not Relevant

Sch.4 Part 4 s.6 PI

Released under RTI - DTMR

2020-08-19 21:02:14 +1000 Safety 57 Intersection of rockhampton/yeppoon road with artillery/dairy inn road needs safety improvements. Rockhampton/yeppoon road needs to be four lane along it's entire length. North bound lane from Norman road turnoff to ironpot creek needs to be upgraded to match standard of southbound lane. Cycling lanes need to be constructed in both directions along entire length of rockhampton/yeppoon road. NR

Not Relevant

2020-08-15 09:16:00 +1000 Safety 45 LH turning vehicle from Artillery Rd heading east did not stop or slow down for on coming traffic. Happened twice. RYR is the main thoroughfare and should not be impeded (keep at 100k).

Not Relevant

2020-08-22 11:22:08 +1000 Safety 75 Traffic coming down hill into right turn lane face head on traffic sitting in right turn lane for Artillery Road. Cannot see down the road for clear space when a vehicle is waiting to turn into Artillery Road.

Released under RTI - DTMR

Sch.4 Part 4 s.6 PI

Not Relevant

Sch.4 Part 4 s.6 PI

Sch.4 Part 4 s.6 PI

People pull out in front of traffic and then the main flowing traffic seems to be compelled to get past at any cost (barely a day passes without unsafe driving). This junction would be much safer with a well-designed/constructed roundabout (which would keep traffic moving- unlike traffic lights) and full dual carriageway to the Iron Pot intersection.

2020-08-24 12:30:42 +1000 Safety

83

Not Relevant

Assess roads on existing 4 lane section inappropriate. No room to speed up or when slowing down to change or merge. Dairy Inn road intersection onto Yeppoon Road, in the morning, merging can be a problem. When you stop before turning left, you can get caught out by traffic flowing at a higher speed than the speed limit. Merge lane needs to be longer when turning towards Rockhampton. This would also be necessary for the other intersections towards Yeppoon.

2020-08-27 08:52:37 +1000 Safety

88

Sch.4 Part 4 s.6 PI

Not Relevant

Yeppoon Rd / Dairy Inn Rd / Artillery Rd intersection. The volume and speed of traffic travelling on Yeppoon Rd makes crossing this intersection or joining the Yeppoon Rd from D.I. Rd or Artillery Rd difficult. Also, when turning right off Yeppoon Rd onto either D.I. Rd or Artillery Rd there is reduced visibility of oncoming traffic by any vehicle coming from the opposite direction that is attempting the same action. A half way median strip and dedicated turning lanes would increase safety.

2020-08-29 20:37:26 +1000 Safety

91

Not Relevant

Released under RTI - DTMR

Not Relevant

Sch.4 Part 4 s.6 PI

2020-09-04 10:27:28 +1000 Safety

103

To increase visibility of this section of Yeppoon Road (Westerly side of Artillery/Dairy Inn Rd's), this section of roadway could be lowered, taking away the small rise. There is already a cutaway through the small hill, by lowering the cutaway further would increase visibility for approaching traffic to this black spot intersection.

Need improved turning lanes off of Rockhampton-Yeppoon Road onto Artillery Rd and onto Dairy Inn Roads. Should two facing vehicles wish to turn right off the Rocky-Ypn Rd they come close to having a head-on crash.

Not Relevant

2020-09-04 10:30:11 +1000 Safety

104

Perhaps ripple-strips could be applied to the section of Artillery Road approaching the Intersection would ensure that road users were completely aware of the imminent intersection ahead.

Released under RTI - DTMR