

## **Queensland Manual of Uniform Traffic Control Devices**

### **Part 10: Pedestrian control and protection**

**September 2025**



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## About this document

This document sets out the traffic control devices to be used for the control and protection of pedestrians. Treatments provided include:

- pedestrian crossings
- pedestrian refuges
- pedestrian malls, and
- facilities for pedestrians with disabilities.

Pedestrian safety largely depends upon public understanding of accepted methods for traffic control. This principle is particularly important regarding the control of pedestrians and vehicles near schools. Pedestrian safety in these areas can be a highly sensitive subject.

Safe and effective traffic control is best obtained through consistently applying realistic policies, practices and standards which have been developed through traffic engineering studies. It is important that traffic control devices are used in accordance with the conditions prescribed in this Part of the Manual. Uniform treatment for comparable situations promotes uniform behaviour by both motorists and pedestrians.

Where possible, symbolic legends should be used in association with pedestrian facilities. Symbolic legends are used in place of worded legends to increase sign legibility.

## How to use this document

This document is designed to be read and applied together with AS 1742.10-2024 *Manual of Uniform Traffic Control Devices Part 10* (AS 1742.10-2024). You must have access to the Australian Standard to understand what applies in Queensland.

This document:

- sets out how AS 1742.10-2024 applies in Queensland
- has precedence over AS 1742.10-2024 when applied in Queensland, and
- has the same section and clause numbering and headings as AS 1742.10-2024.

The following table summarises the relationship between AS 1742.10-2024 and this document:

Applicability	Meaning
Accepted	The Australian Standard section or clause is accepted.
Accepted, with amendments	Part or all of the section or clause has been accepted with additions, deletions or differences.
New	There is no equivalent section or clause in the Australian Standard.
Not accepted	The Australian Standard section or clause is not accepted.

Where updated signage reference numbers are presented in this document, the historical signage reference is given in parenthesis (round brackets). For example: R3-3-Q01 (TC9472).

## Definitions

The following general amended definitions apply when reading AS 1742.10-2009.

Reference to...	Means
AS 1742.10-2024	AS 1742.10-2024, as amended by this document For example, a reference to AS 1742.10-2024 means you must refer to the Australian Standard Part 10, and Part 10 of the Queensland <i>Manual of Uniform Traffic Control Devices</i> (Queensland MUTCD). Throughout AS 1742.10-2024, references are made to other parts of the Australian Standards (for example, when reading Part 10 you may be referred to Part 1 for further information). In this case, you must refer to the equivalent Part within the Queensland MUTCD first. Check the applicability of the equivalent Part in the Queensland MUTCD before referring to the referenced Australian Standard Part.
QGTM	Transport and Main Roads <a href="#">Queensland Guide to Traffic Management</a>
Queensland MUTCD	Transport and Main Roads' <a href="#">Queensland Manual of Uniform Traffic Control Devices</a> .
RPDM	Transport and Main Roads' <a href="#">Road Planning and Design Manual</a> .
TRUM	Transport and Main Roads' <a href="#">Traffic and Road Use Management manual</a>

## Relationship table

Section	Clause	Description	Applicability
<b>1</b>	<b>Scope</b>		Accepted
<b>2</b>	<b>Normative References</b>		Accepted, with amendments
<b>3</b>	<b>Terms and Definitions</b>		
	3.1	May	Accepted, with amendments
	3.2	Shall	Accepted, with amendments
	3.3	Should	Accepted, with amendments
<b>4</b>	<b>Introduction and general requirements</b>		
	<b>4.1</b>	<b>Types of pedestrian facilities</b>	Accepted
	<b>4.2</b>	<b>Purpose and suitability of facilities</b>	Accepted, with amendments
	<b>4.3</b>	<b>Provision for access and mobility</b>	Accepted, with amendments
	<b>4.4</b>	<b>Grade separation</b>	Accepted
	<b>4.5</b>	<b>Left turn slip lanes</b>	Accepted, with amendments
	<b>4.6</b>	<b>Raised pavements at crossings</b>	Accepted
	<b>4.7</b>	<b>Kerb ramps and approach paths</b>	Accepted, with amendments
	<b>4.8</b>	<b>Signs</b>	Accepted, with amendments
<b>5</b>	<b>Pedestrian crossing (zebra crossing)</b>		
	5.1	Introduction	Accepted
	5.2	Description	Accepted, with amendments
	5.3	Requirements for installation	Accepted, with amendments
	5.4	Signs	Accepted
	5.5	Pavement markings	Accepted, with amendments
	5.6	Typical layouts	Accepted, with amendments
<b>6</b>	<b>Priority path crossing</b>		
	6.1	Introduction	Accepted, with amendments
	6.2	Description	Accepted
	6.3	Requirements for installation	Accepted, with amendments
	6.4	Signs	Accepted
	6.5	Pavement markings	Accepted, with amendments
	6.6	Typical layouts	Accepted, with amendments
<b>7</b>	<b>Children's crossing</b>		

Section	Clause	Description	Applicability
	7.1	Purpose and safe operation	Not accepted
	7.2	Description	Not accepted
	7.3	Requirements for installation	Accepted
	7.4	Signs	Accepted, with amendments
	7.5	Typical layouts	Accepted, with amendments
<b>8</b>	<b>Pedestrian actuated traffic signals (mid-block)</b>		
	8.1	Description	Accepted, with amendments
	8.2	Guidelines for installation	Accepted, with amendments
	8.3	Limitations on installation	Accepted, with amendments
	8.4	Signs and pavement markings	Accepted
	8.5	Provision for pedestrians at signal controlled intersections and railway crossings	Accepted
	8.6	Typical layouts	Accepted
<b>9</b>	<b>Physical pedestrian facilities</b>		
	9.1	General	Accepted
	9.2	Pedestrian refuge islands, traffic islands and medians	Accepted, with amendments
	9.2.1	<i>Refuge Island - General</i>	Accepted
	9.2.2	<i>Installation</i>	Accepted, with amendments
	9.2.3	<i>Guidelines for installation of refuge islands</i>	Accepted, with amendments
	9.2.4	<i>Central islands at marked crossings</i>	Accepted
	9.2.5	<i>Barrier lines</i>	New
	9.3	Kerb extensions	Accepted
	9.4	Loading islands and safety zones	Accepted, with amendments
	9.4.1	Description	Accepted
	9.4.2	Guidelines for installation	Accepted, with amendments
	9.4.3	Signs	Accepted, with amendments
	9.4.4	Notes for Figure 9.1	Accepted, with amendments
<b>10</b>	<b>Pedestrian malls</b>		Accepted, with amendments

Section	Clause	Description	Applicability
<b>11</b>	<b>Warning signs</b>		
	11.1	General	Accepted
	11.2	Warning signs for pedestrian crossings	Accepted
	11.3	Warning signs for pedestrians not at crossings	Accepted
<b>12</b>	<b>Pedestrian direction signs (G5-7, G5- 8)</b>		Accepted
<b>13</b>	<b>Lighting</b>		Accepted, with amendments
<b>14</b>	<b>Other signs</b>		
	14.1	General	New
	14.2	Signs	New
<b>Appendices</b>			
<b>A</b>	<b>Model instructions for adult supervisors at crossings (informative)</b>		Not accepted

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## 2 Normative references

### Difference

The following are additional reference documents:

- AS 1428.4.1 *Means to assist the orientation of people with vision impairment – Tactile ground surface indicators.*

## 3 Terms and Definitions

### Addition

#### 3.1 **May**

This term is further specified in the Queensland MUTCD Part 0: *Glossary of terms.*

### Addition

#### 3.2 **Shall**

This term is further specified in the Queensland MUTCD Part 0: *Glossary of terms.*

#### 3.3 **Should**

### Addition

This term is further specified in the Queensland MUTCD Part 0: *Glossary of terms.*

## 4 Introduction and general requirements

#### 4.2 **Purpose and suitability of facilities**

### Addition

The *Queensland Guide to Traffic Management* Part 6 provides crossing selection guidance for Queensland.

### Not accepted

The second sentence in the first paragraph is not applicable in Queensland.

### **4.3 Provision for access and mobility**

#### Difference

The following replaces that in the standard.

The following devices are designed to enable access for pedestrians that have physical impairments or the need to use mobility aids:

- a) Kerb ramps: see Clause 4.7.
- b) Pavement level ("cut-through") access across medians and islands.
- c) Tactile ground surface indicators in accordance with AS/NZS 1428.4.1.
- d) Audio-tactile push-button facilities at traffic signals in accordance with AS 2353.
- e) Sign controlled facilities and signal-controlled facilities (see Table 1)

### **4.5 Left turn slip lanes**

#### Addition

The *Queensland Guide to Traffic Management* Part 6 provides guidance for left turn treatment selection and crossing treatment type for slip lanes for Queensland.

#### Not accepted

Delete the 'Note' paragraph.

### **4.7 Kerb ramps and approach paths**

#### Difference

The following replaces that in the standard.

Where provided, kerb ramps shall be designed in accordance with AS 1428.1, except that the width of the ramp should be no less than the width of any associated marked crossing or area defined by Pedestrian Guide Lines (PL) as set out in AS 1742.2. The width of any approach path should be no less than the width of the ramp.

## 4.8 Signs

### Addition

Add the following content to Table 2.

Sign	Sign number	Size, mm	Clause
REGULATORY FLAG AND FITTINGS - "CHILDREN CROSSING"	R3-3-Q01 (TC9472)	600 x 600	7.4
INSTRUCTION SIGN "WHEELED RECREATIONAL DEVICES AND WHEELED TOYS PROHIBITED"	R6-100-Q01 (TC1037)	A 450 x 450 B 600 x 600 C 750 x 750	Figure 10.1
INSTRUCTION SIGN "CYCLING, WHEELED RECREATIONAL DEVICES AND WHEELED TOYS PROHIBITED"	R6-100-Q02 (TC1050)	600 x 450	Figure 10.1
REGULATORY SIGN "E-SCOOTERS PROHIBITED"	R6-100-Q03_1	AA 300 x 300 A 450 x 450 B 600 x 600	Figure 10.1
REGULATORY SIGN "PERSONAL MOBILITY DEVICES PROHIBITED"	R6-100-Q03_2 (R6-Q04_2)	A 825 x 450 B 1100 x 600	Figure 10.1

## 5 Pedestrian (zebra) crossing

### 5.2 Description

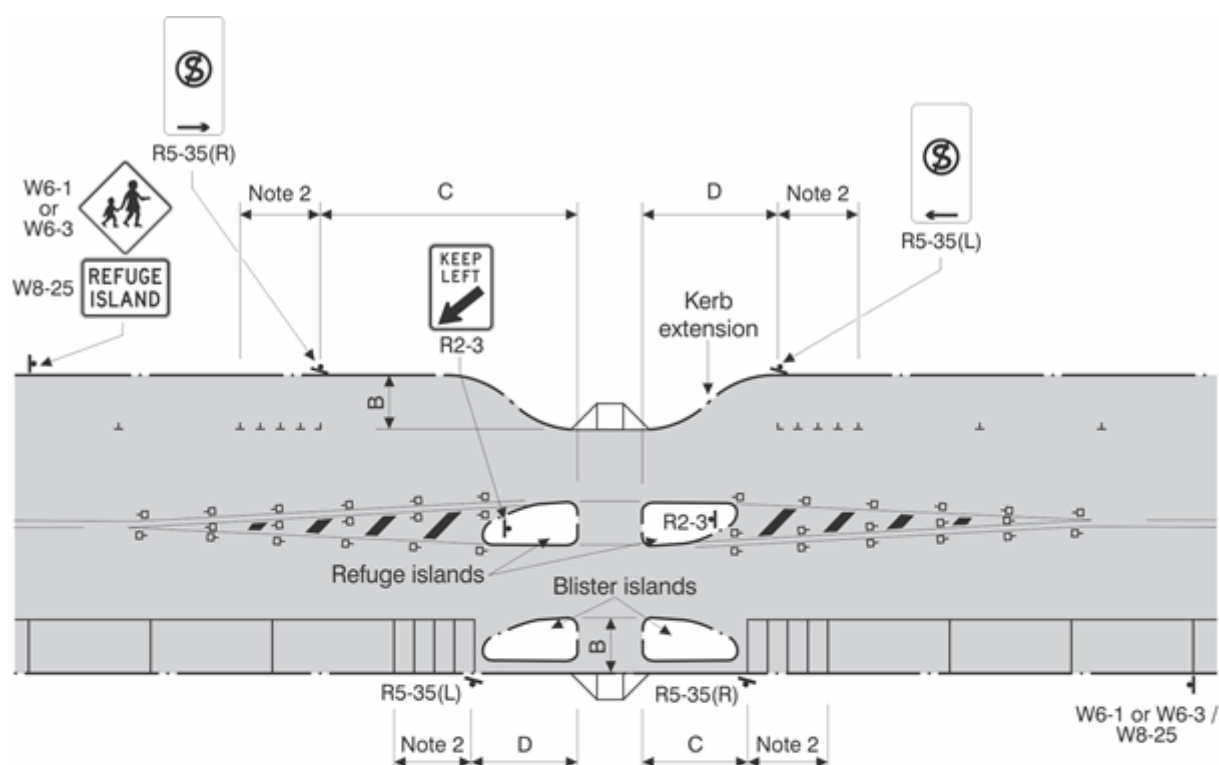
#### Difference

Information contained in the sixth paragraph in this clause about the use of twin alternating flashing signals (lights) shall be replaced with the following:

The R3-1 sign may be supplemented with twin, diagonally opposed, alternatively flashing yellow signals (lights). Where flashing signals (lights) are installed they shall only be used where activated passively by pedestrian detection technology. Push button activation of such lights is not accepted. Where used, the display and flash cycle should be consistent throughout a region.

#### Addition

Foreshortening of the no stopping zone may be implemented as shown in Figure 5.2(a).

**Figure 5.2(a) – Kerb extension blister****Notes:**

1. Parking restrictions may also be reduced at rear-in 'angled parking' situations. Kerb extensions must project at least 200 mm further from the kerb than the length of the angled parking bay.
2. When parking restrictions have been reduced, the first parking spaces should be allocated to motorcycles or bicycles so as to ensure extra visibility to and from the facility.

Placement of  
"No Stopping Any Time" (R5-35)  
signs at parallel parking situations

B (m)	C (m)	D (m)
0	20	10
1.5	15	10
2.0	10	10
≥2.5	5	5

**5.3 Requirements for installation****Difference**

The following replaces that in the standard:

Requirements and options for the installation of pedestrian (zebra) crossings are as follows:

- a) Mid-block crossing (not raised and without physical speed control features [see Section (e) for raised crossings on multi-lane roads]). The following shall apply:
  - i. No more than one lane of moving traffic in any one direction shall be encountered by a pedestrian using a crossing.

- ii. There shall be adequate sight distance between approaching vehicles and pedestrians about to use the crossing for the former to be able to stop in time to give way to the latter. This may be achieved primarily by means of parking restrictions near the crossing (see Figure 5.1). Kerb extensions (Clause 5.2) may also be required to achieve this sight distance where kerbside parking is frequent (see Figure 5.2(a)). Limits to the height of any vegetation and, control of the speed of motor vehicles may also be provided.
- iii. The 85th percentile operating speed at the crossing once constructed should not be expected to exceed 50 km/h. This criterion shall be applied to enhance crossing safety, not remove existing crossings. Measures to reduce 85th percentile speed at crossings shall be considered to align with safe system principles so far as is reasonably practical (refer to *Road Planning and Design Manual* Part 4B Table 5.1.1(b) for ramp grade speed reductions).
- iv. Approach sight distance to the leading edge of the crossing pavement marking shall be provided appropriate to the 85th percentile speed of the site.
- v. If at a location with a posted speed limit greater than 50 km/h and speed limiting devices are not incorporated into the design, an RPEQ certified risk assessment (for example a Safe System Assessment) shall be undertaken to support the use of the treatment at the site, including an assessment of the risk of a 'do nothing' option.

b) Crossings on slip lanes

The conditions specified for crossings in Item (a) and (e) shall also apply to pedestrian crossing installations on slip lanes.

c) Other locations

Pedestrian crossings may be provided at other locations including roundabouts and side roads by following the principles given for mid-block crossings and providing sufficient sight distance for the likely speed of vehicles. In the immediate vicinity of a roundabout which physically slows all vehicles, a pedestrian crossing may be provided on a road with an 85th percentile speed not expected to exceed 50 km/h at the crossing once a speed limiting device is constructed (such as a raised platform). See Figure 5.7 for regulatory and warning signs for raised pedestrian crossings at roundabouts.

d) Advanced warning

Where the presence of a pedestrian crossing, the pedestrians who use it, or the presence of a raised platform may be unexpected or difficult to anticipate, warning of the crossing shall be provided, see AS 1742.10 Clause 11.2(b) and Clause 11.2(e).

e) Pedestrian (zebra) crossings with physical features to control motor vehicle speed at the crossing (for example zebra on raised platform [wombat] or zebra adjacent to speed cushions). The following shall apply:

- i. If more than one lane of moving traffic in any one direction is encountered by a pedestrian using a crossing, an RPEQ certified risk assessment (for example a Safe System Assessment) shall be undertaken to support the use of the treatment at the site, including an assessment of the risk of a 'do nothing' option. Figures 5.5 and 5.6 show typical layouts incorporating setback lines and visibility islands, which can be used alone or in combination to treat the multiple vehicle visibility blockage (of pedestrians crossings) risk at multi-lane pedestrian crossings.
- ii. As per s5.3(a)(ii).
- iii. As per s5.3 (a)(iii).
- iv. Approach sight distance to the top of the raised crossing leading edge ramp shall be provided appropriate to the 85th percentile speed of the site.

For guidance on selecting and planning crossings, refer to *Queensland Guide to Traffic Management* Part 6, Transport and Main Roads' Technical Guidelines for *Raised Priority Crossings on Pedestrian and Cycle Paths*, and *Selection and Design of Cycle Tracks*.

Refer to *Raised Priority Crossings for Pedestrian and Cycle Paths* for examples of application of Queensland MUTCD Part 10 85th percentile speed requirements for raised priority crossings.

## **5.5 Pavement markings**

### Difference

Point (a), replace the second paragraph with the following.

The road pavement between the bars shall be of a uniform colour that contrasts with the white bars. This uniform colour shall extend for at least 1 m at each end of the bars, so that the crossing is clearly identifiable and visually isolated. The only permitted encroachment into this area of uniform colour is the markings described in Item (b). See also Item (d).

Red and yellow pavement treatments do not contrast sufficiently with white bars for people with vision impairments.

For the profile of raised pedestrian (zebra) crossings at any location refer to *Road Planning and Design Manual 2<sup>nd</sup> Edition* Part 4B.

## **5.6 Typical layouts**

### Addition

Widths shown in typical layouts are desirable. They should be adapted as needed to allow for crossing installation at constrained sites.

### Addition

- e) A multi-lane raised pedestrian crossing consideration with a splitter island, see Figure 5.5.
- f) A multi-lane raised pedestrian crossing consideration with setback give way line, see Figure 5.6.
- g) Regulatory and warning signs at a roundabout with raised pedestrian crossings, see Figure 5.7.

Figure 5.1 NOTE 5

### Difference

The following replaces that in the standard:

The R3-1 sign may be supplemented with twin, diagonally opposed, alternatively flashing yellow signals (lights). Where flashing signals (lights) are installed they shall only be used where activated passively by pedestrian detection technology. Push button activation of such lights is not accepted.

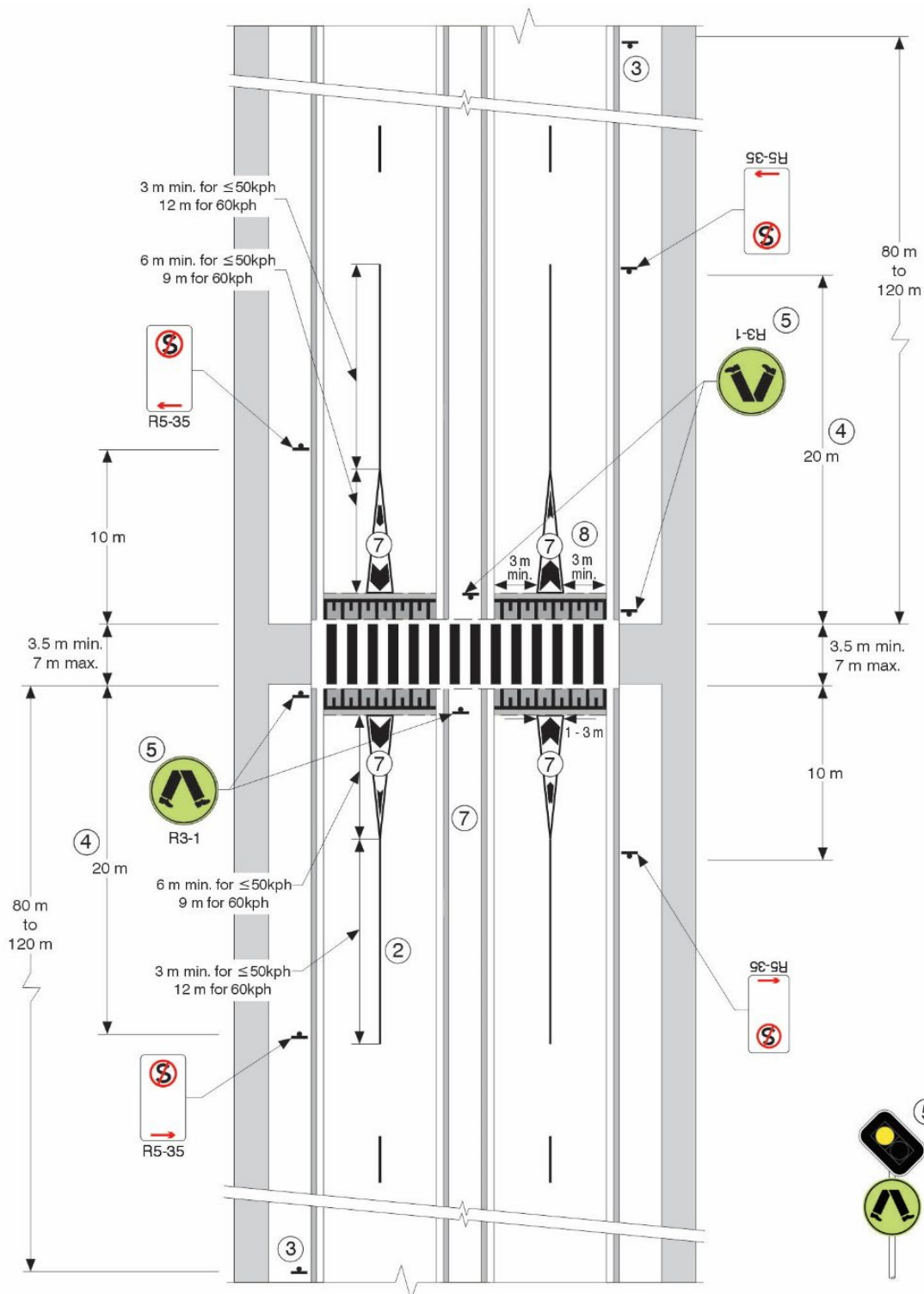
Figure 5.2 NOTE 5

### Difference

The following replaces that in the standard:

The R3-1 sign may be supplemented with twin, diagonally opposed, alternatively flashing yellow signals (lights). Where flashing signals (lights) are installed they shall only be used where activated passively by pedestrian detection technology. Push button activation of such lights is not accepted.

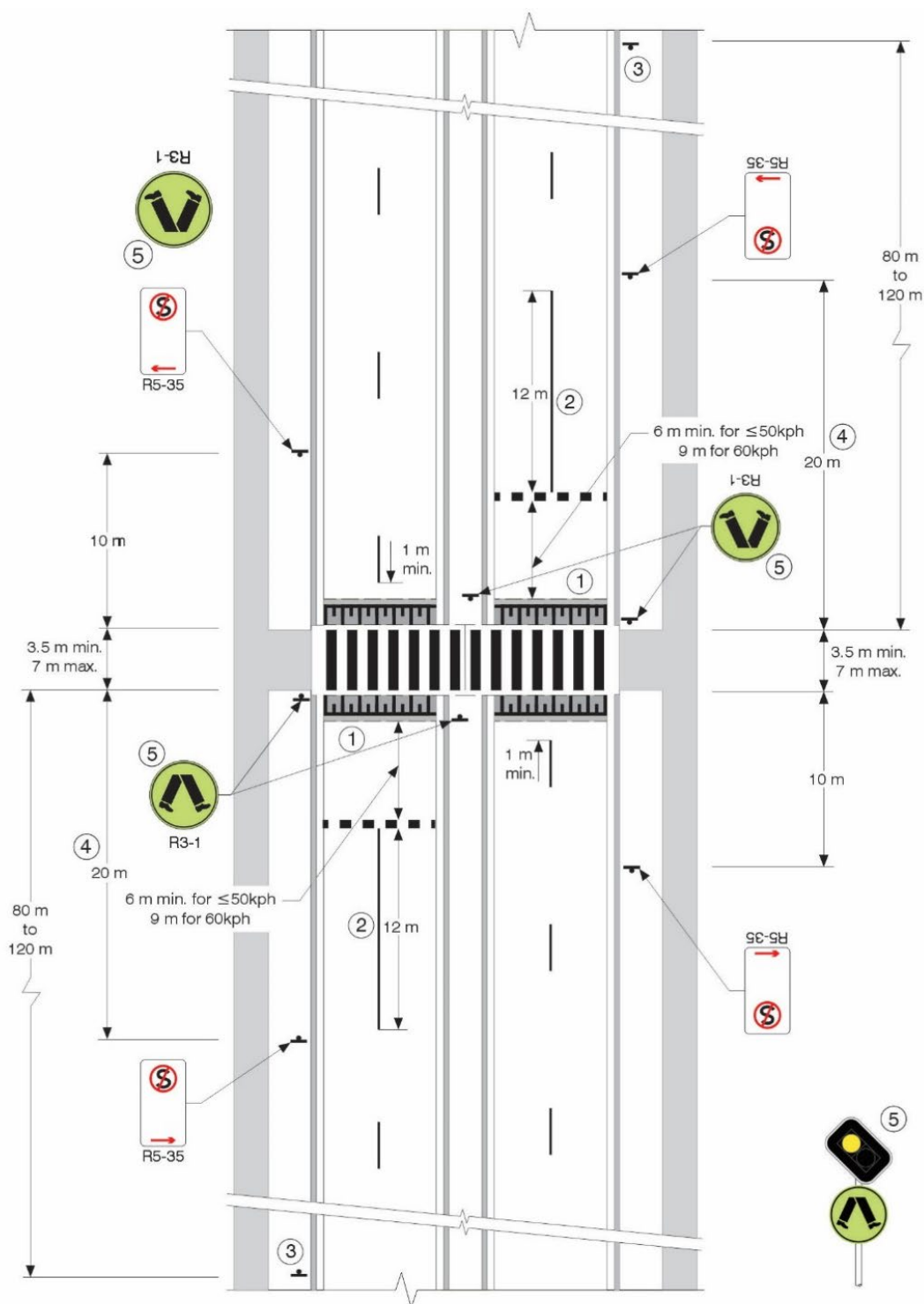




1. For details of markings at the crossing, see Clause 5.5. Raised platforms on median divided roads may

3. The W6-2-1 sign shall be used in advance of the crossing where sight distance to the R3-1 signs is substandard, see Clause 11.2(b). If required, the W8-31 sign is located here and is mounted below the W6-2-1 sign, see Clause 11.2(e).
4. Variations to no stopping distances may be required, see Clause 5.2.
5. The R3-1 sign may be supplemented by flashing yellow signals when passively activated by a pedestrian presence detector.
6. For additional speed reduction on approach to the crossing, speed reduction devices such as speed cushions or speed bumps may be installed, see QMUTCD Part 13 for further information.
7. Design guidance for splitter islands, see Austroads Guide to Road Design (AGRD) Chapter 6 Part 4A.
8. For lane narrowing design guidelines, see departmental *Road Planning and Design Manual 2<sup>nd</sup> Edition* (RPDM), Part 3, Section 4.2.4.
9. For delineation of splitter islands or medians, refer to the Queensland MUTCD Part 2.

**Figure 5.6 – Raised pedestrian crossing on a multi-lane road with setback give way line**



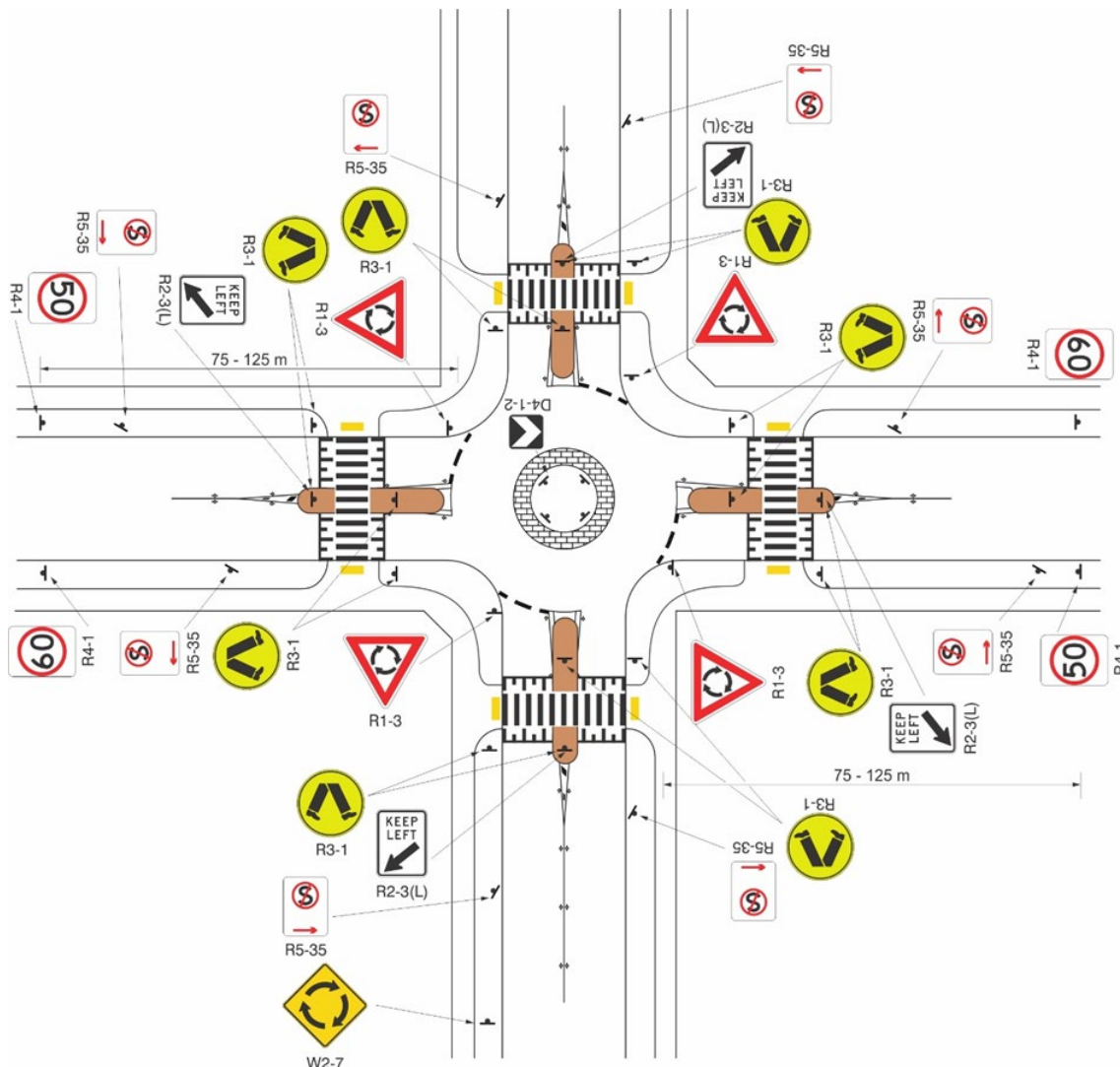
**Notes:**

1. For details of markings at the crossing, see Clause 5.5. Raised platforms on median divided roads may omit road hump markings on the departure side of the platform.
2. A barrier line shall be provided on each approach to the crossing if the road has a dividing line. It should end a minimum of 1 m before the ramp markings.

3. The W6-2-1 sign is used in advance of the crossing where sight distance to the R3-1 signs is substandard, see Clause 11.2(b). If required, the W8-31 sign is located here and is mounted below the W6-2-1 sign, Clause 11.2(e).
4. Variations to no stopping distances may be required, see Clause 5.2.
5. The R3-1 sign may be supplemented by flashing yellow signals when passively activated by a pedestrian presence detector.
6. For additional speed reduction on approach to the crossing, speed reduction devices such as speed cushions or speed bumps may be installed, see Queensland MUTCD Part 13 for further information.

### Addition

**Figure 5.7 – Regulatory and warning signs at roundabout with raised pedestrian crossings**



i Figure 5.7:

**NOTE 1** Pedestrian Crossing (R3-1) sign shall be installed on both sides of the lane and facing each approach, at or in the immediate vicinity of the pedestrian crossing, refer to Queensland MUTCD Part 10.

- NOTE 2 The Roundabout (R1-3) sign shall be installed as near as possible to the give way line on all approaches, see Queensland MUTCD Part 2, Clause 2.6.3.
- NOTE 3 For use of the Roundabout Ahead (W2-7) sign, see Queensland MUTCD Part 2, Clause 2.10.3(c).
- NOTE 4 The Pedestrian Crossing Ahead (W6-2) sign shall be used in advance of pedestrian crossings (zebra) where visibility of the Pedestrian Crossing sign (R3-1) is obstructed due to road curvature or other local conditions, refer to Queensland MUTCD Part 10.
- NOTE 5 Speed Restriction (R4-1) signs should be installed 30–50 m from intersections in urban areas, refer to Queensland MUTCD Part 4.
- NOTE 6 The Road Hump (W5-10) sign shall not be used at raised pedestrian crossings (zebra), refer to Queensland MUTCD Part 13.
- NOTE 7 For the required setback of the raised pedestrian crossing (zebra), refer to RPDM Volume 3 Part 4B.
- NOTE 8 For markings on a raised pedestrian crossing (zebra), see Figure 5.15.
- NOTE 9 The KEEP LEFT (R2-3(L)) sign may not be necessary where traffic is clearly required to pass to the left of the island or where a Roundabout (R1-3) sign is located in the island, see Queensland MUTCD Part 2, Clause 2.8.2(b).
- NOTE 10 The No Stopping (R5-35) sign may be provided 20 m prior and 10 m past all raised pedestrian crossings (wombat) where yellow no stopping lines have proven to be ineffective.
- NOTE 11 The Unidirectional Hazard Marker (D4-1-2) sign should be used at local street roundabouts for each approach, but will necessarily be omitted where the central island is required to be mounted by larger turning vehicles. In these cases, other means of retroreflective delineation (e.g., RRPMS) together with an appropriate level of street lighting should be provided.
- NOTE 12 Where provided, landscaping in the central island should be low enough to not restrict visibility across the island.

## 6 Priority path crossing

### 6.1 Introduction

#### Difference

The following replaces the last two sentences in Section 6.1 in the standard.

Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for guidance around use of STOP signs at crossings.

Requirements and recommendations for the lighting of crossings are given in Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths*.

### 6.3 Requirements for installation

#### Difference

Replace (b)(iii) as follows

- iii The speed limit on approach to the crossing shall be 50 km/h or lower and the 85th percentile speed at the crossing (when constructed) should not exceed 50 km/h.

#### Addition

Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for examples of application of Queensland MUTCD Part 10 85th percentile speed requirements for raised priority crossings.

#### Deletion

Delete the remainder of Section (b) after (iii).

### 6.5 Pavement markings

#### Difference

The following replaces (a) in the standard.

The edges of the path at the crossing may be marked across the full width of the road with pedestrian guide lines (PL) as set out in AS 1742.2.

A give way line (GW), refer to AS 1742.2, shall be marked across the approach traffic lane. The Give Way line should be offset from other linemarking and optimise visibility to path users for a stopped driver. Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for Give Way line placement.

### Difference

The following replaces the first line of (d) in the standard.

A coloured surface treatment should be applied to a priority path crossing to enhance its visibility by providing a contrasting colour between the crossing and the surrounding road pavement.

## **6.6 Typical Layouts**

### **6.6.2 Notes for Figure 6.1**

#### Difference

For Note 2 replace '3 m' with 'desirable  $\geq 1$  m'. Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for additional layouts.

Notes for Figure 6.2

#### Difference

For Note 2 replace '3 m' with 'desirable  $\geq 1$  m'. Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for additional layouts.

Notes for Figure 6.3

#### Difference

For Note 2 replace '3 m' with 'desirable  $\geq 1$  m'. Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for additional layouts.

## **7 Children's crossing**

### **7.1 Purpose and safe operation**

#### Not accepted

The following replaces that in the Standard:

The children's crossing is a part-time facility catering primarily for school children. It is only in operation when the CHILDREN'S CROSSING (R3 3) flags are displayed.

CHILDREN'S CROSSING (R3 3) flags shall only be displayed during school zone times of operation.

NOTE:

1. Pedestrians on (or entering) a crossing place a legal obligation on vehicles to stop and not enter the crossing. Vehicles may not proceed until all pedestrians are clear of the crossing. This is a higher obligation on traffic than at a pedestrian crossing (refer to Clause 6).
2. For information regarding traffic management and road safety at schools in Queensland, refer to *Queensland Road Safety Technical User Volumes (QRSTUV) Guide to Schools*.

## 7.2 Description

### Not accepted

The following replaces that in the Standard:

A children's crossing as shown in Figure 7.2(a) shall comprise the following items:

- i. STOP lines minimum 450 mm wide.
- ii. Two posts painted in red and white alternate bands on each side of the road at the crossing and one post in red and white alternate bands in advance of the crossing at the STOP line.
- iii. The CHILDREN'S CROSSING flags (R3 3) displayed to indicate that the crossing is in operation.
- iv. Provision for mounting the CHILDREN'S CROSSING flags on the advance posts specified in Item ii.
- v. NO STOPPING signs (R5 36) for control of parking near the crossing during the times the crossing is in operation.
- vi. The hand STOP banner (R6 7) which is used by a crossing supervisor when in attendance.
- vii. The advance warning sign assembly, Children's Crossing Ahead (W6 5-1) used where sight distance to the crossing is substandard.
- viii. Dashed crosswalk lines.
- ix. Yellow pedestrian holding lines each side of the crossing.

At some schools, a pedestrian crossing may be required for use by pedestrians outside of school zone hours. This facility would normally be installed in accordance with Clause 6. Figure 7.2(b) provides details for signing and marking a combined children's crossing and pedestrian crossings (zebra) at schools.



## 7.4 Signs

### Difference

The following sentence replaces the first sentence after the STOP sign image in the standard.

No other sign shall be displayed in the immediate vicinity of a children's crossing unless it is combined with a pedestrian crossing.

### Addition

#### *REGULATORY FLAG AND FITTINGS – "CHILDREN CROSSING" (R3-3-Q01)*



R3-3-Q01 (TC9472)

For flag details refer to R3-3. R3-3-Q01 specifies materials, colours and fittings. Flag material shall be opaque and double thickness. Legend shall be provided on both sides of the flag.

## 7.5 Typical layouts

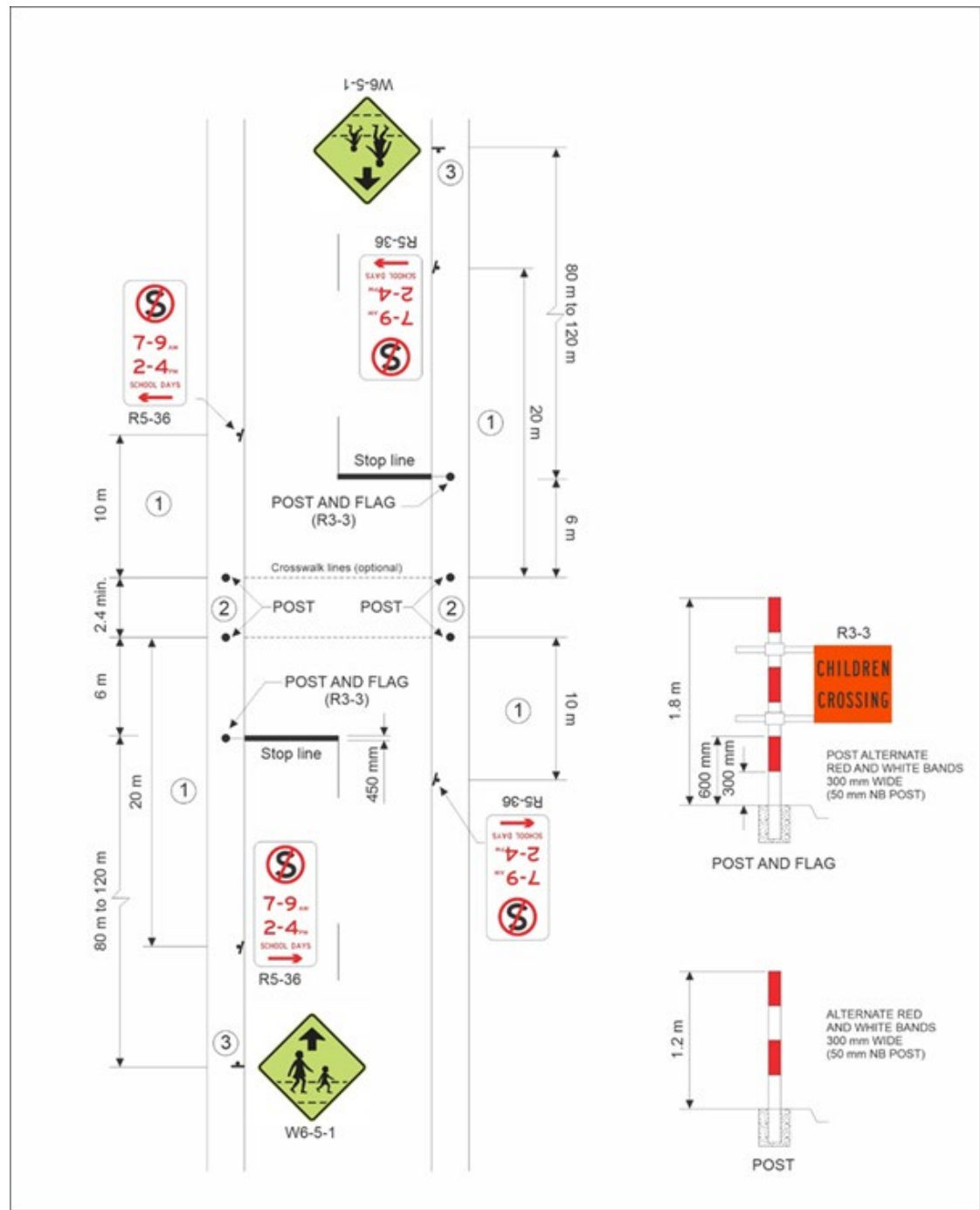
### Difference

Replace Figure 7.1 with 7.1(a)

### Addition

Include Figure 7.1(b)

--



Notes:

1. Variations to no stopping distances may be required where visibility to the crossing is substandard.
2. A yellow line (approximately 100 mm wide) shall be painted on the footpath – at least 0.5 m behind the face of the kerb – to indicate the position where pedestrians should wait until directed to cross the roadway. Where used, this line extends the width of the sealed apron connecting the footpath and kerb or a distance of 2.4 metres, that is, between the crossing posts (without flags).
3. The W6 5-1 assembly is required if the sight distance to the crossing is substandard.



### Addition

Figure 7.2

Note 6: A pedestrian (zebra) crossing can be used at a children's crossing on a side street near an intersection. See Figure 7.1(b) and associated notes.

## **8 Pedestrian operated traffic signals (mid-block)**

### **8.1 Description**

#### Difference

The fourth paragraph in Section 8.1 is not approved for use in Queensland. See *Queensland Road Safety Technical User Volumes (QRSTUV) Guide to Schools* for guidance on use of supervision at signalised pedestrian crossings.

#### Not accepted

The last paragraph in Section 8.1 is not approved for use in Queensland

### **8.2 Guidelines for installation**

#### Not accepted

This section is not accepted for use in Queensland. Refer to *Queensland Guide to Traffic Management (QGTM)* Part 6 for guidelines on installation.

### **8.3 Limitations on installation**

#### Not accepted

The section after (c) related to pelican crossing phasing is not approved for use in Queensland.

## **9 Physical pedestrian facilities**

### **9.1 General**

#### Addition

Add the following sentence:

When used in isolation (not in combination with a marked pedestrian crossing or signals) on roads with moderate to high volumes, a refuge crossing may not allow a person with either cognitive impairments or vision loss (of any level) to cross the road and may be discriminatory.

## 9.2 Pedestrian refuge islands, traffic islands and medians

### 9.2.2 Installation

#### Difference

The first two paragraphs in Section 9.2.2 are not accepted for use in Queensland.

#### Addition

If more than one lane of moving traffic in any one direction is encountered by a pedestrian using a crossing or the posted speed limit is >50km/h, an RPEQ certified risk assessment (for example a Safe System Assessment) shall be undertaken to support the use of this treatment at the site, including an assessment of the risk of a 'do nothing' option.

### 9.2.3 Guidelines for installation of refuge islands

#### Difference

Replace all text in this section with the following:

Refer to the *Queensland Guide to Traffic Management* (QGTM) Part 6 for guidelines on installation of refuge islands.

### 9.2.5 Barrier lines

#### New

With reference to Figure 9.1 a double barrier line, augmented with RRPMS, should form the outline on the approach sides of the painted splays (pavement marked median chevrons). A barrier line should also precede each splay for the distance given in Table 9.2.5.

A = splay (pavement marked chevron area) including the full length of lead-in barrier line

C = lead-in barrier line only

**Table 9.2.5 – Required distances**

$V_{85}$ (km/h)	A (m)	C (m)
<75	80–120	30
75–90	120–180	60
>90	180–250	100

## **9.4 Loading islands and safety zones**

### **9.4.2 Guidelines for installation**

#### Difference

The fifth paragraph in the Standard is replaced with the following:

The safety zone is a particular form of loading island at which special traffic regulations apply. Loading islands may be designated as safety zones by the installation of the SAFETY ZONE sign (R3 2) (see Clause 9.4.3(b)) on the island facing approaching traffic.

### **9.4.3 Signs**

#### Difference

Replace point (b) with the following:

Safety Zone (R3 2)

The SAFETY ZONE sign shall be used to designate loading islands as safety zones where large numbers of passengers board or leave public transport vehicles other than the footway.

### **9.4.4 Notes for Figure 9.1**

#### Difference

Note 4 says extend barrier line minimum of 30 m. Replace '30 m' with Column 'C' m in Table 9.2.5.

Figure 9.1. Replace '80-120 m' with Column 'A' in Table 9.2.5.

## **10 Pedestrian malls**

### *d) Other signs*

#### Deletion

Delete Figure 10.1 (this figure is an example only and this sign is not an approved sign for use in Queensland).

Addition

Add the following to item (d)

- i. *Wheeled Recreational Devices and Wheeled Toys Prohibited (R6-100-Q01)*



R6-100-Q01 (TC1037)

The Wheeled Recreational Devices and Wheeled Toys Prohibited (R6-100-Q01) sign may be to prohibit the use of wheeled recreational devices and wheeled toys only. Supplementary plate TC2360 may be used in conjunction with this sign to define an area to which the prohibition is to apply.

- ii. *Cycling, Wheeled Recreational Devices and Wheeled Toys Prohibited (R6-100-Q02)*



R6-100-Q02 (TC1050)

The Cycling, Wheeled Recreational Devices and Wheeled Toys Prohibited (R6-100-Q02) sign may be used on pedestrian facilities to prohibit the use of bicycles, wheeled recreational devices and wheeled toys on footpaths.

Sign elements may be deleted in circumstances where not all devices are prohibited. Supplementary plate TC2360 may be used in conjunction with this sign to define an area to which the prohibition is to apply.

Refer to the *Queensland Guide to Traffic Management* Part 7 for further guidance on prohibiting cycling, wheeled recreational devices and/or personal mobility devices.

iii. *E-Scooters Prohibited (R6-100-Q03\_1), Personal Mobility Devices Prohibited (R6-100-Q03\_2)*



R6-100-Q03\_1



R6-100-Q03\_2 (R6-Q04\_2)

The E-Scooters Prohibited (R6-100-Q03\_1) and PERSONAL MOBILITY DEVICES PROHIBITED (R6-100-Q03\_2) signs may be used to prohibit the use of personal mobility devices. Supplementary plate TC2360 may be used in conjunction with this sign to define an area to which the prohibition is to apply.

## 13 Lighting

### Addition

Refer to Technical Guideline *Raised Priority Crossings for Pedestrian and Cycle Paths* for lighting guidance on raised priority crossings at side roads.

## 14 Other signs

### 14.1 General

#### New

Signs described in this section provide additional guidance to pedestrians and cyclists and are listed in Table 14.1.

**Table 14.1 – Other signs**

Sign	Sign number	Size, mm	Clause
UNDERPASS SUBJECT TO FLOODING	G9-100-Q01_1 (TC1019)	750 x 450	14.2 (a)
PATH SUBJECT TO FLOODING INDICATORS SHOW DEPTH	G9-100-Q01_2 (TC1831)	750 x 450	14.2 (a)
PEDESTRIANS MUST CROSS AT SIGNALS	G9-100-Q02_1 (TC9867_1)	A 300 x 225 B 800 x 600	14.2 (b)
PEDESTRIANS MUST CROSS AT FACILITY	G9-100-Q02_2 (TC9867_2)	A 300 x 225 B 800 x 600	14.2 (b)



Sign	Sign number	Size, mm	Clause
Pedestrian UNDERPASS	G9-100-Q03 (TC1731)	700 x 600	14.2 (c)
Pedestrian Direction	G9-100-Q04 (TC1838)	A 600 x 450 B 800 x 600 C 1000 x 750	14.2 (d)
PEDESTRIANS MUST USE FOOTPATH	G9-100-Q05_1 (TC1839)	A 450 x 300 B 600 x 400 C 750 x 500	14.2 (d)
PEDESTRIANS MUST USE OVERPASS	G9-100-Q05_2 (TC1839)	A 450 x 300 B 600 x 400 C 750 x 500	14.2 (d)
SCHOOL WALK STOP	G9-100-Q06_1 (TC1776_1)	600 x 225	14.2 (e)
SCHOOL BIKE STOP	G9-100-Q06_2 (TC1776_2)	600 x 225	14.2 (e)

## 14.2 Signs

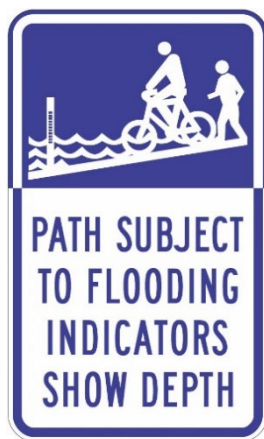
### New

The following signs are approved for use in Queensland:

- a) UNDERPASS SUBJECT TO FLOODING (G9-100-Q01\_1), PATH SUBJECT TO FLOODING INDICATORS SHOW DEPTH (G9-100-Q01\_2)*



G9-100-Q01\_1 (TC1019)



G9-100-Q01\_2 (TC1831)

The UNDERPASS SUBJECT TO FLOODING (G9-100-Q01\_1) sign shall be used on bikeways and paths at the approach to an underpass where there is a possibility of flooding.

The PATH SUBJECT TO FLOODING INDICATORS SHOW DEPTH (G9-100-Q01\_2) sign shall only be used on bikeways and paths where flood depth indicators (G9-22 series) are installed, there is a possibility of flooding, and the pavement has been designed to be trafficable with care both during, and after, inundation.

- b) *PEDESTRIANS MUST CROSS AT SIGNALS (G9-100-Q02\_1), PEDESTRIANS MUST CROSS AT FACILITY (G9-100-Q02\_2)*



G9-100-Q02\_1 (L) G9-100-Q02\_1 (R) G9-100-Q02\_2 (L) G9-100-Q02\_2 (R)

The PEDESTRIANS MUST CROSS AT SIGNALS (G9-100-Q02\_1) and PEDESTRIANS MUST CROSS AT FACILITY (G9-100-Q02\_2) signs may be used to encourage pedestrians near a pedestrian crossing facility to cross at a designated location. These signs shall not be installed at the designated crossing location itself.

- c) *Pedestrian UNDERPASS (G9-100-Q03)*



G9-100-Q03 (TC1731)

The Pedestrian UNDERPASS sign may be used on the approach to a pedestrian underpass where the location might not otherwise be apparent.

- d) *Pedestrian Direction (G9-100-Q04), PEDESTRIANS MUST USE FOOTPATH (G9-100-Q05\_1), PEDESTRIANS MUST USE OVERPASS (G9-100-Q05\_2)*



G9-100-Q04 (L)



G9-100-Q04 (R)



G9-100-Q05\_1



G9-100-Q05\_2

The Pedestrian Direction (G9-100-Q04) sign may be used where the direction pedestrians should travel might not be apparent.

The PEDESTRIANS MUST USE FOOTPATH (G9-100-Q05\_1) and PEDESTRIANS MUST USE OVERPASS (G9-100-Q05\_2) signs may be used to supplement the Pedestrian Direction (G9-100-Q04) sign where applicable.

- e) *SCHOOL WALK STOP (G9-100-Q06\_1), SCHOOL BIKE STOP (G9-100-Q06\_2)*



G9-100-Q06\_1



G9-100-Q06\_2

The SCHOOL WALK STOP (G9-100-Q06\_1) and SCHOOL BIKE STOP (G9-100-Q06\_2) signs should be used to indicate a location point for the assembly / collection of school children participating in a supervised walk or bike ride to / from school as part of the school bike bus or school walking bus program. These signs shall not be installed facing road traffic, so as not to be confused with bus stop signs.

Where Federal Government, State Government, Local Government and school logos are required see G9-100-Q06\_3 and G9-100-Q06\_4.

## **Appendices**

### **Appendix A – Model instructions for adult supervisors at crossings (informative)**

Not accepted

See *Queensland Road Safety Technical User Volumes (QRSTUV) Guide to Schools*.

